

STATE OF OKLAHOMA
CITY OF WEATHERFORD

PLAN OF PROPOSED
CITY STREET
FEDERAL AID PROJECT NO. ACSTP-220B(040)SS
GRADE, DRAIN, SURFACING & TRAFFIC SIGNAL PLANS
MAIN STREET AND WASHINGTON AVE.
CUSTER COUNTY

STATE JOB NO. 27911(06)
CONTROL SECTION NO. 40-20-04

FOR SURVEY REFERENCE
SEE SURVEY CONTROL DATA SHEET 27 &
SEE GEOMETRIC LAYOUT SHEET 28-30

DESIGN DATA

MAIN ST.
ADT 2015 - 14700
ADT 2035 - 21478
D - 55%
T(% of DHV) - 5%
V - 40MPH
(20)FLEX ESAL'S - 6.9 M

DESIGN DATA

WASHINGTON AVE.
ADT 2015 - 7500
ADT 2035 - 13300
D - 55%
T(% of DHV) - 5%
V - 35MPH
(20)FLEX ESAL'S - 4.0 M

DESIGN DATA

I-40 RAMP C
ADT 2015 - 3300
ADT 2035 - 4620
D - 100%
T₃ - 5%
T(% of DHV) - 6%
T(% of AADT) - 10%
V - 25MPH
(20)FLEX ESAL'S - 1.8 M

DESIGN DATA

I-40 RAMP A
ADT 2015 - 3300
ADT 2035 - 4620
D - 100%
T₃ - 5%
T(% of DHV) - 6%
T(% of AADT) - 10%
V - 25MPH
(20)FLEX ESAL'S - 2.1 M

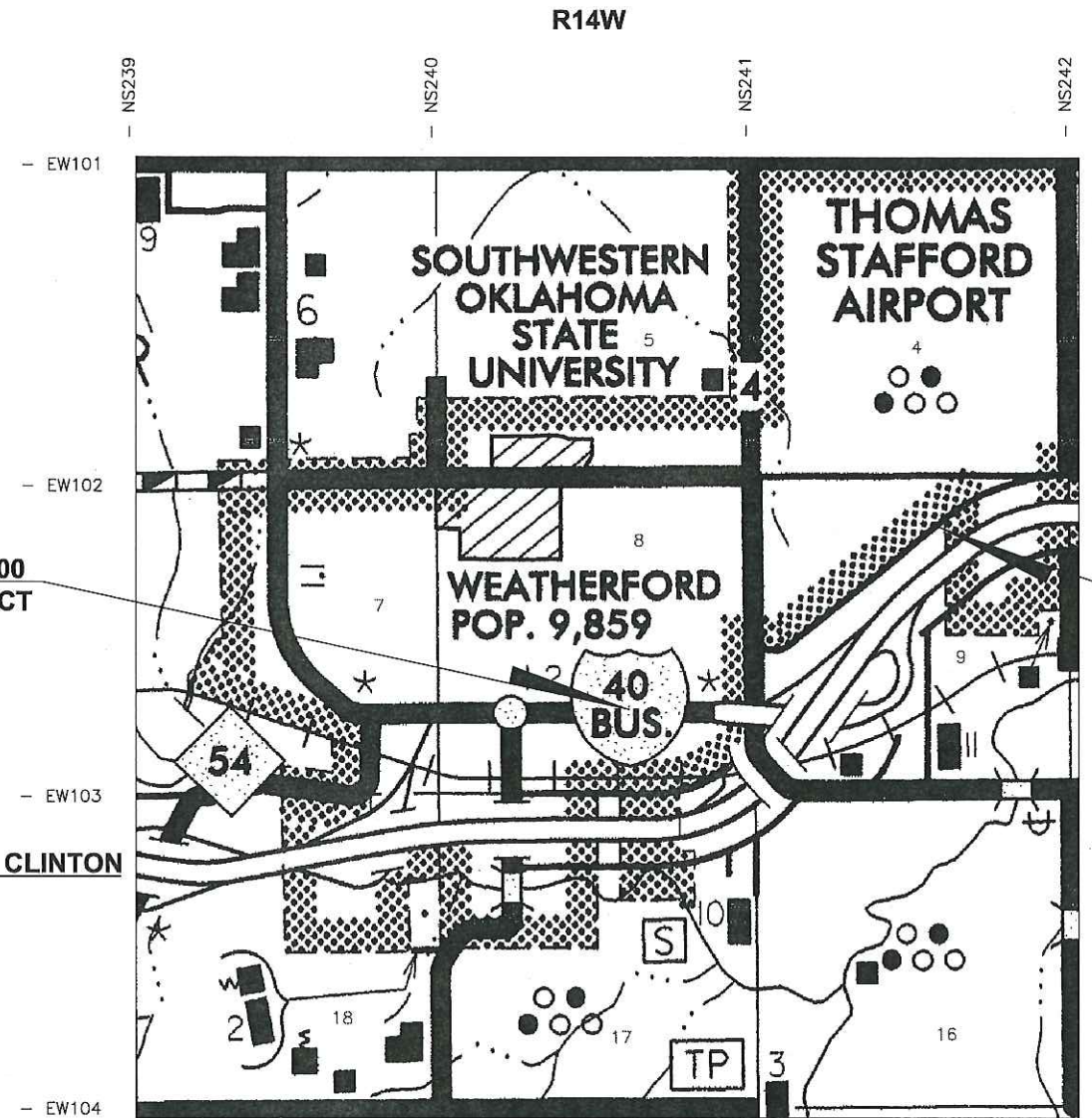
FOR INDEX OF SHEETS & STANDARDS,
REFER TO SHEET NO. 2.

SCALES

PLAN 1:50
PROFILE HOR. 1:50
VER. 1:10
LAYOUT MAP 1"=2640'

CONVENTIONAL SYMBOLS

- PROPOSED ROADS
- SECTION LINES
- - - QUARTER SECTION LINES
- x x FENCES
- - - EXISTING GRADE
- - - EXISTING ROADS
- BASE LINE
- - - PROPOSED GRADE
- TUG- COMMUNICATION LINES (EXISTING)
- PW- POWER LINES (EXISTING)
- OHE- OVERHEAD POWER LINES (EXISTING)
- PUG- POWER UNDER GROUND LINES (EXISTING)
- G- GAS LINE (EXISTING)
- SS- SANITARY SEWER LINES (EXISTING)
- ST- STORM SEWER LINES (EXISTING)
- W- WATER LINES (EXISTING)
- TUG- COMMUNICATION LINES (PROPOSED)
- OHE- POWER LINES (PROPOSED)
- PUG- POWER LINES (PROPOSED)
- G- GAS LINE (PROPOSED)
- SS- SANITARY SEWER LINES (PROPOSED)
- ST- STORM SEWER LINES (PROPOSED)
- W- WATER LINES (PROPOSED)
- ||||| BUILDINGS
- - - DRAINAGE STRUCTURES (EXISTING)
- - - DRAINAGE STRUCTURES (PROPOSED)
- - - RIGHT-OF-WAY LINES (EXISTING)
- - - RIGHT-OF-WAY LINES (PROPOSED)
- - - RIGHT-OF-WAY FENCE
- - - FLOWLINE (EXISTING)
- - - FLOWLINE (PROPOSED)
- - - TOE OF SLOPE (EXISTING)
- - - TOE OF SLOPE (PROPOSED)
- CITY LIMITS
- ~~~~~ LANDSCAPE
- +++++ RAILROAD



STA. 4370+60.00
BEGIN PROJECT
MAIN STREET

STA. 4391+25.00
END PROJECT
MAIN STREET

ROADWAY LENGTH _____ 2065 FT _____ 0.391 MI
BRIDGE LENGTH _____ FT _____ MI
TOTAL PROJECT LENGTH _____ 2065 FT _____ 0.391 MI
EXCEPTIONS _____ NONE
EQUATIONS _____ NONE

PREPARED BY:
CEC CORPORATION
CA32 6/30/18
OKLAHOMA CITY, OKLAHOMA

CEC
infrastructure solutions

FELICIA C. JACKSON
22820
7-8-2016
DATE

FELICIA C. JACKSON
OKLA. REG. NO. 22820

PREPARED BY:
CEC CORPORATION
CA32 6/30/18
OKLAHOMA CITY, OKLAHOMA

CEC
infrastructure solutions

KYLE L. MORSE
27689
7-8-2016
DATE

KYLE L. MORSE
OKLA. REG. NO. 27689

TRAFFIC SIGNALS
SHEETS 53-56

OKLAHOMA DEPARTMENT OF TRANSPORTATION	DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION
DATE APPROVED _____	DATE APPROVED _____
BY _____ CHIEF ENGINEER	BY _____ DIVISION ADMINISTRATOR

PROJ. NO. ACSTP-220B(040)SS SHEET NO. 1

P.E. NO. 27911(01)

2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION-ENGLISH GOVERN,
APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY
ADMINISTRATION, JANUARY 04, 2010

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1.	TITLE SHEET
2.	INDEX OF SHEETS & STANDARDS
3.-7.	TYPICAL SECTIONS
8.	PAY ITEMS - ROADWAY
9.	PAY ITEMS NOTES - ROADWAY
10.	PAY ITEMS - TRAFFIC
11.-12.	PAY ITEMS NOTES - TRAFFIC
13.	SUMMARY OF DRAINAGE STRUCTURES
14.-17.	SUMMARY SHEETS
18.	STORM WATER MANAGEMENT PLAN
19.-26.	TEMPORARY EROSION CONTROL SHEETS
27.	SURVEY CONTROL DATA SHEET
28.-30.	GEOMETRIC LAYOUT
31.	OVERVIEW LAYOUT SHEET
32.-33.	MASS HAUL DIAGRAMS
34.-42.	PLAN AND PROFILE SHEETS
43.-52.	SIGNING AND STRIPING PLANS
53.-56.	TRAFFIC SIGNAL PLAN SHEETS
57.-71.	SEQUENCING & TRAFFIC CONTROL SHEETS
72.-74.	SPECIAL SIGN DETAILS
75.-78.	JOINT LAYOUT
79.	REMOVAL LAYOUT
80.-83.	CONSTRUCTION LAYOUT SHEETS
84.	HIGH MAST LIGHT FOUNDATION REPORT
85.	SPECIAL TAPERED CONC. PARAPET END SECTION
86.	STR. NO. 7 GENERAL NOTES AND PAY ITEMS
87.	GENERAL PLAN & ELEVATION
88.	RCB DETAILS
X1.-X48	CROSS SECTIONS

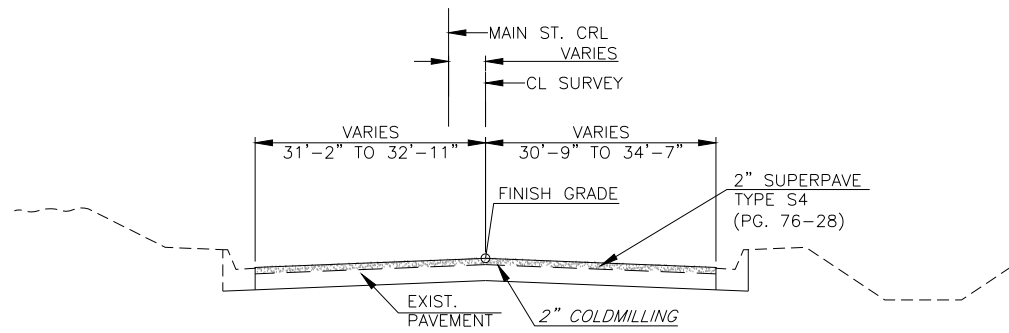
THE FOLLOWING ODOT STANDARDS ARE
REQUIRED FOR THIS PROJECT:

ROADWAY	BRIDGE	TRAFFIC			
SSS-1-1	RCB-E1-H4-01-01E	TCS1-1-01	PMAP1-2-00	PM1-1-02	SKT-1-00
TSC2-3-2	RCB-E1-H4-00-02-01E	TCS2-1-00	SA1-1-02	PM2-1-01	GA31-1-00
TSD-2-0	RCB-CW1-D4-0-01E	TCS3-1-01	ID1-1-00	PM3-1-02	GHW1-1-00
ASCD-5-2	RCB-C1-8(14-20)-01E	TCS4-1-01	ID2-1-00	PM4-1-01	GHW2-1-00
CSCD-5-3		TCS5-1-00	SNS1-1-02	PM5-1-00	
LECS-4-1		TCS6-1-02	TSSS1-1-00	PM6-1-00	
LTU-4-0		TCS7-1-02	PWD1-2-00	PM8-1-00	
PSE-1-0		TCS8-1-00	CFD1-2-00	DU1-1-00	
WCR-3-1		TCS9-1-01	TSSP1-1-00	DU2-1-00	
TWD-1-0		TCS10-1-00	SWD1-1-00	RSD1-1-00	
CET4S-3-2		TCS11-1-01	HMF1-1-00	RSD2-1-00	
CET6S-3-2		TCS14-1-00	HMP1-1-00	WSD1-1-00	
PCES-4-1		TCS17-1-00		WSD2-1-00	
SMD-3-1		TCS18-1-01		WSD3-1-00	
CI-1-2		TCS19-1-01		MSD1-1-00	
SSIF-4-0		TCS20-1-00		MSD2-1-00	
CIG-3-0		TCS21-1-02		MSD3-1-01	
MFC-4-1		TCS24-1-02		MSD4-1-00	
MJB-3-1				MSD5-1-00	
SPI-4-1				SIS3-1-01	
SPB-1-4				SIS4-1-00	
FHTCP-3-1				SBS1-1-00	
SBI-4-2				SBS3-1-00	
CLB-1-2				SBS4-1-00	
DC-3-2				SBS5-1-00	
PDT-1-3				GMS1-1-00	
SUEL1-3-2				GMS2-1-00	
SUEL2-3-2				SSP1-1-02	
SUEL3-3-2				SSA2-1-00	
				FGS1-1-00	
				FGS2-1-01	
				SPA1-1-00	

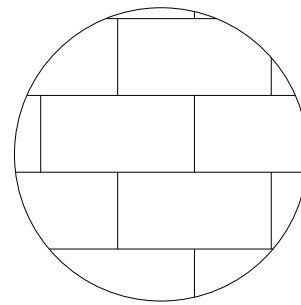
MAIN ST. CUSTER COUNTY

INDEX OF SHEETS AND STANDARDS

JOB PIECE NO. 27911(06) SHEET NO. 2

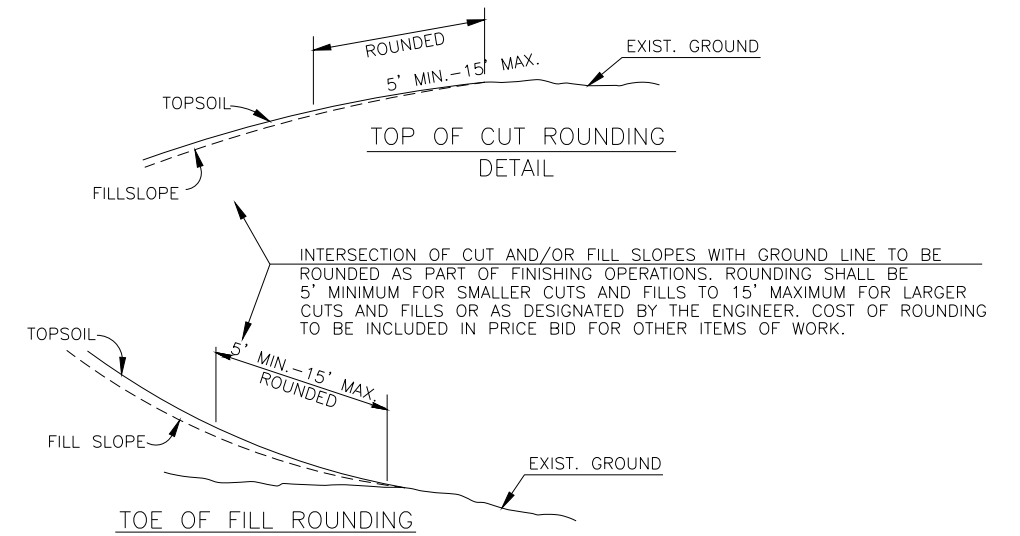


MAIN STREET
STA. 4370+60.00 TO STA. 4375+48.00

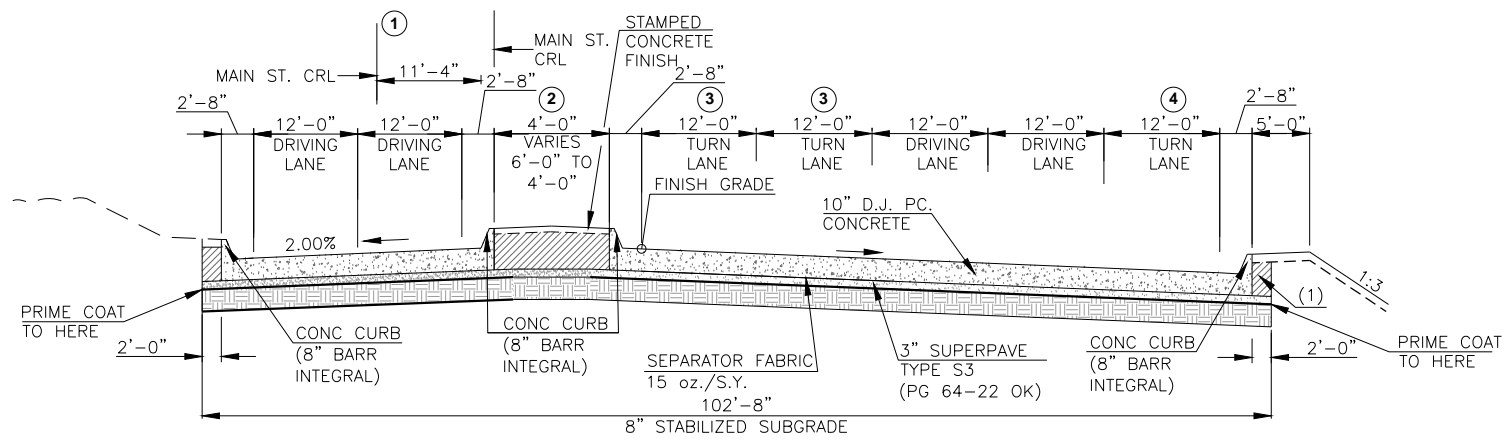


BRICK STAMPED FACE PATTERN

RED DYED CONCRETE WITH
BRICK STAMPED FACE
(SEE SPECIAL PROVISION)



- (1) BACKFILL NOTE:
THIS AREA TO BE BACKFILLED AND COMPACTED AS A PART OF THE FINISHING OPERATIONS. COST TO BE INCLUDED IN UNCLASSIFIED BORROW.
- (2) TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATION. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATION SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM. THE GRADING LINE IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- (3) SEE ROUNDING DETAIL THIS SHEET.



MAIN STREET
STA. 4375+48.00 TO STA. 4379+33.58

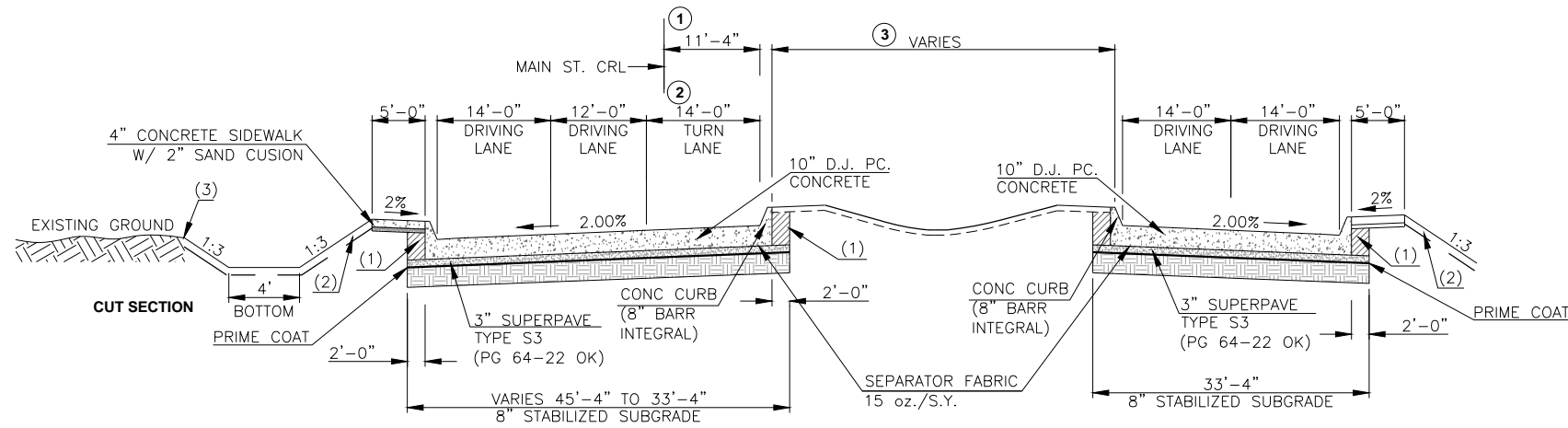
- ① VARIES FROM 11'-4" TO 0'-0" STA. 4380+28.74 TO STA. 4382+09.60
- ② VARIES FROM 6'-0" TO 4'-0" STA. 4375+48.00 TO STA. 4377+00.00
- ③ VARIES FROM 0'-0" TO 12'-0" STA. 4376+00.00 TO STA. 4377+00.00
- ④ VARIES FROM 0'-0" TO 12'-0" STA. 4376+00.00 TO STA. 4377+93.34

MAIN ST. CUSTER COUNTY

TYPICAL SECTION 1 OF 5

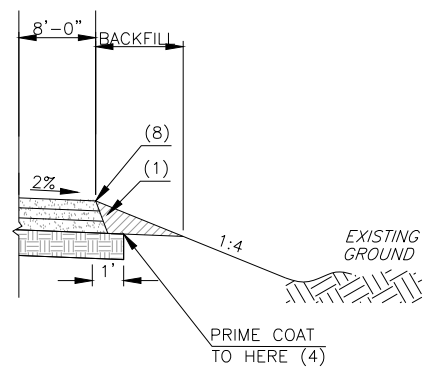
JOB PIECE NO. 27911(06) SHEET NO. 3

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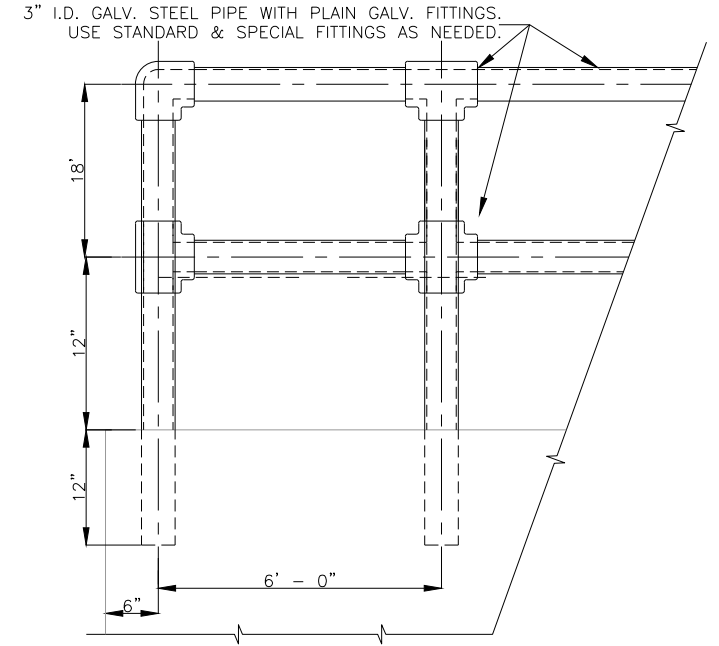
**MAIN STREET
STA. 4380+23.65 TO STA. 4382+93.94**

- ① VARIES FROM 11'-4" TO 0'-0" STA. 4380+23.65 TO STA. 4381+14.86
- ② VARIES FROM 14'-0" TO 0'-0" STA. 4381+14.86 TO STA. 4381+76
- ③ VARIES FROM 14'-2" TO 10'-7" STA. 4380+23.65 TO STA. 4381+14.86

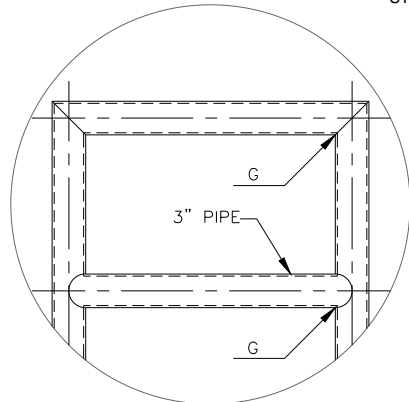


**SHOULDER DETAIL
STA. 4383+88.23 TO STA. 4387+43.67**

- (1) BACKFILL NOTE:
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- (3) SEE ROUNDING DETAIL SHEET 3.

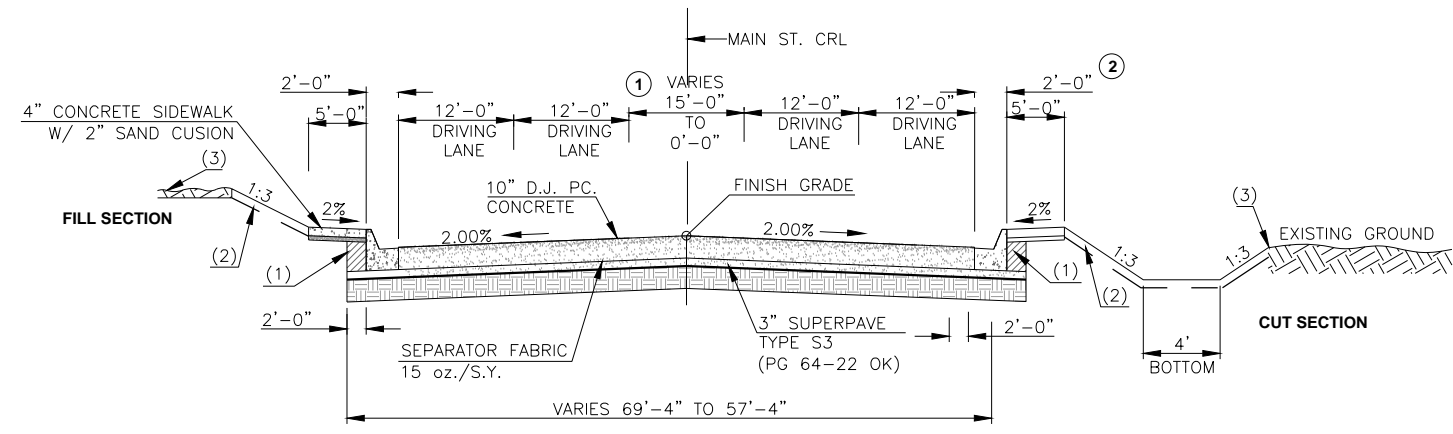


**PIPE HANDRAIL DETAIL
STA. 4377+62.00 TO STA. 4377+82.00**



**ALTERNATE DETAIL
(USING WELD CONNECTIONS
ON PIPE HANDRAILS)**

- NOTES:
- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CURRENT OKLAHOMA CITY STANDARD SPECIFICATIONS.
 - REINFORCING STEEL IN BOTTOM SLAB OR WING WALL FOOTING SHALL BE SUPPORTED ON SLAB BOLSTERS. SLAB BOLSTERS SHALL BE SUPPORTED ON CLASS "C" CONCRETE STRIPS OR BLOCKS PLACED AT 4' C/C.
 - REINFORCING STEEL IN TOP SLAB SHALL BE SUPPORTED ON SLAB BOLSTERS.
 - REINFORCING STEEL IN THE WALLS SHALL BE HELD IN PLACE BY METAL CHAIRS. MAXIMUM SPACING OF THE CHAIRS SHALL BE 6'.
 - COST OF BOLSTERS, CHAIRS AND CONCRETE STRIPS OR BLOCKS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.
 - ALL COST OF WEEP HOLES SHALL BE INCLUDED IN OTHER ITEMS OF WORK.



**MAIN STREET
STA. 4382+93.94 TO STA. 4391+25.00**

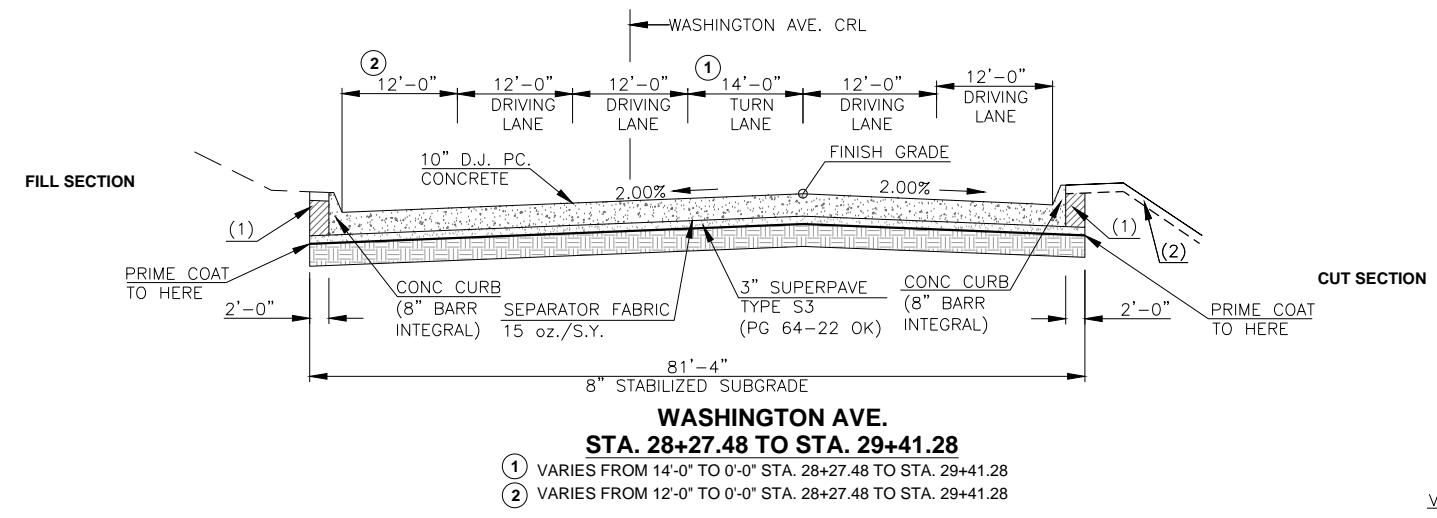
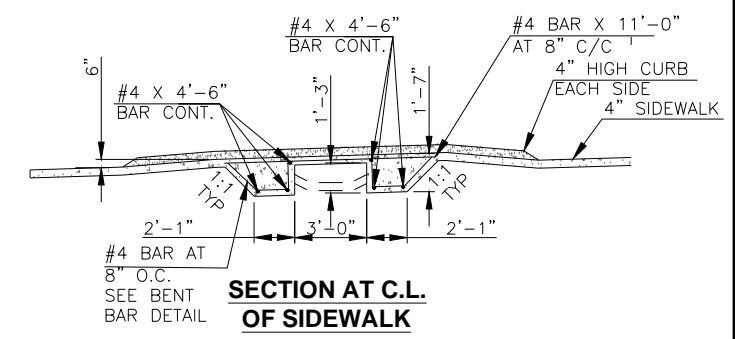
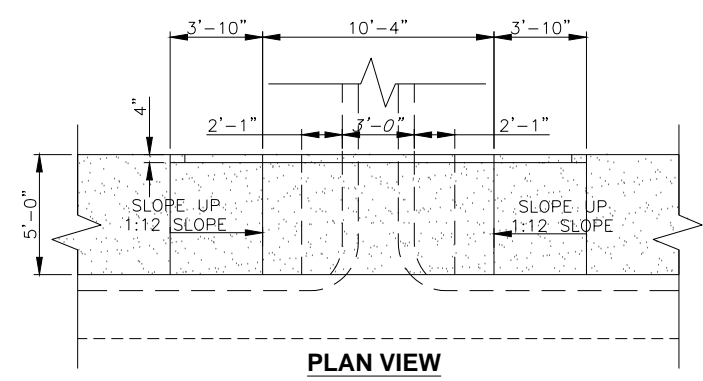
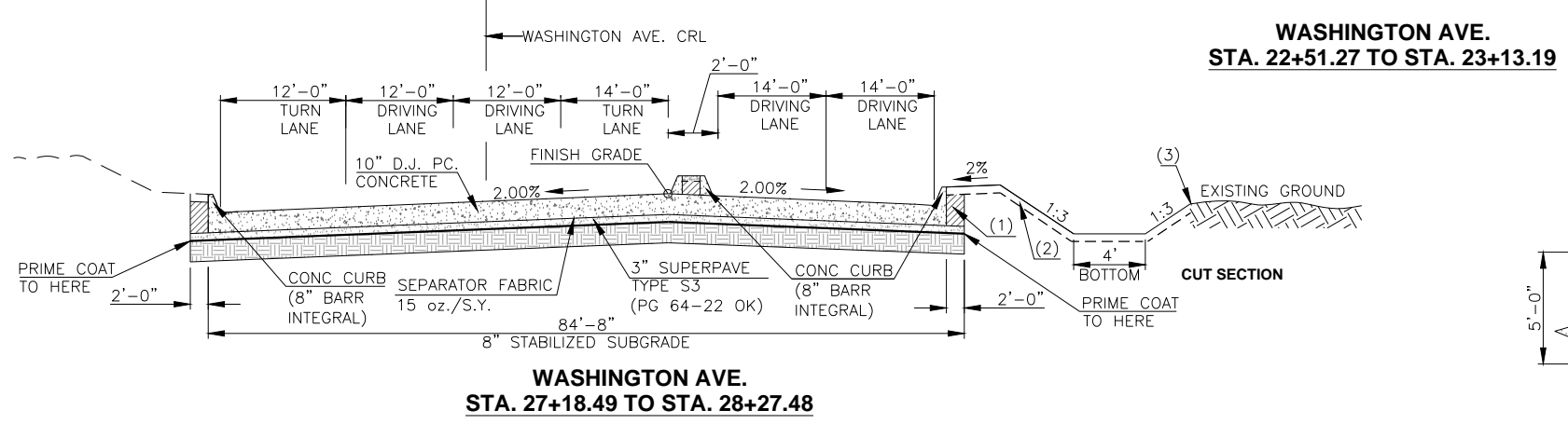
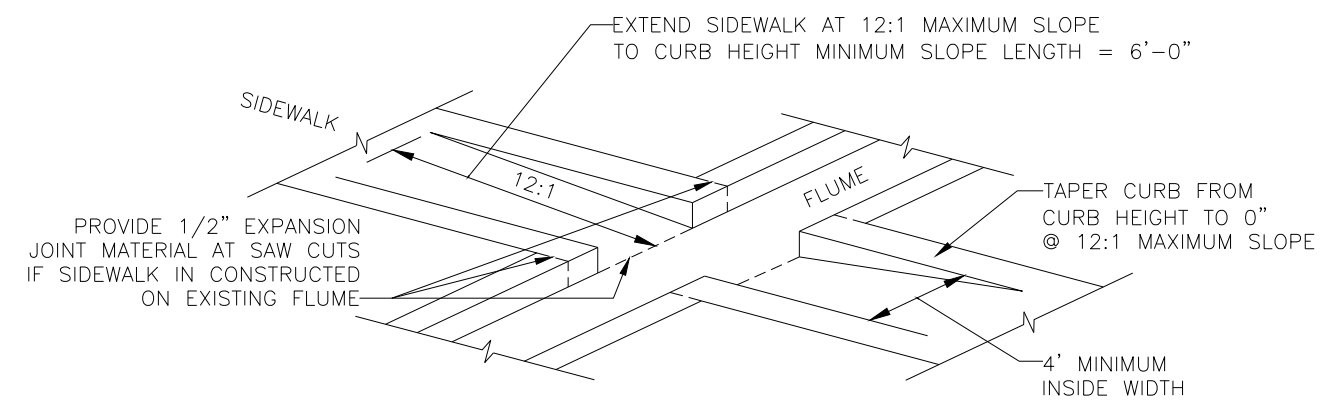
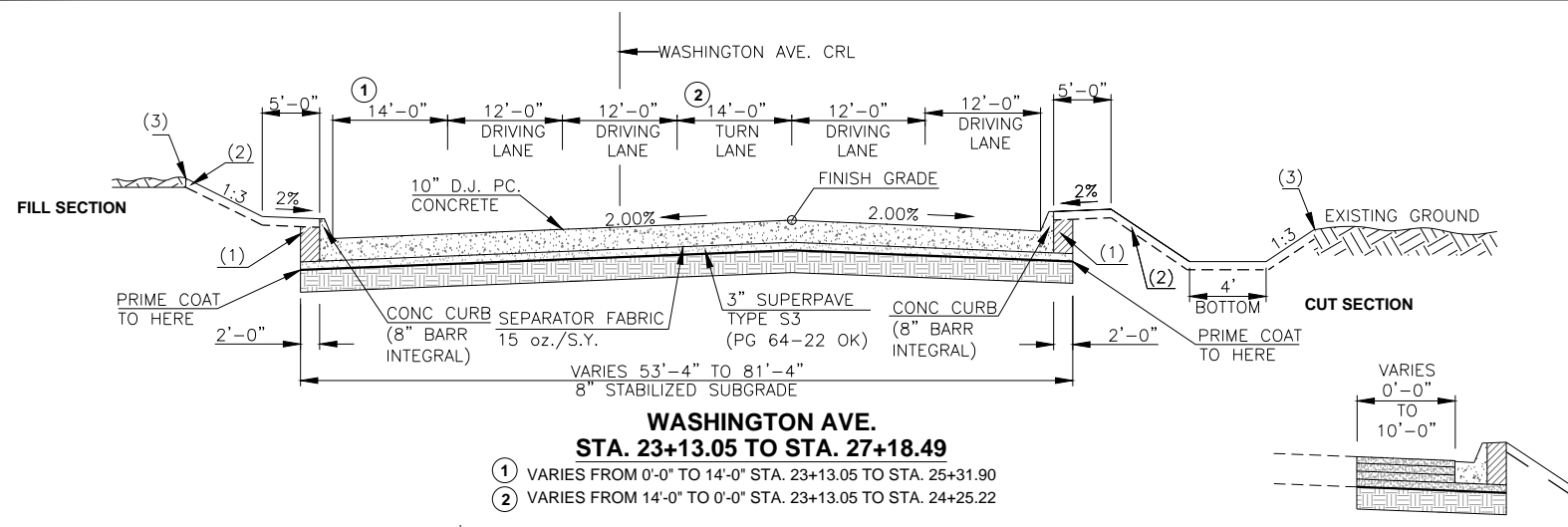
- ① VARIES FROM 15'-0" TO 0'-0" STA. 4382+93.94 TO STA. 4383+92.72
- ② SEE SHOULDER DETAIL

MAIN ST. CUSTER COUNTY

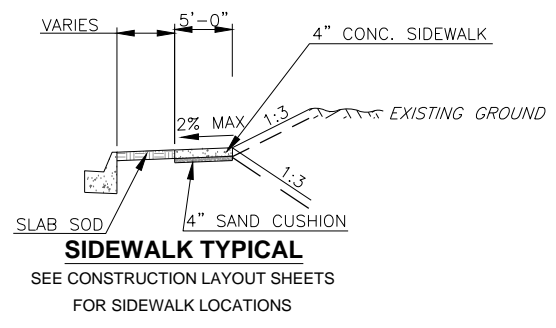
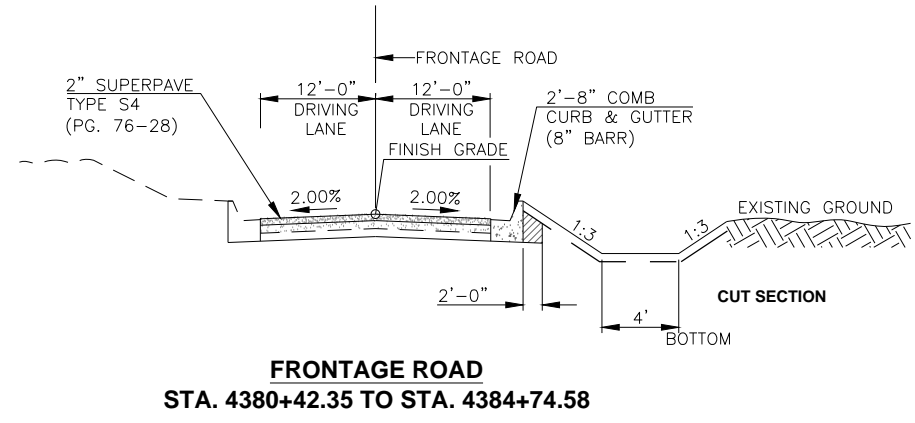
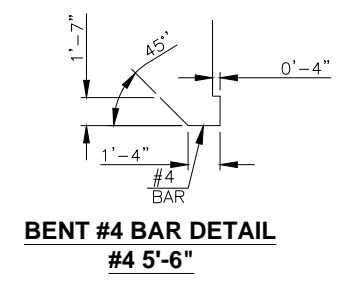
TYPICAL SECTION 2 OF 5

JOB PIECE NO. 27911(06) SHEET NO. 4

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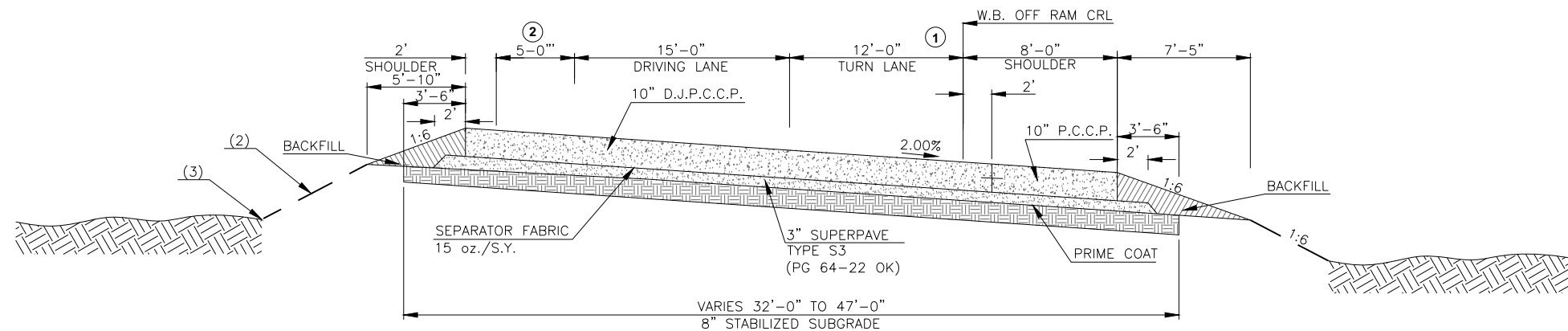


NOTE: COST OF ADDITIONAL P.C. CONCRETE & REINFORCING STEEL TO CONSTRUCT SPECIAL SIDEWALK SECTION SHALL BE INCLUDED IN PRICE BID FOR 4" CONCRETE SIDEWALK.

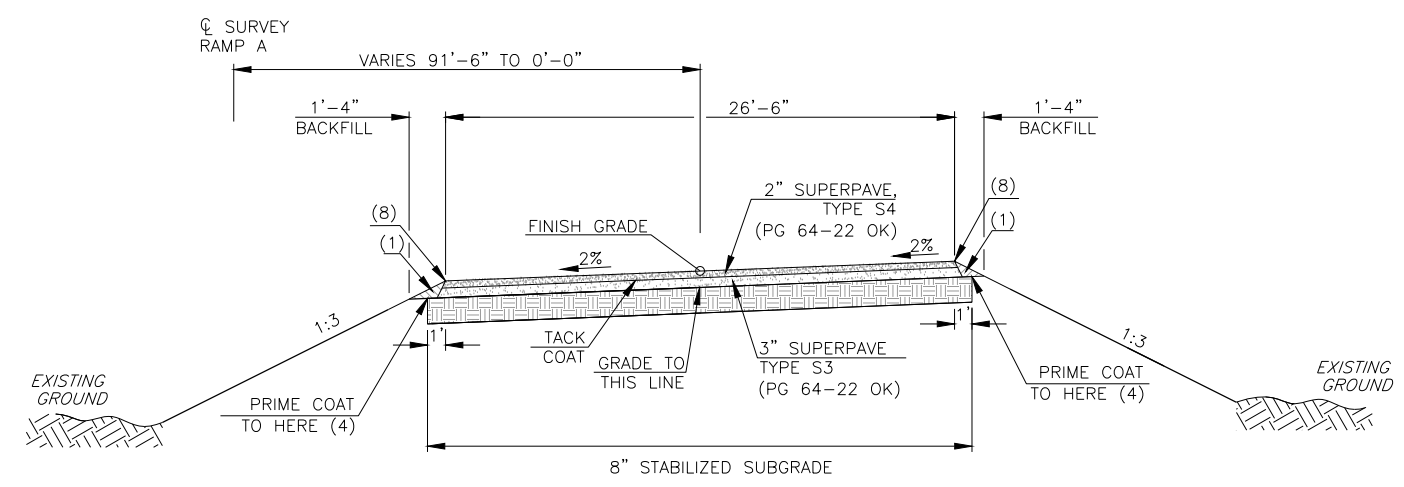


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- (3) SEE ROUNDING DETAIL SHEET 3.

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**RAMP A
WEST BOUND OFF RAMP
STA. 4399+39.00 TO STA. 4411+92.17**
 (1) VARIES FROM 0'-0" TO 12'-0" STA. 4407+73.65 TO STA. 4409+00.00
 (2) STA. 4406+50.00 TO STA. 4410+00.00



TEMPORARY RAMP

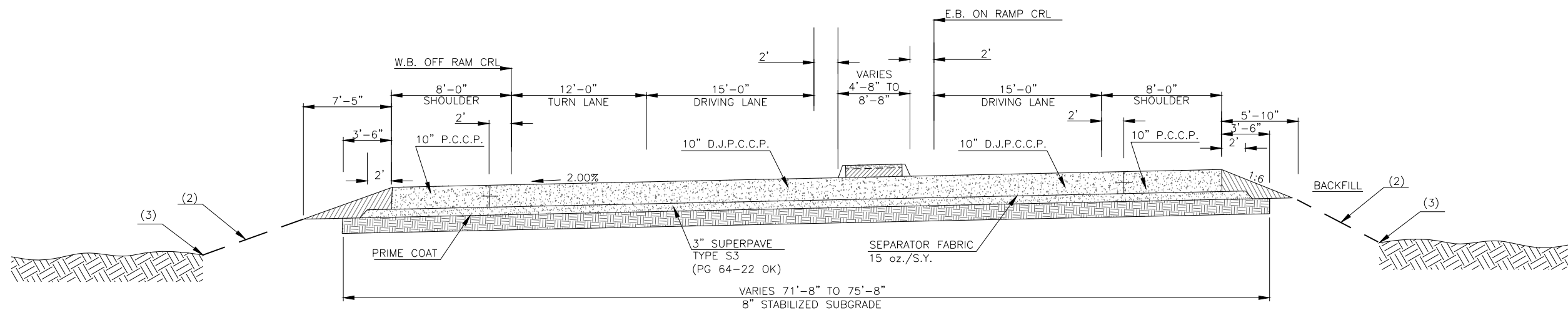
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- (3) SEE ROUNDING DETAIL SHEET 3.

MAIN ST. CUSTER COUNTY

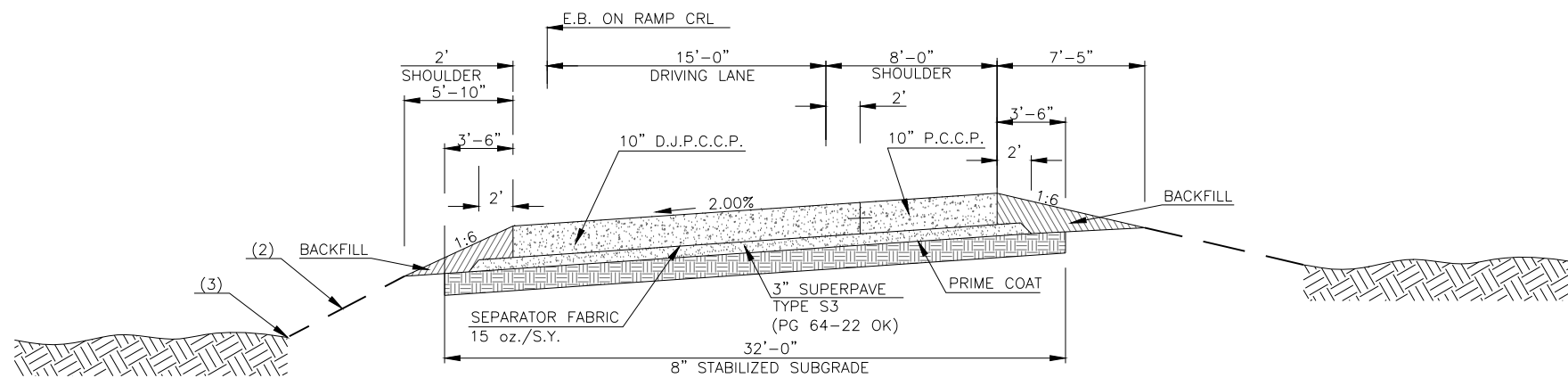
TYPICAL SECTION 4 OF 5

JOB PIECE NO. 27911(06) SHEET NO. 6

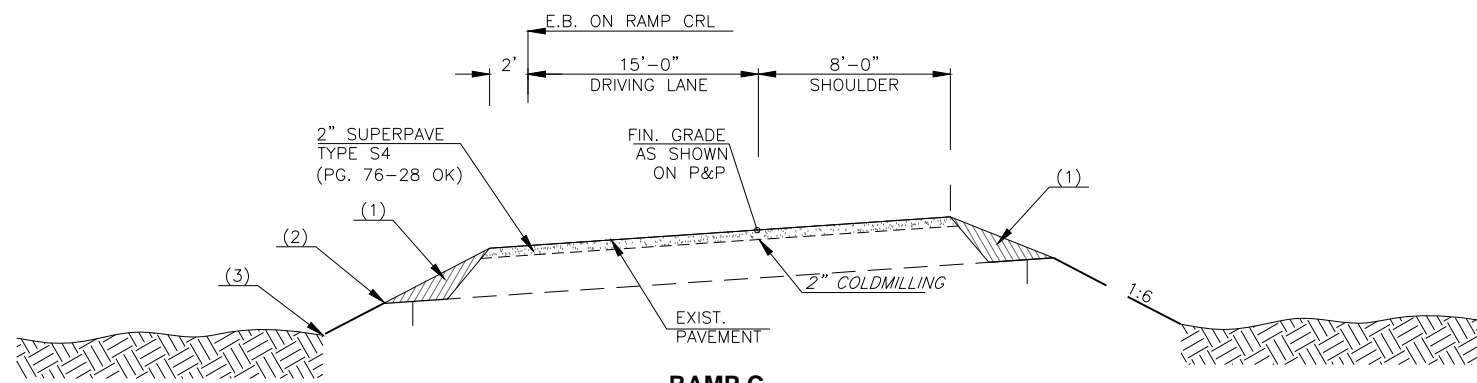
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**RAMP C
EAST BOUND ON RAMP
STA. 4382+45.00 TO STA. 4383+00.00**



**RAMP C
EAST BOUND ON RAMP
STA. 4383+00.00 TO STA. 4388+75.00**



**RAMP C
EAST BOUND ON RAMP
STA. 4388+75.00 TO STA. 4389+95.47**

- (1) BACKFILL NOTE:
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- (3) SEE ROUNDING DETAIL SHEET 3.

MAIN ST. CUSTER COUNTY

TYPICAL SECTION 5 OF 5

JOB PIECE NO. 27911(06) SHEET NO. 7

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27911(06) 0600			
PAY QUANTITIES			
STAKING			
ITEM	DESCRIPTION	UNIT	QUANTITY
642(B)	0096 CONSTRUCTION STAKING LEVEL II	(12) L.SUM	1.00

27911(06) 0640			
PAY QUANTITIES			
CONSTRUCTION			
ITEM	DESCRIPTION	UNIT	QUANTITY
220	2800 SWPPP DOCUMENTATION AND MANAGEMENT	L.SUM	1.00
641	1552 MOBILIZATION	L.SUM	1.00

27911(06) 0100			
PAY QUANTITIES			
ROADWAY			
ITEM	DESCRIPTION	UNIT	QUANTITY
201(A)	0102 CLEARING AND GRUBBING	L.SUM	1.00
202(A)	0183 UNCLASSIFIED EXCAVATION (1)(R-1)	C.Y.	21,652.00
202(D)	0184 UNCLASSIFIED BORROW (R-4)	C.Y.	26,590.00
205(A)	4229 TYPE A-SALVAGED TOPSOIL (R-5)	L.SUM	1.00
221(C)	2801 TEMPORARY SILT FENCE (3)(2)	L.F.	4,000.00
221(D)	2803 TEMPORARY SEDIMENT FILTER (2)	EA.	5.00
221(F)	0100 TEMPORARY SILT DIKE (2)	L.F.	300.00
229	4318 DITCH LINER PROTECTION	L.F.	80.00
230(A)	2806 SOLID SLAB SODDING (R-7)(R-8)	S.Y.	23,327.00
233(A)	2817 VEGETATIVE MULCHING (R-11)	AC.	4.82
241	2832 MOWING (4)(R-16)	AC.	9.64
307(K)	4300 STABILIZED SUBGRADE (5)	S.Y.	27,148.00
325	5271 SEPARATOR FABRIC	S.Y.	26,651.00
402(E)	0225 TRAFFIC BOUND SURFACE COURSE TYPE E (6)(R-25)	TON	250.00
407(B)	0250 TACK COAT (7)	GAL.	329.00
408	5774 PRIME COAT (R-28)	GAL.	9,397.00
411(B)	5945 SUPERPAVE, TYPE S3 (PG 64-22 OK) (R-32)	TON	4,447.00
411(C)	5950 SUPERPAVE, TYPE S4 (PG 76-28 OK) (R-32)	TON	584.00
412	5267 COLD MILLING PAVEMENT (R-34)	S.Y.	5,322.00
414(B)	5725 DOWEL JOINTED P.C. CONCRETE PAVEMENT (PLACEMENT)	S.Y.	24,058.00
414(G)	5275 P.C. CONCRETE FOR PAVEMENT	C.Y.	6,683.00
501(A)	0313 STRUCTURAL EXCAVATION UNCLASSIFIED	C.Y.	85.00
509(A)	0319 CLASS AA CONCRETE (8)(R-1)	C.Y.	215.00
509(D)	0325 CLASS C CONCRETE (R-41)	C.Y.	40.00
511(A)	0332 REINFORCING STEEL (R-1)	LB.	31,990.00
609(A)	0380 CONCRETE CURB (8" BARRIER-INTEGRAL)	L.F.	4,662.00
609(B)	1526 2'-8" COMB. CURB & GUTTER (8" BARRIER)	L.F.	656.00
610(A)	0602 4" CONCRETE SIDEWALK	S.Y.	7,640.00
610(B)	0604 6" CONCRETE DRIVEWAY	S.Y.	1,513.00
610(C)	0610 8" CONCRETE DIVIDING STRIP	S.Y.	247.00
610(I)	4610 TACTILE WARNING DEVICE-NEW	S.F.	188.00
610(J)	4810 (PL) STAMPED CONCRETE FINISH (9)	S.Y.	215.00
611(G)	5112 INLET CI DES. 2 (STD) (R-44)	EA.	5.00
611(G)	6002 INLET (SMD-TYPE 2)	EA.	4.00
611(H)	5325 ADD'L DEPTH IN INLET CI DES. 2	V.F.	7.00
611(I)	4488 REPLACEMENT OF INLET FRM & GRT (SSIF-FRM, CIG-GRT-VG-F)	EA.	6.00
611(M)	4488 REPLACEMENT OF CAST IRON HOOD	EA.	10.00
612(A)	0641 MANHOLES ADJUST TO GRADE	EA.	2.00
612(C)	0645 INLET ADJUST TO GRADE	EA.	1.00
613(A)	0491 18" R.C. PIPE CLASS III (10)	L.F.	298.00
613(A)	0492 24" R.C. PIPE CLASS III (10)	L.F.	174.00
613(L)	5726 18" PREFAB. CULVERT END SECTION, ROUND (1)	EA.	1.00
619(A)	0920 REMOVAL OF STRUCTURES & OBSTRUCTIONS (R-48)(R-49)	L.SUM	1.00
619(B)	4726 REMOVAL OF CURB AND GUTTER (11)(R-49)(R-50)	L.F.	3,392.00
619(B)	4727 REMOVAL OF CONCRETE PAVEMENT (11)(R-49)(R-50)	S.Y.	5,162.00
619(B)	4728 REMOVAL OF ASPHALT PAVEMENT (11)(R-49)(R-50)	S.Y.	24,013.00
619(B)	4766 REMOVAL OF CONCRETE DRIVEWAY (11)(R-49)(R-50)	S.Y.	1,232.00
619(B)	4780 REMOVAL OF GUARDRAIL (R-49)(R-50)	L.F.	2,780.00
619(B)	6133 REMOVAL OF 8" CONCRETE DIVIDING STRIP (R-49)(R-50)	S.Y.	812.00
622(A)	4746 PIPE RAILING	L.F.	20.00
623(A)	0932 BEAM GUARDRAIL W-BEAM SINGLE	L.F.	650.00
623(F)	8300 GUARDRAIL TRAIL END TURNDOWN (31")	EA.	2.00
623(G)	8590 GUARDRAIL END TREATMENT (31")	EA.	2.00
627(A)	4317 CONCRETE LONGITUDINAL BARRIER, DESIGN 1 (R-54)	L.F.	550.00
627(B)	4410 CONCRETE LONGITUDINAL BARRIER END SECTIONS	EA.	4.00

MAIN ST. CUSTER COUNTY

PAY ITEMS (ROADWAY)

JOB PIECE NO. 27911(06) SHEET NO. 8

GENERAL CONSTRUCTION NOTES:

IN ORDER TO ALLEVIATE DUST CONDITIONS DURING GRADING OPERATIONS AND BEFORE PAVEMENT WORK IS COMPLETED, THE CONTRACTOR SHALL SPRINKLE GRADING AT INTERVALS APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE CONTRACTOR SHALL NOT WASTE ANY EXCESS EXCAVATION UNTIL ALL PLANNED EMBANKMENTS AND BACKFILLS ARE COMPLETED. EXCESS UNCLASSIFIED EXCAVATION MATERIAL DETERMINED BY THE ENGINEER TO BE SUITABLE FOR BACKFILL SHALL BE USED TO REDUCE ANY UNCLASSIFIED BORROW NEEDED. COST OF SECOND HANDLING SHALL BE INCLUDED IN OTHER ITEMS OF WORK. ANY REMAINING EXCESS EXCAVATION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND BE DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

PRIME COAT SHALL BE APPLIED TO THE SUBGRADE IMMEDIATELY AFTER FINAL COMPACTION AND SHAPING TO RETAIN MOISTURE FOR PROPER CHEMICAL REACTION OF THE SOIL ADDITIVE.

THE CONTRACTOR SHALL KEEP THE OPEN TRENCH DRAINED. COST IS TO BE INCLUDED IN OTHER ITEMS OF WORK.

AREAS ON WHICH SALVAGED TOPSOIL IS TO BE REPLACED SHALL HAVE 18-46-0 FERTILIZER APPLIED, AT THE RATE OF 150 POUNDS PER ACRE, JUST PRIOR TO THE REPLACEMENT OF SALVAGED TOPSOIL.

SURFACING OF RETURNS, UNLESS OTHERWISE SHOWN ON THE PLANS, SHALL BE OF THE SAME MATERIAL (BASE AND SURFACE) AS THAT OF THE ABUTTING SHOULDER OF THE MAINLINE. BASE AND SURFACE THICKNESS SHALL BE THE THICKNESS SHOWN ON PLANS.

THE ENGINEER SHALL CHECK GRADES AT RAMP TERMINALS AND MAKE ANY ADJUSTMENTS OF THE GRADES AND SUPERELEVATIONS, WHICH ARE REQUIRED TO OBTAIN SMOOTH PROFILES FOR BOTH EDGES OF THE RAMP PAVEMENT. CROSS SLOPE BREAKOVER SHALL NOT EXCEED 5% (FIVE PERCENT).

PRIOR TO FINAL ACCEPTANCE, ALL EXPOSED CURB SURFACES SHALL BE CLEANED OF ALL DISCOLORATION SUCH AS ASPHALT STAIN, TIRE MARKS, OR OTHER DISFIGUREMENT.

EXCESS ASPHALT AT JOINTS AND CRACKS IN EXISTING PAVEMENT SHALL BE REMOVED FLUSH TO TOP OF PAVING IN A MANNER APPROVED BY THE ENGINEER.

IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. *CALL OKIE* 1-800-522-6543 OR 811.

THIS PROJECT IS LOCATED NEAR KNOWN SOURCES OF GYPSUM (SULFATE) DEPOSITS. SPECIAL ATTENTION SHOULD BE USED TO AVOID BORROW MATERIAL THAT COULD ADVERSELY INTERACT WITH THE CALCIUM BASED ADDITIVES (FLY ASH, PORTLAND CEMENT, CEMENT KILN DUST, AND LIME) USED IN THE STABILIZED SUBGRADE. THE CONTRACTOR MAY BE REQUIRED TO PROVED SULFATE TESTING OF BORROW PIT SITES AS DIRECTED BY THE ENGINEER.

ENVIRONMENTAL MITIGATION NOTES:

LATITUDE/LONGITUDE	OCC FACILITY NO./CASE NO.	FACILITY
35.5255, -98.6943	20-09917/064-3607	FASTLANE

PETROLEUM CONTAMINATIONS MAY EXIST AT OR NEAR THE REFERENCED LEAKING UNDERGROUND STORAGE TANK (LUST) SITES. BASED ON THE AVAILABLE INFORMATION, CONTAMINATION IS NOT EXPECTED TO AFFECT CONSTRUCTION ACTIVITIES, BUT IS STILL POSSIBLE. IN THE EVENT CONTAMINATED SOIL OR GROUNDWATER IS ENCOUNTERED, THE CONTRACTOR SHALL ADHERE TO ODOT'S HAZARDOUS MATERIALS SPECIFICATION 107.15 AND NOTIFY THE RESIDENT ENGINEER, WHO MAY THEN CONTACT THE ENVIRONMENTAL PROGRAMS DIVISION AT (405) 521-3026 FOR ASSISTANCE.

SWALLOW NOTE:
CLIFF SWALLOWS AND BARN SWALLOWS ARE SMALL COLONIAL NESTING BIRDS PROTECTED BY THE FEDERAL MIGRATORY BIRD TREATY ACT. THESE SPECIES COMMONLY USE BRIDGES AND CULVERTS FOR NESTING. THE NESTING SEASON FOR THE SWALLOWS RUNS FROM APRIL 1 TO AUGUST 31. SWALLOW USE OF THE WASHINGTON AVENUE BRIDGE OVER I-40 AND THE EAST AND WEST BOUND I-40 BRIDGES OVER I-40 EAST-BOUND ON AND OFF RAMP AT EXIT 82 HAS BEEN OBSERVED DURING THE INITIAL SURVEYS CONDUCTED AS PART OF THE BIOLOGICAL STUDIES IN 2012. THE RESIDENT ENGINEER WILL EVALUATE THE CONTRACTOR'S PROPOSED WORK METHODS AND CONCLUDE WHETHER THE PROPOSED WORK WOULD HARM THE NESTING BIRDS BEFORE WORK NEAR THE STRUCTURE IS AUTHORIZED. IF THE PROPOSED WORK WILL HARM THE NESTING BIRDS, THE BRIDGE MAY BE NETTED PRIOR TO APRIL 1 OR THE WORK WILL BE DELAYED UNTIL THE NESTING SEASON IS COMPLETE. METHODS OTHER THAN NETTING MUST BE PRE-APPROVED BY THE ODOT BIOLOGIST.

LOCATIONS OUTSIDE THE PROJECT AREA IN THE FOLLOWING AREA MUST NOT BE UTILIZED FOR BORROW, EQUIPMENT STAGING, HAUL ROADS, SPOIL DUMPS OR ANY OFF-SITE PROJECT-RELATED ACTIVITY.

T12N R14W
SECTION 16 - NW¼ SW¼ NE¼

PAY QUANTITY NOTES:

- (R-1) PAYMENT FOR THIS ITEM WILL BE BASED ON PLAN QUANTITY ONLY. SEE SECTION 109.01B OF THE STANDARD SPECIFICATIONS.
- (R-4) INCLUDES 1,000 CU. YDS. FOR DRIVEWAYS, RETURNS, DIKES, AND MISCELLANEOUS EARTHWORK.
- (R-5) AN ESTIMATED QUANTITY OF 3,240 C.Y. TOPSOIL TO BE RESERVED FOR REPLACEMENT OF APPROXIMATELY 5" COMPLETED FORESLOPES, DITCHES, AND BACKSLOPES. THIS QUANTITY IS INCLUDED IN THE EARTHWORK BALANCE. ANY ADDITIONAL EXCAVATION REQUIRED IN CUT SECTIONS TO ALLOW FOR PLACEMENT OF TOPSOIL TO FINAL GRADE, SHALL BE INCLUDED IN THE PRICE BID.
- (R-7) FOR SOLID SLAB SODDING PRICE BID TO INCLUDE COST OF 10-20-10 FERTILIZER, ESTIMATED AT 200 POUNDS PER 1,000 SQUARE YARDS.
- (R-8) FOR SOLID SLAB SODDING PRICE BID TO INCLUDE COST OF WATERING, ESTIMATED AT 40 GALLONS PER SQUARE YARD.
- (R-11) THE QUANTITIES ESTIMATED FOR TEMPORARY EROSION AND SEDIMENT CONTROL IS 4.82 ACRES.
- (R-16) QUANTITY BASED ON TWO APPLICATIONS.
- (R-25) ESTIMATED AT 120 LBS. PER CU. FT.
- (R-28) PRIME COAT SHALL BE APPLIED AT AN ESTIMATED RATE OF 0.35 GAL. PER SQ. YD. WHEN APPLIED TO SUBGRADE, AND 0.25 GAL. PER SQ. YD. WHEN APPLIED TO AGGREGATE BASE. THE ACTUAL CUTBACK PRIME COAT REQUIRED FOR PLACEMENT OPERATIONS WILL BE DETERMINED BY THE CONTRACTOR, AND SHALL CONSIDER THE RESIDUE FROM DISTILLATION PERCENTAGE SHOWN IN SECTION 708.03 OF THE STANDARD SPECIFICATIONS.
- (R-32) ESTIMATED AT 112 LBS. PER SQ. YD. PER 1" THICK.
- (R-34) PRICE BID TO INCLUDE COST OF FOG SEAL, MEETING THE REQUIREMENTS OF SECTION 407 OF THE STANDARD SPECIFICATIONS.
- (R-41) QUANTITY INCLUDES AN ESTIMATED 20 C.Y. TO BE USED AS DIRECTED BY THE ENGINEER.
- (R-44) PRICE BID TO INCLUDE COST OF 0 - 4" MOUNTABLE CURB HOODS, 0 - 6" MOUNTABLE CURB HOODS, 0 - 6" BARRIER CURB HOODS, 10 - 8" BARRIER CURB HOODS.
- (R-48) INCLUDES REMOVAL OF ALL EXISTING ROADWAY DRAINAGE STRUCTURES, HEADWALLS (UNLESS OTHERWISE SPECIFIED), INLETS, FENCES, AND OTHER STRUCTURES WITHIN THE RIGHT OF WAY.
- (R-49) TO BECOME THE PROPERTY OF AND BE DISPOSED OF BY THE CONTRACTOR IN A MANNER APPROVED BY THE ENGINEER.
- (R-50) MATERIALS REMOVED SHALL NOT BE MEASURED FOR PAYMENT UNDER SECTION 202.06 UNCLASSIFIED EXCAVATION.
- (R-54) CONCRETE LONGITUDINAL BARRIER TO INCLUDE POST HOLES FITTED WITH GALVANIZED STEEL SLEEVE FOR GLARE DEFLECTOR FENCE OR THREADED FLANGE.
- (1) INCLUDES 1,000 C.Y. FOR MISC EARTHWORK WHERE NO QUANTITY IS SHOWN ON P&P SHEETS.
- (2) ESTIMATED QUANTITY FOR TEMPORARY EROSION AND SEDIMENT CONTROL TO BE USED IN A MANNER APPROVED BY THE ENGINEER. PRICE BID TO INCLUDE THE COST OF NECESSARY MAINTENANCE, MAINTAINING IN AN UPRIGHT POSITION, REMOVAL OF CONTROL, AND SEDIMENT REMOVAL.
- (3) QUANTITY INCLUDES 500 L.F. FOR TEMPORARY EROSION CONTROL AS DETERMINED BY THE ENGINEER.
- (4) INCLUDES LITTER PICK UP BEFORE AND AFTER MOWING.
- (5) "STABILIZED SUBGRADE" SHALL INCLUDE THE COST OF THE CHEMICAL ADDITIVE TO ACHIEVE THE RATE SPECIFIED FOR THE APPROPRIATE SOIL CLASSIFICATION AS SPECIFIED IN THE MOST CURRENT ODOT MATERIALS DIVISION OHD L-50.
- (6) QUANTITY INCLUDES 250 TONS FOR TEMPORARY ACCESS AS DETERMINED BY THE ENGINEER.
- (7) ESTIMATED AT 0.075 S.Q. YD. PRIOR TO DILUTION.
- (8) QUANTITY SHOWN INCLUDES 0.36 C.Y. FOR HIGH MAST FOOTING.
- (9) THE MEDIAN ON MAIN STREET WEST OF WASHINGTON AVENUE SHALL HAVE A RED BRICK STAMPED CONCRETE FINISH IN ACCORDANCE WITH THE SPECIAL PROVISION FOR STAMPED CONCRETE FINISHES.
- (10) PRICE BID INCLUDES TRENCH EXCAVATION AND STANDARD BEDDING QUANTITIES SHOWN ON THE SUMMARY OF DRAINAGE STRUCTURES.

PAY QUANTITY NOTES CONT.:

- (11) PRICE BID INCLUDES SAW CUTTING AS NECESSARY TO REMOVE EXISTING PAVEMENT.
- (12) IN ADDITION TO THE RESPONSIBILITIES SHOWN IN THE SPECIFICATIONS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND/OR REESTABLISHING THE SURVEY CONTROL POINTS SHOWN ON THE PLANS, STAKING THE CENTERLINE OF CONSTRUCTION AND REESTABLISHING RIGHT-OF-WAY STAKES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND VERIFYING BENCHMARKS SHOWN ON THE PLANS AND FOR ESTABLISHING NEW BENCHMARKS AS NEEDED TO CONSTRUCT THE PROJECT.

MAIN ST. CUSTER COUNTY

PAY ITEM NOTES (ROADWAY)

JOB PIECE NO. 27911(06) SHEET NO. 9

27911(06) 0302				
PAY QUANTITIES				
TRAFFIC SIGNAL				
ITEM		DESCRIPTION	UNIT	QUANTITY
802(B)	8340	2" PVC SCH.40 PLASTIC CONDUIT BORED	L.F.	65.00
802(B)	8342	2" PVC SCH.40 PLASTIC CONDUIT TRENCHED	L.F.	209.00
802(B)	8344	3" PVC SCH.40 PLASTIC CONDUIT BORED	L.F.	357.00
802(B)	8346	3" PVC SCH.40 PLASTIC CONDUIT TRENCHED	L.F.	30.00
803(A)	8065	PULL BOX (SIZE I)	EA.	3.00
803(A)	8066	PULL BOX (SIZE II)	EA.	2.00
804(A)	2915	STRUCTURAL CONCRETE	C.Y.	21.00
804(B)	2916	REINFORCING STEEL	LB.	2,999.90
806(A)	8313	32"MH POLE 45'TS & 10'LMA (G.STL.)	EA.	1.00
806(A)	8314	32"MH POLE 50'TS & 10'LMA (G.STL.)	EA.	1.00
806(A)	8355	32"MH POLE,TWIN TS & 10'LMA (G.STL.)	EA.	1.00
806(A)	8351	32"MH POLE,25'TS & 10'LMA (G.STL.)	EA.	1.00
806(A)	8353	32"MH POLE,55'TS & 10'LMA (G.STL.)	EA.	2.00
806(B)	8890	6' MTG.HT.TS PED.POLE (G.STL.)	(SP-8) EA.	1.00
806(B)	8892	8' MTG.HT.TS PED.POLE (G.STL.)	(SP-8) EA.	7.00
809(A)	8090	ROADWAY LUMINAIRE	EA.	6.00
811	8044	1/C NO. 10 ELECTRICAL CONDUCTOR	L.F.	3,126.00
825	8550	TRAFFIC SIGNAL CONTROLLER ASSEMBLY	(SP-1)(SP-2)(SP-3)(SP-5)(SP-12)(TR-6) EA.	1.00
828	8132	(PL) DETECTION SYSTEM (VIDEO)	(SP-4) L.SUM	1.00
830	8000	PEDESTRIAN PUSH BUTTON	EA.	8.00
831	8231	1WAY 3SEC. ADJ. SIG. HD. S-6	(SP-6)(SP-7)(SP-10)(SP-11) EA.	12.00
831	8252	1WAY 3SEC. ADJ. SIG. HD. S-9	(SP-6)(SP-7)(SP-10)(SP-11) EA.	7.00
831	8262	1WAY 3SEC. ADJ. SIG. HD. S-10	(SP-6)(SP-7)(SP-10)(SP-11) EA.	2.00
831	8295	1WAY 2SEC. ADJ. PED. SIG. HD. S-20	EA.	8.00
833	3030	BACKPLATE	(SP-9) EA.	29.00
834(A)	8207	5/C TRAFFIC SIGNAL ELECTRICAL CABLE	L.F.	2,511.00
834(A)	8209	9/C TRAFFIC SIGNAL ELECTRICAL CABLE	L.F.	80.00
834(A)	8211	15/C TRAFFIC SIGNAL ELECTRICAL CABLE	L.F.	131.00
834(A)	8212	18/C TRAFFIC SIGNAL ELECTRICAL CABLE	L.F.	195.00
834(A)	8213	21/C TRAFFIC SIGNAL ELECTRICAL CABLE	L.F.	430.00
840(B)	8593	E.P.S. OPTICAL DETECTOR	EA.	7.00
840(C)	8594	E.P.S. OPTICAL DETECTOR CABLE	L.F.	923.00
850(C)	8118	MAST ARM MOUNTED SIGNS (ALUMINUM)	(TS-6) S.F.	65.00

27911(06) 0303				
PAY QUANTITIES				
TRAFFIC LIGHTING				
ITEM		DESCRIPTION	UNIT	QUANTITY
516(A)	6093	DRILLED SHAFTS 42" DIAMETER	L.F.	20.00
803(A)	8060	PULL BOX	EA.	1.00
804(B)	2916	REINFORCING STEEL	LB.	57.80
805(B)	8730	(PL) RESET OF HIGH MAST TOWER	EA.	1.00

27911(06) 0300				
PAY QUANTITIES				
TRAFFIC TEMPORARY				
ITEM		DESCRIPTION	UNIT	QUANTITY
823	8478	(SP) PORTABLE TRAFFIC SIGNAL SYSTEM	(TC-84) S.D.	280.00
857(A)	8839	CONSTRUCTION TRAFFIC STRIPE (PAINT) (4" WIDE)	(TC-13)(TC-17)(TC-20)(TC-75) L.F.	10,800.00
857(E)	8887	(PL) CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TAB) TYPE 2-1	(TC-21)(TC-61)(TC-70)(TC-73)(TC-75) EA.	5,400.00
857(F)	8006	PAVEMENT MARKING REMOVAL (TRAFFIC STRIPE)	(TC-22)(TC-70) L.F.	16,800.00
880(B)	8818	CONSTRUCTION SIGNS 0 TO 6.25 SF	(TC-26)(TC-28)(TC-33)(TC-84) S.D.	10,630.00
880(B)	8821	CONSTRUCTION SIGNS 6.26 SF TO 15.99 SF	(TC-26)(TC-29)(TC-33)(TC-84) S.D.	560.00
880(B)	8824	CONSTRUCTION SIGNS 16.0 TO 32.99 SF	(TC-26)(TC-30)(TC-33)(TC-84) S.D.	7,620.00
880(C)	8842	CONSTRUCTION BARRICADES (TYPE III)	(TC-26)(TC-33)(TC-84) S.D.	1,440.00
880(C)	8848	WING BARRICADES	(TC-26)(TC-33)(TC-84) S.D.	2,680.00
880(E)	8860	WARNING LIGHTS (TYPE A)	(TC-26)(TC-84) S.D.	2,880.00
880(F)	8878	DRUMS	(TC-26)(TC-33)(TC-84) S.D.	20,490.00
880(G)	8890	CHANNELIZER CONES	(TC-26)(TC-33)(TC-84) S.D.	1,200.00

27911(06) 0301				
PAY QUANTITIES				
TRAFFIC PERMANENT				
ITEM		DESCRIPTION	UNIT	QUANTITY
804(A)	2915	STRUCTURAL CONCRETE	C.Y.	7.60
804(B)	2916	REINFORCING STEEL	LB.	1,060.00
805(A)	8724	(PL) REMOVAL OF EXISTING SIGNS	(TS-41) EA.	18.00
805(D)	8756	(PL) REMOVE & RESET EXISTING SIGNS	(TS-39) EA.	24.00
850(A)	8110	SHEET ALUMINUM SIGNS	(TS-33) S.F.	180.87
850(B)	8112	EXTRUDED ALUMINUM PANEL SIGNS	S.F.	623.00
851(A)	3207	6"@15 GALV. STEEL WIDE FLANGE BEAM POST	L.F.	157.50
851(C)	8324	2" SQUARE TUBE POST	L.F.	299.00
853	9063	BARRIER DELINEATORS (TYPE 2, CODE 1)	EA.	8.00
853	9066	GUARDRAIL DELINEATORS (TYPE 1, CODE 1)	EA.	19.00
855(A)	8812	TRAFFIC STRIPE (PLASTIC) (4" WIDE)	(TS-19) L.F.	6,609.00
855(A)	8813	TRAFFIC STRIPE (PLASTIC) (6" WIDE)	(TS-20)(1) L.F.	5,474.00
855(A)	8814	TRAFFIC STRIPE (PLASTIC) (8" WIDE)	(TS-21) L.F.	1,684.00
855(A)	8818	TRAFFIC STRIPE (PLASTIC) (12" WIDE)	(TS-22) L.F.	102.00
855(A)	8825	TRAFFIC STRIPE (PLASTIC) (24" WIDE)	(TS-23) L.F.	858.00
855(B)	3300	TRAFFIC STRIPE (PLASTIC) (SYMBOLS, WORDS, ETC.)	EA.	6.00
855(B)	8818	TRAFFIC STRIPE (PLASTIC) (ARROWS)	EA.	31.00

MAIN ST. CUSTER COUNTY

PAY ITEMS (TRAFFIC)

JOB PIECE NO. 27911(06) SHEET NO. 10

**TRAFFIC OPERATIONS GENERAL
CONSTRUCTION NOTES:**

ANY SIGNS AND/OR DELINEATORS WHICH ARE TO BE REMOVED DURING THIS PROJECT WILL BE STORED IN A PROTECTED AREA DESIGNATED BY THE RESIDENT ENGINEER, UNTIL SUCH A TIME THAT THEY ARE TO BE RESET BY THE CONTRACTOR. COST OF THIS WORK TO BE INCLUDED IN OTHER ITEMS OF WORK.

EXISTING ROADWAY SHALL REMAIN OPEN DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER BARRICADES, LIGHTS, AND SIGNING WITHIN THE LIMITS OF CONSTRUCTION. ALL CONSTRUCTION SIGNING WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS. CONSTRUCTION TRAFFIC CONTROL WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION) AND COMPLIANT WITH APPLICABLE ODOT STANDARD DRAWINGS.

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING TRAFFIC ON CROSS STREETS. A MINIMUM OF ONE LANE IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES.

THE STRUCTURAL DESIGN OF ALL POLES, MAST ARMS, HIGH-MAST POLES, AND OTHER SUPPORTS FOR SIGNS, LUMINAIRES, AND SIGNALS, AS WELL AS THEIR CONNECTIONS, SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS. THE MANUFACTURER SHALL ENSURE THE FOLLOWING ARE APPLIED TO THE DESIGN:

THE MINIMUM DESIGN WIND SPEED AND DESIGN LIFE AS REQUIRED IN THE AASHTO SPECIFICATIONS;

THE CALCULATED STRESSES AND FORCES FROM THE DESIGN LOADINGS DO NOT EXCEED THOSE REQUIRED IN THE AASHTO SPECIFICATIONS;

A CATEGORY I FATIGUE IMPORTANCE FACTOR (IF) FOR ALL STRUCTURES; NO VIBRATORY MITIGATION SHALL BE ALLOWED. TRUCK-INDUCED GUSTS SHALL BE APPLIED TO ALL OVERHEAD TRAFFIC SIGNAL SUPPORTS;

ALL MEMBERS ARE AT LEAST THE MINIMUM THICKNESS AS REQUIRED IN THE AASHTO SPECIFICATIONS;

LUMINAIRE MAST ARMS SHALL BE DESIGNED TO SUPPORT AT LEAST A 50 LB. (22.7 KG) LUMINAIRE WITH AN EFFECTIVE PROJECTED AREA OF 2.5 FT² (0.23 M²);

THE ANCHOR BOLT DESIGN AND AMOUNT OF ANCHOR BOLTS TO BE USED SHALL BE AS REQUIRED IN THE AASHTO SPECIFICATIONS.

SIGNAL MAST ARMS AND POLES SHALL BE DESIGNED FOR SPECIFIC SIGNAL HEAD AND SIGN PLACEMENT.

UNLESS SITE SPECIFIC GEOTECHNICAL DATA IS AVAILABLE, FOUNDATIONS SHALL BE DESIGNED UTILIZING THESE PARAMETERS: SHEAR STRENGTH OF COHESIVE SOIL (C) OF 500 PSF, ANGLE OF INTERNAL FRICTION (ϕ) OF 22 DEGREES, AND EFFECTIVE UNIT WEIGHT OF SOIL (γ) OF 120 PCF. MINIMUM HAND HOLE SIZE OF 3-INCH WIDTH BY 5-INCH HEIGHT.

**TRAFFIC SIGNING GENERAL
CONSTRUCTION NOTES:**

REMOVED MATERIAL TO BECOME PROPERTY OF CONTRACTOR AND IT SHALL BE DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING THE EXISTING ROAD TO LOCAL AND THROUGH TRAFFIC. SEE ODOT STANDARDS AND DETAIL DRAWINGS FOR MAINTENANCE OF LOCAL AND THROUGH TRAFFIC.

ANY DAMAGE CAUSED BY THE CONTRACTOR TO ANY STRUCTURES, ROADWAY SURFACES, STRIPING, RAISED PAVEMENT MARKERS, GUARDRAIL, SLOPES, AND SIGNS SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.

THE STATIONS AND LOCATIONS OF THE SIGN PLACEMENT, AS SHOWN ON THE PLAN SHEETS, ARE APPROXIMATE. EXACT STATIONS AND LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR SO THAT THE SIGN IS INSTALLED IN ACCORDANCE WITH DEPARTMENT STANDARDS AND THE MUTCD IN ORDER TO PROVIDE OPTIMUM VISIBILITY TO THE ONCOMING/APPROACHING MOTORIST. IF A PROPOSED LOCATION CONFLICTS WITH OTHER SIGNS, UTILITIES, OR OTHER ROADWAY FEATURES, THE ENGINEER SHALL BE NOTIFIED.

THE COST OF REPLACEMENT OF MISSING OR DAMAGED EDGE STRIP ON EXISTING SIGNS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

ALL REGULATORY SIGNS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956--(LATEST REVISION) FOR TYPE III SHEETING.

ALL WARNING SIGNS SHALL HAVE FLUORESCENT YELLOW SHEETING. THE FLUORESCENT YELLOW SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956--(LATEST REVISION) REQUIREMENTS FOR TYPE VIII SHEETING.

ALL GREEN AND BLUE SIGNS ON CONVENTIONAL HIGHWAYS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956--(LATEST REVISION) FOR TYPE III SHEETING.

ALL PANEL AND OVERHEAD SIGNS SHALL HAVE TYPE III HIGH INTENSITY BACKGROUND WITH TYPE VIII LEGENDS AND BORDERS. THE TYPE III BACKGROUND AND THE TYPE VIII LEGENDS AND BORDERS SHALL MEET THE REQUIREMENTS OF ASTM D4956--(LATEST REVISION).

THE MANUFACTURER SHALL FURNISH A TYPE "A" CERTIFICATION IN ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS, LATEST EDITION, SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON THE MATERIAL SUBMITTED FOR APPROVAL.

ALL BROKEN CONCRETE, INCLUDING OLD SIGN FOOTINGS WITH STUBS, WASTE MATERIAL, AND DEBRIS, SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE LIMITS OF THE PROJECT AND DISPOSED OF IN AN AREA APPROVED BY THE ENGINEER. NO PAYMENT SHALL BE MADE FOR THE DISPOSAL OF THIS MATERIAL. ANY PIPE POST OR WIDE-FLANGE POST ABOVE THE OLD SIGN FOOTINGS SHALL BE CUT AND HANDLED AS PROPERTY OF THE STATE AND SHALL BE NEATLY STACKED ON THE JOB SITE AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL FROM THE JOB SITE.

ALL ANCHOR BOLTS SHALL BE GRADE A-36 STEEL.

POST LENGTHS SHOWN ON THE SIGN SUMMARY SHEETS ARE APPROXIMATE. EXACT LENGTHS SHALL BE DETERMINED BY FIELD SURVEY BY THE CONTRACTOR.

ALL REMOVED SIGNS, SIGN POSTS, BOLTS, MISCELLANEOUS HARDWARE, AND DELINEATORS SHALL REMAIN THE PROPERTY OF THE STATE. THE CONTRACTOR SHALL NEATLY STACK SUCH REMOVED MATERIAL AT A LOCATION ON THE JOB SITE AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL FROM THE JOB SITE.

TRAFFIC SIGNING PAY QUANTITY NOTES:

(TS-6) SHOP DRAWINGS FOR ATTACHING SIGNS TO LIGHT AND/OR SIGNAL POLES AND MAST ARMS SHALL BE SUBMITTED TO THE TRAFFIC ENGINEER FOR APPROVAL BEFORE FABRICATION. NO HOLES SHALL BE PERMITTED IN ANY LIGHT AND/OR SIGNAL POLE OR MAST ARM. THE PRICE BID SHALL INCLUDE ALL MATERIALS, LABOR, HARDWARE AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS DESCRIBED.

(TS-19) QUANTITY SHOWN INCLUDES 2,335 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 4,724 L.F. TRAFFIC STRIPE(PLASTIC)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF FOUR INCH (4") WIDE TRAFFIC STRIPE.

(TS-20) QUANTITY SHOWN INCLUDES 2,613 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (6") WIDE TRAFFIC STRIPE.

(TS-21) QUANTITY SHOWN INCLUDES 1,091 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 593 L.F. TRAFFIC STRIPE(PLASTIC)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF EIGHT INCH (8") WIDE TRAFFIC STRIPE.

(TS-22) QUANTITY SHOWN INCLUDES 102 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 0 L.F. TRAFFIC STRIPE (PLASTIC)(YELLOW) WILL BE MEASURED BY THE LINEAR FOOT OF TWELVE INCH (12") WIDE TRAFFIC STRIPE.

(TS-23) QUANTITY SHOWN INCLUDES 858 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND WILL BE MEASURED BY THE LINEAR FOOT OF TWENTY-FOUR INCH (24") WIDE TRAFFIC STRIPE.

(TS-33) INCLUDED IN THIS PAY ITEM IS ALL HARDWARE ASSOCIATED WITH PROPERLY ANCHORING AND MOUNTING THE HIGHWAY SIGN IN ACCORDANCE WITH ODOT PLANS AND STANDARD DRAWINGS SSA1-1 AND SSP1-1--(LATEST REVISION).

(TS-39) OVERHEAD SIGN STRUCTURES AND SIGNS THAT ARE TO BE REMOVED, RESET, AND/OR RELOCATED SHALL BE CAREFULLY REMOVED BY THE CONTRACTOR AND STORED AT A SITE SELECTED BY THE ENGINEER. ANY DAMAGE TO THE STRUCTURES OR SIGNS DURING THE REMOVAL, TRANSPORTATION, STORAGE, RESETTING, AND/OR RELOCATION OF THE STRUCTURE OR SIGN SHALL BE REPAIRED BY, AND AT THE EXPENSE OF THE CONTRACTOR.

(TS-41) "REMOVAL OF EXISTING SIGNS" SHALL INCLUDE THE REMOVAL OF A COMPLETE SIGN ASSEMBLY WHICH MAY INCLUDE MULTIPLE SIGNS, POSTS, FOOTINGS, AND ANY FOOTINGS ADJACENT TO THE SIGN ASSEMBLY. WHEN APPROVED BY THE ENGINEER, FOOTINGS MAY BE OBLITERATED TO A POINT BELOW GROUND LEVEL IN LIEU OF BEING COMPLETELY REMOVED. SEE GENERAL CONSTRUCTION NOTES FOR DISPOSAL OF OLD CONCRETE FOOTING MATERIAL.

(1) QUANTITY SHOWN INCLUDES 2,861 L.F. TRAFFIC STRIPE (PLASTIC)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (6") WIDE TRAFFIC STRIPE.

MAIN ST. CUSTER COUNTY

**PAY ITEM NOTES (TRAFFIC)
1 OF 2**

JOB PIECE NO. 27911(06) SHEET NO. 11

TRAFFIC SIGNAL PAY QUANTITY NOTES

(TR-6) THE CONTROLLERS TO BE FURNISHED ON THIS PROJECT SHALL BE 8-PHASE VEHICLE ACTUATED SOLID STATE DIGITAL TRAFFIC SIGNAL CONTROLLERS. A MINIMUM OF SIXTEEN (16) LOAD SWITCH RECEPTACLES SHALL BE FURNISHED AND WIRED TO THE MOUNTING FRAMES. ALL WIRING FROM THE FIELD TERMINALS SHALL BE WIRED TO THE MOUNTING FRAME FOR AN 8-PHASE OPERATION. NO CABINET OR CONTROLLER WIRING SHALL BE REQUIRED EXCEPT FOR ADDITIONAL DETECTOR CONNECTING CABLES WHEN CONTROLLER IS EXPANDED FOR AN 8-PHASE OPERATION. THE CONTROLLER SHALL BE CAPABLE OF PERFORMING AS SHOWN ON THE PHASE AND SEQUENCE DIAGRAM.

SPECIAL PAY QUANTITY NOTES

- (SP-1) THIS ITEM IS FOR INSTALLING PEEK 3000E TS/2 TYPE 1 TRAFFIC SIGNAL CONTROLLERS IN THE CABINET. WORK SHALL INCLUDE THE INITIAL PROGRAMMING OF EACH OF THE LOCAL CONTROLLERS FOR THREE (3) DAILY TIMING PLANS TO BE SUPPLIED BY THE ENGINEER. THE PROGRAMMING OF THE MASTER CONTROLLER AT WASHINGTON AND MAIN SHALL COORDINATE THE SIGNAL SYSTEM FOR TWO (2) PERIODS OF THE DAY.
- (SP-2) THE CABINET SHALL BE P44 NEMA TS/2 TYPE 1 OR AN APPROVED EQUAL. A MINIMUM OF SIXTEEN (16) LOAD SWITCH RECEPTACLES SHALL BE FURNISHED AND WIRED TO THE MOUNTING FRAMES. ALL WIRING FROM THE FIELD TERMINALS SHALL BE CONNECTED TO THE MOUNTING FRAME FOR AN 8-PHASE OPERATION. ALL CORRESPONDING RECEPTACLE WIRING IN THE CABINET AND FIELD WIRING SHALL BE INSTALLED FOR THE CONTROLLER AS REQUIRED. NEW CONFLICT MONITORS SHALL BE PROVIDED THAT ARE COMPATIBLE WITH THE SIGNAL CONTROLLER AND OPERATION OF THE INTERSECTION CONTROL. CONFLICT MONITORS SHALL BE AN EDI SSM12LE OR APPROVED EQUAL.
- (SP-3) INCLUDED IN THIS ITEM IS A TRAFFIC SIGNAL BATTERY BACK-UP SYSTEM IN ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS SECTION 825.02.
- (SP-4) THIS PROJECT INVOLVES THE INSTALLATION OF TWO (2) VIDEO DETECTION SYSTEMS. THE CONTRACTOR SHALL FURNISH AND INSTALL THE FOLLOWING:
- A. TWO (2) OF THE FOLLOWING: A PEEK VIDEO TRAK, 8-CHANNEL PROCESSOR (OR APPROVED EQUAL) VEHICLE DETECTOR SYSTEM UNITS INCLUDING A LAPTOP COMPUTER. ALL NECESSARY CABLES, HARNESSSES, MATERIALS, FITTINGS, AND MISCELLANEOUS COMPONENTS NECESSARY TO PROVIDE A COMPLETE AND OPERATIONAL SYSTEM AT TWO (2) INTERSECTIONS. MINIMUM SPECIFICATIONS FOR THE COMPUTER SYSTEM: PENTIUM IV, 850 MHZ PROCESSOR, 5624 MB RAM, 200 GB HARD DRIVE, USB PORTS, AND A 48X CDR-RW.
- B. SEVEN (7) CAMERAS WITH ZOOM LENS CAPABILITY.
- (SP-5) WORK SHALL INCLUDE PROGRAMMING THE DETECTION ZONES SHOWN ON THE PLANS.
- (SP-6) RED, YELLOW, AND GREEN LIGHT EMITTING DIODE (LED) TRAFFIC SIGNAL HEADS, MODULES, LENSES, AND ALL ASSOCIATED MATERIAL AND EQUIPMENT FURNISHED AND INSTALLED ON THIS PROJECT SHALL CONFORM TO ITE VEHICLE TRAFFIC CONTROL SIGNAL HEAD (VTC SH) STANDARDS IN EFFECT AT THE TIME THAT THE ORDER IS PLACED. LED HEADS SHALL BE CAPABLE OF OPERATING WITHOUT A REFLECTOR.
- (SP-7) CIRCULAR AND ARROW LED SIGNAL MODULES SHALL HAVE A UNIFORM APPEARANCE.
- (SP-8) THIS ITEM IS FOR INSTALLING PEDESTRIAN PUSH BUTTONS POLES. SEE STANDARD PMAP1-1-(LATEST REVISION)
- (SP-9) INCLUDED IN THIS PAY ITEM IS THE COST OF APPLYING 2" YELLOW RETROREFLECTIVE TAPE (TYPE VIII) ONTO BACKPLATES. (SEE STANDARD SA1-1-02)
- (SP-10) TRAFFIC SIGNAL HEAD FACES ARE TO BE COMPLETELY COVERED FROM THE TIME THEY ARE INSTALLED UNTIL PLACED IN FLASHING OR FULL OPERATION. COVERING THE LENSES ONLY IS NOT ACCEPTABLE.
- (SP-11) PRICE BID FOR THIS PAY ITEM TO INCLUDE DEFROST DEVICE FOR FROST, SNOW, ICE, OR LENS FOG REMOVAL.
- (SP-12) CABINET SHALL BE MANUFACTURED WITH TWO (2) DOORS. SEE STANDARD ON SHEET CC1-1-00.

TRAFFIC CONSTRUCTION PAY QUANTITY NOTES:

- (TC-13) A PART, OR ALL, OF THIS ITEM IS INTENDED FOR REPLACEMENT OF REMOVED EXISTING CONFLICTING STRIPING.
- (TC-17) INCLUDES AN ESTIMATED 5,400 L.F. (PAINT) (4" WIDE) WHITE 5,400 L.F. (PAINT)(4" WIDE) YELLOW STRIPE.
- (TC-20) ALL STRIPING TO BE PLACED ON TEMPORARY SURFACES OR ON SURFACES SCHEDULED TO BE REMOVED SHALL BE DONE WITH PAINT UNLESS OTHERWISE NOTED ON THE PLANS OR STANDARD DRAWINGS. TEMPORARY PAVEMENT MARKINGS PLACED ON FINISHED PAVEMENT OR EXISTING PAVEMENT TO REMAIN IN PLACE SHALL USE ONE OF THE FOLLOWING METHODS:
- REMOVABLE PAVEMENT MARKING TAPE
 - CLASS A PAVEMENT MARKERS
- (TC-21) INCLUDED IN THE COST OF THIS ITEM SHALL BE INSTALLATION, MAINTENANCE, AND REMOVAL. THIS ITEM SHALL BE BID ACCORDINGLY.
- (TC-22) AMOUNT SHOWN IS AN APPROXIMATION AND THE ACTUAL AMOUNT OF REMOVAL, IF NECESSARY, SHALL BE DETERMINED BY THE ENGINEER. PRICE BID FOR PAVEMENT MARKING REMOVAL SHALL INCLUDE THE COST OF REMOVING STRIPE, ARROWS, WORDS AND SYMBOLS, AS SHOWN IN THE PLANS. THESE ITEMS MAY CONSIST OF PLASTIC, PAINT OR NON-REMOVABLE MARKING TAPE.
- (TC-26) ALL CONSTRUCTION TRAFFIC CONTROL WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS, AND INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (CURRENT EDITION), AND COMPLIANT WITH APPLICABLE ODOT STANDARD DRAWINGS. PRICE BID FOR THIS ITEM SHALL BE PAYMENT IN FULL FOR THE INSTALLATION, MAINTENANCE AND SUBSEQUENT REMOVAL OF ALL NECESSARY CONSTRUCTION TRAFFIC CONTROL DEVICES REQUIRED FOR COMPLETION OF THE PROJECT.
- ALL SIGNS AND BARRICADES WHICH ARE SHOWN WITH TYPE "A" LIGHTS IN THE STANDARD DRAWINGS SHALL HAVE THE CORRESPONDING LIGHT ATTACHED DURING NON-DAYLIGHT HOURS.
- (TC-28) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 0.00 S.F. AND 6.25 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-29) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 6.26 S.F. AND 15.99 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-30) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 16.00 S.F. AND 32.99 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-33) ALL CONSTRUCTION WORK ZONE SIGNS SHALL HAVE FLUORESCENT SHEETING. THE FLUORESCENT SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956 (LATEST REVISION)
- IN THE MANUFACTURER SHALL FURNISH A TYPE "D" CERTIFICATION ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS (CURRENT EDITION) SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON MATERIAL SUBMITTED FOR APPROVAL.
- (TC-61) ANY DAMAGE TO A FINISHED OR EXISTING SURFACE RESULTING FROM THE CONTRACTORS NEGLIGENCE IN THE REMOVAL OF CONSTRUCTION ZONE PAVEMENT MARKERS OR CHANNELIZING DEVICES AND THE BITUMINOUS ADHESIVE USED IN THEIR INSTALLATION, SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE ENGINEER.
- (TC-70) THIS ITEM IS AN ESTIMATED QUANTITY TO BE USED AS DEEMED NECESSARY BY ENGINEER.

TRAFFIC CONSTRUCTION PAY QUANTITY NOTES CONT.:

- (TC-73) QUANTITY SHOWN INCLUDES 2,700 EA. (WHITE) AND 2,700 EA. (YELLOW) CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TABS). THESE CONSTRUCTION ZONE PAVEMENT MARKERS SHALL BE EITHER "DAVIDSON PLASTICS: MODEL TOM," OR AN APPROVED EQUAL. PRICE BID FOR THIS ITEM SHALL INCLUDE THE INITIAL PLACEMENT, SUBSEQUENT REPLACEMENT, AND REMOVAL. THE CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TABS) SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND AS SHOWN ON STANDARD DRAWING TCS21-1-(LATEST REVISION).
- (TC-75) TEMPORARY PAVEMENT MARKINGS SHALL BE IN PLACE THE SAME DAY THAT EXISTING PAVEMENT MARKINGS ARE REMOVED FROM ANY ROADWAY OPEN TO TRAFFIC. ALSO, ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO THE INSTALLATION OF FINAL STRIPING.
- (TC-84) 280 CONSTRUCTION CALENDAR DAYS WERE USED TO COMPUTE THE SIGN DAY PAY ITEMS. THE AMOUNT OF CALENDAR DAYS USED TO COMPUTE THE SIGN DAY PAY ITEMS IS AN ESTIMATED QUANTITY ONLY, BASED ON THE CURRENT ODOT STANDARDS AND SUGGESTED CONSTRUCTION SEQUENCE FOR THIS PROJECT. THESE ESTIMATED SIGN DAY QUANTITIES MAY CHANGE AS THE PROJECT'S CONSTRUCTION TRAFFIC CONTROL IS MODIFIED DURING CONSTRUCTION.
- NOTES: THE CONTRACTOR MAY SUBMIT A WRITTEN ALTERNATE SEQUENCE OF CONSTRUCTION OPERATION TO THE ENGINEER FOR APPROVAL. ALL CONSTRUCTION SIGNING AND STRIPING IN ADVANCE OF AND THROUGHOUT THE PROJECT LIMITS WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH THE MUTCD (LATEST EDITION), AND APPLICABLE ODOT STANDARDS.

MAIN ST. CUSTER COUNTY

PAY ITEM NOTES (TRAFFIC) 2 OF 2

JOB PIECE NO. 27911(06) SHEET NO. 12

SUMMARY OF DRAINAGE STRUCTURES

STR. NO.	ALIGNMENT	STATION	DESCRIPTION	DESIGN STANDARD(S)	AVERAGE FILL HEIGHT		TRENCH EXCAVATION 613(V)*	STD. BEDDING MATERIAL, CLASS B 613 *	UNCLASSIFIED EXCAVATION 202(A)	STRUCTURAL EXCAVATION UNCLASSIFIED 501(A)	CLASS AA CONCRETE 509(A)	REINFORCING STEEL 511(A)	INLET CI DES. 2 (STD) 611(G)	INLET (SMD-TYPE 2) 611(G)	REPLACEMENT OF INLET FRM & GRT (SSIF-FRM, CIG-GRT-VG-F) 611(I)	REPLACEMENT OF CAST IRON HOOD 611(M)	MANHOLES ADJUST TO GRADE 612(A)	INLET ADJUST TO GRADE 612(C)	18" R.C. PIPE CLASS III 613(A)	24" R.C. PIPE CLASS III 613(A)	18" PREFAB. CULVERT END SECTION, ROUND 613(L)	
					L.F.	V.F.																C.Y.
1	MAIN STREET	4379+00	REMOVE EXIST. GRATE & PLUG																			
2	MAIN STREET	4379+22	ADJUST EXIST. MANHOLE TO GRADE	MJB-3													1					
3	MAIN STREET	4380+05	ADJUST EXIST. MANHOLE TO GRADE	MJB-3													1					
4	MAIN STREET	4380+75	CONST. SMD TYPE 2 RT. W/ 24"x68' LG RCP STUBBED TO STR. NO. 3	SMD-3, SPI-4, SPB-1, FHTCP-3		8		81	0						1						68	
5	MAIN STREET	4383+16	CONST. INLET CI DES. 2 (STD) LT. W/ 18"x64' LG RCP STUBBED TO STR. NO. 6	CI-1, SSIF-4, CIG-3, SPI-4, SPB-1, FHTCP-3		1	1	15	18				1								64	
6	MAIN STREET	4383+16	CONST. INLET CI DES. 2 (STD) RT. W/ 18"x88' LG RCP STUBBED OUT W/ PCES	CI-1, SSIF-4, CIG-3, SPI-4, SPB-1, FHTCP-3		1	3	31	24				1								88	1
7	RAMP A	4411+34	EXTEND EXIST. 8'x4' RCB 200 L.F. W/ 4' TEAR BACK	SBI-4		8			1,175	85	214	31,990										
8	MAIN STREET	4384+47	CONST. SMD TYPE 2 RT. W/ 18"x28' LG RCP STUBBED TO STR. NO. 7	SMD-3, SPI-4, SPB-1, FHTCP-3		9		33	8					1							28	
9	MAIN STREET	4386+88	CONST. INLET CI DES. 2 (STD) LT. W/ 18"x30' LG RCP STUBBED TO EXIST. STR.	CI-1, SSIF-4, CIG-3, SPI-4, SPB-1, FHTCP-3		3	3	14	8				1								30	
10	MAIN STREET	4389+06	CONST. INLET CI DES. 2 (STD) LT. W/ 18"x30' LG RCP STUBBED TO EXIST. STR.	CI-1, SSIF-4, CIG-3, SPI-4, SPB-1, FHTCP-3		3		14	8				1								30	
11	MAIN STREET	4389+98	CONST. INLET CI DES. 2 (STD) LT. W/ 18"x30' LG RCP STUBBED TO EXIST. STR.	CI-1, SSIF-4, CIG-3, SPI-4, SPB-1, FHTCP-3		2		11	8				1								30	
12	WASHINGTON AVE.	23+07	ADJUST EXIST. INLET TO GRADE															1				
13	WASHINGTON AVE.	25+03	REMOVE AND REPLACE EXIST. INLET FRAME, GRATES & HOODS	CI-1, SSIF-4, CIG-3											6	10						
14	WASHINGTON AVE.	27+31	CONST. SMD TYPE 2 RT. W/ 18"x4' LG RCP STUBBED TO EXIST. STR.	SMD-3, SPI-4, SPB-1, FHTCP-3					1					1							4	
15	WASHINGTON AVE.	28+34	CONST. SMD TYPE 2 RT. W/ 18"x24' LG RCP STUBBED TO EXIST. STR.	SMD-3, SPI-4, SPB-1, FHTCP-3					7					1							24	
16	RAMP A	4407+16	CONST. SMD TYPE 2 LT. W/ 24"x106' LG RCP STUBBED TO EXIST. STR.	SMD-3, SPI-4, SPB-1, FHTCP-3		15		126	0												106	
TOTALS:						7	325	82	1,175	85	214	31,990	5	4	6	10	2	1	298	174	1	

* QUANTITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. COST OF THESE ITEMS ARE INCLUDED IN OTHER ITEMS OF BID.

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SUMMARY OF DRIVEWAYS

ALIGNMENT	STATION	OFFSET	TYPE	RADIUS	LENGTH	GRADED	SURFACE	6' CONCRETE
				FT.		WIDTH	WIDTH	DRIVEWAY
					FT.	FT.	FT.	S.Y.
MAIN STREET	4375+44	.T.	COMMERCIAL	15/15	16.00	32.00	24.00	54.00
MAIN STREET	4375+34	.T.	COMMERCIAL	15/3	11.00	56.00	50.00	71.00
MAIN STREET	4377+04	RT.	COMMERCIAL	10/6	29.00	31.00	25.00	88.00
MAIN STREET	4377+54	RT.	COMMERCIAL	6/1.5	37.00	56.00	50.00	210.00
MAIN STREET	4387+81	.T.	COMMERCIAL	25/25	43.00	41.00	34.00	196.00
MAIN STREET	4389+53	.T.	COMMERCIAL	25/25	30.00	32.00	34.00	142.00
WASHINGTON AVE.	22+84	RT.	COMMERCIAL	20/15	47.00	28.00	22.00	135.00
WASHINGTON AVE.	22+89	.T.	COMMERCIAL	15/0	21.00	68.00	62.00	167.00
WASHINGTON AVE.	23+41	.T.	COMMERCIAL	0/3	9.00	35.00	29.00	30.00
WASHINGTON AVE.	23+69	.T.	COMMERCIAL	3/3	20.00	26.00	22.00	55.00
WASHINGTON AVE.	23+69	RT.	COMMERCIAL	15/15	16.00	26.00	20.00	88.00
WASHINGTON AVE.	24+05	.T.	COMMERCIAL	3/1.5	20.00	41.00	35.00	87.00
WASHINGTON AVE.	24+60	.T.	COMMERCIAL	10/15	20.00	46.00	40.00	98.00
FRONTAGE ROAD	4384+38	.T.	COMMERCIAL	15/15	20.00	37.00	31.00	92.00
TOTALS:								1513.00

SUMMARY OF LONGITUDINAL BARRIER

ALIGNMENT	STATION TO STATION	CONCRETE	CONCRETE	DELINEATORS (TYPE 2, CODE 1)
		LONGITUDINAL BARRIER, DESIGN 1	LONGITUDINAL BARRIER END SECTIONS	
		627(A)	627(B)	853
		L.F.	EA.	EA.
RAMP A	4406+50 TO 4410+00	350.00	2.00	8.00
MAIN STREET	4384+50 TO 4386+50	200.00	2.00	
TOTALS:		550.00	4.00	8.00

SUMMARY OF SIDEWALKS

ALIGNMENT	STATION TO STATION	OFFSET	4" CONCRETE
			SIDEWALK 610(A) S.Y.
MAIN ST.	4375+19.13 TO 4375+32.72	LT.	7.50
MAIN ST.	4375+59.78 TO 4376+06.05	LT.	26.00
MAIN ST.	4376+58.43 TO 4376+65.98	LT.	4.00
MAIN ST.	4377+19.62 TO 4377+32.08	RT.	7.00
MAIN ST. & WASHINGTON AVE.	4377+79.78 TO 25+00.39	RT.	1164.00
MAIN ST.	4379+00.21	LT.	52.00
MAIN ST. & WASHINGTON AVE.	4380+41.98 TO 24+06.33	RT.	1009.00
MAIN ST.	4380+41.66 TO 4380+41.49	RT.	78.00
MAIN ST. & WASHINGTON AVE.	27+10.26 TO 4387+49.78	LT.	3811.00
MAIN ST.	4388+11.78 TO 4389+21.54	LT.	556.00
MAIN ST.	4389+83.51 TO 4390+86.99	LT.	300.00
WASHINGTON AVE.	22+87.78 TO 23+73.34	RT.	386.00
WASHINGTON AVE.	24+31.88 TO 24+59.35	LT.	178.00
WASHINGTON AVE.	27+10.47	LT.	61.00
TOTAL:			7639.50

SUMMARY OF SURFACING

ALIGNMENT	STABILIZED	SEPARATOR	TACK COAT	PRIME COAT	SUPERPAVE,	SUPERPAVE,	DOWEL JOINTED	P.C. CONCRETE	CONC. CURB	2'-8" COMB.	8" CONCRETE	(PL) STAMPED
	SUBGRADE	FABRIC			TYPE S3 (PG 64-22 OK)	TYPE S4 (PG 76-28 OK)	P.C. CONCRETE PAVEMENT (PLACEMENT)	FOR PAVEMENT	(8" BARRIER- INTEGRAL)	CURB & GUTTER (8" BARRIER)	DIVIDING STRIP	CONCRETE FINISH
	307(K)	325	407(B)	408	411(B)	411(C)	414(B)	414(G)	609(A)	609(B)	610(C)	610(J)
	S.Y.	S.Y.	GAL.	GAL.	TONS	TONS	S.Y.	C.Y.	L.F.	L.F.	S.Y.	S.Y.
MAIN STREET	13,880.38	13,880.38	254.94	4,858.13	2,340.29	380.73	12,956.71	3,598.94	3,840.00	0.00	190.45	190.45
WASHINGTON AVE.	3,981.73	3,981.73	0.00	1,288.80	620.63	0.00	3,551.84	986.58	750.00	0.00	24.22	24.22
FRONTAGE ROAD	138.74	138.74	114.09	48.56	23.46	170.38	123.52	34.31	72.00	656.00	0.00	0.00
RAMP A	6,527.03	6,030.19	0.00	2,284.46	1,019.11	0.00	5,370.55	1,491.76	0.00	0.00	0.00	0.00
RAMP C	2,619.30	2,619.30	22.00	916.76	443.13	32.85	2,055.16	570.86	0.00	0.00	32.00	0.00
TOTALS:	27,147.18	26,650.34	391.03	9,396.71	4,446.62	583.96	24,057.78	6,682.45	4,662.00	656.00	246.67	214.67

SUMMARY OF WHEELCHAIR RAMPS

ALIGNMENT	STATION	OFFSET	TYPE	FILL
				WARNING DEVICE 610(L) S.F.
		L.F.		
MAIN STREET	4375+32.72	40.09 LT.	B	7.00
MAIN STREET	4375+59.78	40.79 RT.	B	7.00
MAIN STREET	4376+06.05	41.01 LT.	U	7.00
MAIN STREET	4376+58.43	40.81 RT.	C	10.00
MAIN STREET	4376+65.98	40.81 LT.	C	10.00
MAIN STREET	4377+19.62	68.87 RT.	C	10.00
MAIN STREET	4377+32.08	70.43 RT.	C	10.00
MAIN STREET	4377+79.78	76.40 RT.	U	7.00
MAIN STREET	4379+00.21	35.77 RT.	B	10.00
MAIN STREET	4379+00.69	1.97 RT.	C	20.00
MAIN STREET	4379+01.39	69.69 RT.	B	10.00
MAIN STREET	4380+40.98	36.33 LT.	A	10.00
MAIN STREET	4380+41.49	11.21 RT.	C	10.00
MAIN STREET	4380+41.66	26.75 RT.	C	10.00
MAIN STREET	4380+41.98	58.52 RT.	B	10.00
MAIN STREET	4387+49.78	28.60 LT.	B	
MAIN STREET	4387+49.78	28.60 RT.	B	
MAIN STREET	4388+11.78	28.60 LT.	B	
MAIN STREET	4389+83.51	28.60 RT.	B	
WASHINGTON AVE.	22+97.78	31.25 RT.	B	
WASHINGTON AVE.	23+73.34	41.48 RT.	B	
WASHINGTON AVE.	24+06.33	43.18 RT.	B	
WASHINGTON AVE.	25+00.39	49.31 LT.	U	
WASHINGTON AVE.	25+55.14	38.48 RT.	B	10.00
WASHINGTON AVE.	22+55.81	48.11 RT.	U	10.00
WASHINGTON AVE.	25+00.00	49.91 LT.	B	
WASHINGTON AVE.	27+10.00	39.83 LT.	B	10.00
WASHINGTON AVE.	27+10.00	50.50 RT.	B	10.00
TOTAL:				188.00

SUMMARY OF PERMANENT EROSION CONTROL

DESCRIPTION	SOLID SLAB SODDING	VEGETATIVE MULCHING
	230(A)	233(A)
	S.Y.	AC.
MAIN ST	4,614.00	0.95
WASHINGTON AVE	213.00	0.05
FRONTAGE RD	201.00	0.04
RAMPS	18,299.00	3.78
TOTALS:	23,327.00	4.82

MAIN ST. CUSTER COUNTY

**SUMMARY SHEET
1 OF 4**

JOB PIECE NO. 27911(06) SHEET NO. 14

SUMMARY OF SIGNS

ALIGNMENT	APPROX. STATION AND LOCATION	SIGN NO.	TYPE	DESCRIPTION	SIGN AREA		BASE LENGTHS				STRUCTURAL CONCRETE	REINFORCING STEEL	(PL) REMOVE OF EXISTING SIGNS	(P.) REMOVE & RESET EXISTING SIGNS
					SHEET ALUMINUM SIGNS	EXTRUDED ALUMINUM PANEL SIGNS	2' SQUARE TUBE POST 851(C)	POST SPACE	6" GALV. STEEL W/ICE FLANGE BEAM POST					
					850(A) S.F.	850(B) S.F.	A	B	A	B				
MAIN STREET	4374+41, 43' RT.	1	M4-6	END		2.00								
		2	R3-9B	CENTER LANE TURN LEFT ONLY		6.00			3.00					
	4375+42, 44' RT.	3		SPECIAL SIGN NO. 2		15.63			3.00	3.00	3.75			
	4375+53, 2' RT.			DIVIDED HIGHWAY										1.00
	4376+20, 3' RT.			LEFT ONLY, LEFT/AHEAD										1.00
	4376+81, 1' RT.			ROUTE US 66										1.00
MAIN STREET	4377+91, 70' RT.	4	R3-S(R)	RIGHT TURN ONLY		7.50								
	4379+95, 25' RT.			KEEP RIGHT										1.00
	4379+99, 16' RT.			ONE WAY										1.00
	4380+11, 51' LT.			ONE WAY										1.00
	4380+30, 6' RT.			DO NOT ENTER										1.00
	4380+55, 42' LT.			DO NOT ENTER										1.00
	4380+84, 119' RT.			DO NOT ENTER										1.00
	4381+35, 8' RT.			WRONG WAY										1.00
	4381+61, 35' RT.	5	R3-6(R)	LANE-RIGHT		7.50			3.00					
	4382+89, 2' LT.			STORM READY COMMUNITY										1.00
	4383+11, 74' RT.			WRONG WAY										1.00
	4383+40, 24' RT.			SWOSU										1.00
	4383+59, 39' LT.			FOOD										1.00
	4383+97, 57' RT.			SPEED LIMIT										1.00
	4384+45, 84' RT.			STOP AHEAD										1.00
MAIN STREET	4384+50, 43' RT.	6	IM3-2	CARDINAL DIRECTION MARKER (EAST)		2.00								
		7	IM1-1(2)	INTERSTATE 40		4.00			3.00					
		8	IM6-1(R)	RIGHT DIRECTIONAL ARROW		2.19								
	4384-96, 96' LT.			LEFT ONLY, RIGHT ONLY										1.00
	4385+27, 36' LT.	9	W1-7	DOUBLE ARROW		8.00			3.00	3.00	2.40			
	4385+76, 37' LT.	10	IM3-2	CARDINAL DIRECTION MARKER (EAST)		2.00								
MAIN STREET	4387-93, 58' LT.	11	IM1-1(2)	INTERSTATE 40		4.00			3.00					
		12	IM6-1(L)	LEFT DIRECTIONAL ARROW		2.19								
	4388+41, 53' LT.			LEFT ONLY, RIGHT ONLY										1.00
	4388+41, 53' LT.			RIGHT CURVE, ADVISORY SPEED										1.00
	4389+32, 56' LT.			STOP										1.00
MAIN STREET	4390+64, 33' LT.	13	IM3-2	CARDINAL DIRECTION MARKER (EAST)		2.00								
		14	IM1-1(2)	INTERSTATE 40		4.00			3.00					
		15	IM5-1(L)	LEFT ADVANCE TURN ARROW		2.19								

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SUMMARY OF SIGNS (CONT.)

ALIGNMENT	APPROX. STATION AND LOCATION	SIGN NO.	TYPE	DESCRIPTION	SIGN AREA		BASE LENGTHS				STRUCTURAL CONCRETE	REINFORCING STEEL	(PL) REMOVAL OF EXISTING SIGNS	(PL) REMOVE & RESET EXISTING SIGNS				
					SHEET ALUMINUM SIGNS	EXTRUDED ALUMINUM PANEL SIGNS	2" SQUARE TUBE POST 851(C)		6" GALV. STEEL WIDE FLANGE BEAM POST						804(A)	804(B)	805(A)	805(D)
					850(A)	850(B)	A	B	A	B					C.Y.	LB.	EA.	EA.
					S.F.	S.F.	L.F.	FT.	L.F.									
				RAILROAD CROSSING										1.00				
	23+00, 25' RT.			STOP LIGHT AHEAD										1.00				
	23+13, 42' LT.			STOP										1.00				
	24+16, 29' RT.			AHEAD/RIGHT									1.00					
	24+39, 32' LT.			RAILROAD CROSSING										1.00				
	24+44, 34' LT.			MERGE LEFT										1.00				
	24+59, 36' RT.			RIGHT TO I-40 EAST										1.00				
	24+60, 32' RT.			ADVANCE LEFT TO I-40 WEST (BUSINESS LOOP)										1.00				
WASHINGTON AVE.	24+61, 54' RT.	22	R3-7(R)	RIGHT LANE MUST TURN RIGHT	6.25		13.00											
	25+15, 38' LT.	24	W4-2(R)	RIGHT LANE ENDS	9.00		13.00											
	25+34, 39' LT.			ONE WAY									1.00	0.00				
		16	IM3-2	CARDINAL DIRECTION MARKER (EAST)	2.00													
	25+38, 65' RT.	17	IM1-1(2)	INTERSTATE 40	4.00		13.00											
		18	IM6-1(R)	RIGHT DIRECTIONAL ARROW	2.19													
		19	LM3-4	CARDINAL DIRECTION MARKER (WEST)	2.00													
	25+38, 55' RT.	20	LM1-2(2)	INTERSTATE 40	4.00		13.00											
		21	LM6-1(L)	LEFT DIRECTIONAL ARROW	2.19													
	27+53, 43' RT.			LODGING										1.00				
	28+36, 35' LT.	23	R3-7(R)	RIGHT LANE MUST TURN RIGHT	6.25		13.00											
RAMP A	4399+57, 23' LT.	25		SPECIAL SIGN NO. 3		30.00												
		25A	E13-1	EXIT GORE ADVISORY SPEED (25 MPH)		14.00		15.50	16.00	1.52	212.00							
	4405+73, 15' RT.	26	W13-7E	ADVISORY RAMP SPEED	15.00		13.00											
RAMP A	4408+71, 21' RT.	27	R3-7(R)E	RIGHT LANE MUST TURN RIGHT	9.00		13.00											
	4409+48, 96' RT.			MERGE RIGHT									1.00					
	4411+52, 20' LT.	29	R5-1a	WRONG WAY	6.00		13.00											
	4411+58, 73' RT.	28	R1-2E	YIELD	6.93		13.00											
	4382+82, 42' RT.	30	R1-2E	YIELD	6.93		13.00											
RAMP C	4383+70, 69' RT.			YIELD									1.00					
	4384+28, 22' RT.			WRONG WAY									1.00					
	4384+43, 57' RT.			WRONG WAY									1.00					
	4380+16, 18' LT.			STOP									1.00					
	4380+38, 21' LT.	31	R1-1	STOP	5.18		13.00											
		32	R3-2	NO LEFT TURN	4.00													
	4380+40, 20' RT.	33	W14-2	NO OUTLET	6.25		13.00											
FRONTAGE ROAD	4380+57, 39' RT.			HISTORIC US 66 ROUTE										1.00				
	4381+64, 19' RT.	34	R2-1(2S)	SPEED LIMIT	5.00		13.00											
	4381+65, 35' RT.			SPEED LIMIT										1.00				
	4382+40, 23' LT.	35	R3-5(R)	RIGHT TURN ONLY	7.50		13.00											
	4385+42, 16' LT.			STOP										1.00				
				ONE MILE EXIT										1.00				
		36		SPECIAL SIGN NO. 7 (WASHINGTON/MAIN 1 MILE)		267.75				20.00	22.50	1.52	212.00					
				SPECIAL SIGN NO. 7 (EXIT 82)		23.75												
				FOOD										1.00				
				EXIT										1.00				
I-40	SEE SHEET NO. 52 FOR SIGN LOCATIONS	37		SPECIAL SIGN NO. 8 (WASHINGTON/MAIN)		236.00		17.50	20.00	1.52	212.00							
				SPECIAL SIGN NO. 8 (EXIT 82)		23.75												
				LODGING										1.00				
		38		SPECIAL SIGN NO. 9		9.00		11.00	11.00	1.52	212.00							
		39		SPECIAL SIGN NO. 10		18.75		12.00	12.00	1.52	212.00							
TOTALS:					180.87	623.00	299.00	-	157.50	7.60	1060.00	18.00	24.00					

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TRAFFIC CONTROL SUMMARY

SHEET NO.	CONSTRUCTION SIGNS																			
	C - E.25 S.F.						E.25 - 15.99 S.F.		16.0 - 32.99 S.F.		WING BARRICADES		CONSTRUCTION BARRICADES (TYPE III)		WARNING LIGHTS (TYPE A)		DRUMS		CHANNELIZER CONES	
	EA.	S.D.	EA.	S.D.	EA.	S.D.	EA.	S.D.	EA.	S.D.	EA.	S.D.	EA.	S.D.	EA.	S.D.	EA.	S.D.		
PHASE 1	880(B)						880(C)		880(E)		880(F)		880(G)							
ADV. WARNING	21	530	5	150	14	420	6	180	-	-	-	-	-	-	-	-	-	-	-	-
53-54	5	-	-	-	-	-	-	-	-	-	75	2,250	-	-	-	-	-	-	-	-
TOTALS:	26	630	5	150	14	420	6	180	-	-	-	-	75	2,250	-	-	-	-	-	-
PHASE 2	EA.						S.D.		EA.		S.D.		EA.		S.D.		EA.		S.D.	
ADV. WARNING	35	4,200	-	-	24	2,880	10	1,200	-	-	-	-	-	-	-	-	-	-	-	-
55-58	5	500	2	240	9	1,080	-	-	3	350	6	720	92	1,040	-	-	-	-	-	-
TOTALS:	40	4,800	2	240	33	3,960	10	1,200	3	350	6	720	92	1,040	-	-	-	-	-	-
PHASE 3	EA.						S.D.		EA.		S.D.		EA.		S.D.		EA.		S.D.	
ADV. WARNING	35	3,150	-	-	24	2,160	10	900	-	-	-	-	-	-	-	-	-	-	-	-
59-62	5	450	1	90	-	-	-	-	8	720	16	1,440	60	5,400	-	-	-	-	-	-
TOTALS:	40	3,600	1	90	24	2,160	10	900	8	720	16	1,440	60	5,400	-	-	-	-	-	-
PHASE 4	EA.						S.D.		EA.		S.D.		EA.		S.D.		EA.		S.D.	
ADV. WARNING	35	1,400	-	-	24	960	10	400	-	-	-	-	-	-	-	-	-	-	-	-
61-62	5	200	2	80	3	120	-	-	9	350	18	720	45	1,800	30	1,200	-	-	-	-
TOTALS:	40	1,600	2	80	27	1,080	10	400	9	350	18	720	45	1,800	30	1,200	-	-	-	-
TOTALS:	10,630	560	7,620	2,680	1,440	2,880	20,490	1,200												

SIGN DAYS: PHASE 1: 30 PHASE 2: 120 PHASE 3: 90 PHASE 4: 40

SUMMARY OF REMOVALS

COLD MILLING PAVEMENT	REMOVAL OF HEADWALL	REMOVAL OF CURB & GUTTER	REMOVAL OF CONCRETE PAVEMENT	REMOVAL OF ASPHALT PAVEMENT	REMOVAL OF CONCRETE DRIVEWAY	REMOVAL OF GUARDRAIL	REMOVAL OF 8" CONCRETE DIVIDING STRIP
412	619(B)*	619(B)	619(B)	619(B)	619(B)	619(B)	619(B)
S.Y.	EA.	L.F.	S.Y.	S.Y.	S.Y.	L.F.	S.Y.
5,322.00	2.00	3,392.00	5,162.00	24,013.00	1,232.00	2,780.00	812.00

* QUANTITY SHOWN IS FOR INFORMATIONAL PURPOSES ONLY. COST OF THIS ITEM IS INCLUDED IN OTHER ITEMS OF BID.

SUMMARY OF DITCH TREATMENT

STATION TO STATION	ALIGNMENT	LOCATION	LENGTH	BOTTOM WIDTH	CURTAIN WALLS	CLASS C CONCRETE 509(D)	DESIGN NO.	DITCH LINER PROTECTION
			FT.	FT.	EA.	C.Y.	L.F.	
4406+50 TO 4407+30	RAMP A	LT.	80.0	4.0	3.0	10.7	2	80.00

SUMMARY OF PAVEMENT MARKING

ALIGNMENT	STATION TO STATION	TRAFFIC STRIPE (PLASTIC) (4" WIDE)	TRAFFIC STRIPE (PLASTIC) (4" WIDE)	TRAFFIC STRIPE (PLASTIC) (6" WIDE)	TRAFFIC STRIPE (PLASTIC) (6" WIDE)	TRAFFIC STRIPE (PLASTIC) (8" WIDE)	TRAFFIC STRIPE (PLASTIC) (8" WIDE)	TRAFFIC STRIPE (PLASTIC) (12" WIDE)	TRAFFIC STRIPE (PLASTIC) (24" WIDE)	TRAFFIC STRIPE (PLASTIC) (SYMBOLS)	TRAFFIC STRIPE (PLASTIC) (ARROWS)	
		855(A)	855(A)	855(A)	855(A)	855(A)	855(A)	855(A)	855(A)	855(A)	855(B)	855(B)
		WHITE	YELLOW	WHITE	YELLOW	WHITE	YELLOW	WHITE	WHITE	855(B)	855(B)	
MAIN STREET	4370+60 TO 4378+00	572.00	1,110.00	0.00	0.00	0.00	190.00	0.00	0.00	0.00	7.00	
MAIN STREET	4378+00 TO 4387+00	871.00	944.00	0.00	0.00	0.00	82.00	0.00	428.00	0.00	9.00	
MAIN STREET	4387+00 TO 4391+25	228.00	525.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
WASHINGTON AVE.	22+51 TO 29+44	664.00	703.00	0.00	0.00	0.00	321.00	0.00	400.00	3.00	11.00	
RAMP A	4397+99 TO 4405+00	0.00	0.00	701.00	1,122.00	279.00	0.00	102.00	0.00	0.00	0.00	
RAMP A	4405+00 TO 4411+92	0.00	0.00	975.00	990.00	630.00	0.00	0.00	15.00	1.00	2.00	
RAMP C	4382+74 TO 4389+00	0.00	0.00	842.00	654.00	182.00	0.00	0.00	0.00	1.00	0.00	
RAMP C	4389+00 TO 4389+95	0.00	0.00	95.00	95.00	0.00	0.00	0.00	0.00	0.00	0.00	
FRONTAGE ROAD	4380+39 TO 4385+35	0.00	992.00	0.00	0.00	0.00	0.00	0.00	15.00	1.00	2.00	
TOTALS:		2,335.00	4,274.00	2,613.00	2,861.00	1,091.00	593.00	102.00	858.00	6.00	31.00	

SUMMARY OF CURB OPENING

ALIGNMENT	STATION	LOCATION	LENGTH	BOTTOM WIDTH	CURTAIN WALLS	CLASS C CONCRETE 509(D)	DESIGN NO.
			FT.	FT.	EA.	C.Y.	
MAIN ST	4378+19	RT.	25	2	2	1.47	1
MAIN ST	4379+13	LT.	5	5	2	0.548	1
MAIN ST	4380+55	RT.	12	2	2	0.745	1
MAIN ST	4380+57	LT.	14	2	2	0.848	1
WASHINGTON AVE	23+15	RT.	32	2	2	1.79	1
WASHINGTON AVE	23+43	LT.	5	5	2	0.726	1
WASHINGTON AVE	27+30	RT.	12	5	2	1.26	1
FRONTAGE RD	4380+36	LT.	10	2	2	0.639	1
FRONTAGE RD	4380+40	RT.	15	2	2	0.932	1
TOTALS:						8.98	

SUMMARY OF GUARDRAIL

ALIGNMENT	STATION TO STATION	LANE		TOTAL PANEL LENGTH INCLUDING ANCHOR UNITS*	BEAM GUARDRAIL W-BEAM SINGLE	GUARDRAIL TRAIL END TURNDOWN (31")	GUARDRAIL END TREATMENT (31")	GUARDRAIL DELINEATORS (TYPE 1, CODE 1)
		LT.	RT.					
		L.F.	FT.					
RAMP C	4386+43.00 TO 4390+43.00	X		400.00	375.00	1.00	1.00	10.00
RAMP C	4386+53.00 TO 4390+03.00		X	350.00	275.00	1.00	1.00	9.00
				650.00	2.00	2.00	19.00	

* QUANTITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. COST OF THESE ITEMS ARE INCLUDED IN OTHER ITEMS OF BID.

MAIN ST. CUSTER COUNTY

**SUMMARY SHEET
4 OF 4**

JOB PIECE NO. 27911(06) SHEET NO. 17

STORM WATER MANAGEMENT PLAN

SITE DESCRIPTION

PROJECT LIMITS: WASHINGTON AVE. - FROM APPROXIMATELY 0.06 MILES
NORTH OF EADS AVE. EXTENDING NORTH APPROXIMATELY 0.18 MILES.
MAIN ST. FROM 0.07 MILES WEST OF THE INTERSECTION OF
WASHINGTON AVE. EXTENDING EAST APPROXIMATELY 0.25 MILES

PROJECT DESCRIPTION: GRADE, DRAIN, SURFACE AND TRAFFIC SIGNAL PLANS

- SUGGESTED SEQUENCE OF EROSION CONTROL ACTIVITIES: _____
1. VEGETATIVE STRIPPING
 2. UNDERCUT & STOCKPILE EXISTING TOPSOIL
 3. INSTALL PERIMETER EROSION CONTROL MEASURES
 4. ROADWAY EXCAVATION AND EMBANKMENT
 5. BRIDGE CONSTRUCTION
 6. CULVERT TRENCHING AND CONSTRUCTION
 7. INSTALL TEMP. SEDIMENT FILTERS, SOD DITCHES, & VEGETATIVE MULCH
 8. CONST. FINISHED ROADWAY PAVING
 9. SPREAD TOPSOIL
 10. INSTALL SOLID SLAB SOD

SOIL TYPE: SAND, LOAMY SAND

AREA TO BE DISTURBED: 9.43 AC.

OFFSITE AREA TO BE DISTURBED: 0.00
 (FOR CONTRACTOR USE)

MAXIMUM ACRES TO BE DISTURBED AT ANY ONE TIME: _____
 (FOR CONTRACTORS USE)

LATITUDE & LONGITUDE OF CENTER OF PROJECT: 35°31'34"N 98°41'39"W

NAME OF RECEIVING WATERS: LITTLE DEEP CREEK

SENSITIVE WATERS OR WATERSHEDS: YES NO

303(d) IMPAIRED WATERS: YES NO

NOTE:
 THIS SHEET SHOULD BE USED IN CONJUNCTION WITH A DRAINAGE MAP THAT ILLUSTRATES THE DRAINAGE PATTERNS/PATHWAYS AND RECEIVING WATERS FOR THIS PROJECT. THIS SHEET SHOULD ALSO BE USED WITH THE EROSION CONTROL SUMMARIES, PAY ITEMS, & NOTES.

EROSION AND SEDIMENT CONTROLS

SOIL STABILIZATION PRACTICES:

- TEMPORARY SEEDING
- PERMANENT SODDING, SPRIGGING OR SEEDING
- VEGETATIVE MULCHING
- SOIL RETENTION BLANKET
- PRESERVATION OF EXISTING VEGETATION

NOTE: TEMPORARY EROSION CONTROL METHODS ARE TO BE USED ON ALL DISTURBED AREAS WHERE CONST. ACTIVITIES HAVE CEASED FOR OVER 14 DAYS. METHODS USED WILL BE AS SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER.

STRUCTURAL PRACTICES:

- STABILIZED CONSTRUCTION EXIT
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKES
- TEMPORARY FIBER LOG
- DIVERSION, INTERCEPTOR OR PERIMETER DIKES
- DIVERSION, INTERCEPTOR OR PERIMETER SWALES
- ROCK FILTER DAMS
- TEMPORARY SLOPE DRAIN
- PAVED DITCH W/ DITCH LINER PROTECTION
- TEMPORARY DIVERSION CHANNELS
- TEMPORARY SEDIMENT BASINS
- TEMPORARY SEDIMENT TRAPS
- TEMPORARY SEDIMENT FILTERS
- TEMPORARY SEDIMENT REMOVAL
- RIP RAP
- INLET SEDIMENT FILTER
- TEMPORARY BRUSH SEDIMENT BARRIERS
- SANDBAG BERMS
- TEMPORARY STREAM CROSSINGS

OFFSITE VEHICLE TRACKING:

- HAUL ROADS DAMPENED FOR DUST CONTROL
- LOADED HAUL TRUCKS TO BE COVERED WITH TARPAULIN
- EXCESS DIRT ON ROAD REMOVED DAILY

NOTES:

THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE FOLLOWING:

MAINTENANCE AND INSPECTION:

ALL EROSION AND SEDIMENT CONTROLS WILL BE MAINTAINED IN GOOD WORKING ORDER FROM THE BEGINNING OF CONSTRUCTION UNTIL AN ACCEPTABLE VEGETATIVE COVER IS ESTABLISHED. INSPECTION BY THE CONTRACTOR AND ANY NECESSARY REPAIRS SHALL BE PERFORMED ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS AFTER ANY STORM EVENT GREATER THAN 0.5 INCH AS RECORDED BY A NON-FREEZING RAIN GAUGE TO BE LOCATED ON SITE. POTENTIALLY ERODIBLE AREAS, DRAINAGEWAYS, MATERIAL STORAGE, STRUCTURAL DEVICES, CONSTRUCTION ENTRANCES AND EXITS ALONG WITH EROSION AND SEDIMENT CONTROL LOCATIONS ARE EXAMPLES OF SITES THAT NEED TO BE INSPECTED.

WASTE MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF CONSTRUCTION WASTE MATERIAL IS REQUIRED BY THE CONTRACTOR. MATERIALS INCLUDE STOCKPILES, SURPLUS, DEBRIS AND ALL OTHER BY-PRODUCTS FROM THE CONSTRUCTION PROCESS. PRACTICES INCLUDE DISPOSAL, PROPER MATERIALS HANDLING, SPILL PREVENTION AND CLEANUP MEASURES. CONTROLS AND PRACTICES SHALL MEET THE REQUIREMENTS OF ALL FEDERAL, STATE AND LOCAL AGENCIES.

HAZARDOUS MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF HAZARDOUS WASTE MATERIALS IS REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING MANUFACTURER'S RECOMMENDATIONS, STATE AND FEDERAL REGULATIONS TO ENSURE CORRECT HANDLING, DISPOSAL, SPILL PREVENTION AND CLEANUP MEASURES. EXAMPLES INCLUDE BUT ARE NOT LIMITED TO: PAINTS, ACIDS, CLEANING SOLVENTS, CHEMICAL ADDITIVES, CONCRETE CURING COMPOUNDS AND CONTAMINATED SOILS.

GENERAL NOTES:

A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED TO COMPLY WITH THE OKLAHOMA POLLUTION DISCHARGE ELIMINATION SYSTEM (OPDES) REGULATIONS. THIS PLAN IS INITIATED DURING THE DESIGN PHASE, CONFIRMED IN THE PRE-WORK MEETINGS AND AVAILABLE ON THE JOB SITE ALONG WITH COPIES OF THE NOTICE OF INTENT (NOI) FORM AND PERMIT CERTIFICATE THAT HAVE BEEN FILED WITH THE OKLAHOMA DEPARTMENT OF ENVIRONMENTAL QUALITY (ODEQ). THE PLAN MUST BE KEPT CURRENT WITH UP-TO-DATE AMENDMENTS DURING THE PROGRESSION OF THE PROJECT. ALL CONTRACTOR OFF-SITE OPERATIONS ASSOCIATED WITH THE PROJECT MUST BE DOCUMENTED IN THE SWPPP, I.E., BORROW PITS, WORK ROADS, DISPOSAL SITES, ASPHALT/CONCRETE PLANTS, ETC. THE BASIC GOAL OF STORM WATER MANAGEMENT IS TO IMPROVE WATER QUALITY BY REDUCING POLLUTANTS IN STORM WATER DISCHARGES. RUNOFF FROM CONSTRUCTION SITES HAS A POTENTIAL FOR POLLUTION DUE TO EXPOSED SOILS AND THE PRESENCE OF HAZARDOUS MATERIALS USED IN THE CONSTRUCTION PROCESS. THE PREVENTION OF SOIL EROSION, CONTAINMENT OF HAZARDOUS MATERIALS AND/OR THE INTERCEPTION OF THESE POLLUTANTS BEFORE LEAVING THE CONSTRUCTION SITE ARE THE BEST PRACTICES FOR CONTROLLING STORM WATER POLLUTION.

THE FOLLOWING SECTIONS OF THE 2009 ODOT STANDARD SPECIFICATIONS SHOULD BE NOTED:

103.05	BONDING REQUIREMENTS
104.10	FINAL CLEANING UP
104.12	CONTRACTOR'S RESPONSIBILITY FOR WORK
104.13	ENVIRONMENTAL PROTECTION
106.08	STORAGE AND HANDLING OF MATERIAL
107.01	LAWS, RULES AND REGULATIONS TO BE OBSERVED
107.20	STORM WATER MANAGEMENT
220	MANAGEMENT OF EROSION, SEDIMENTATION AND STORM WATER POLLUTION PREVENTION AND CONTROL
221	TEMPORARY SEDIMENT CONTROL

IN ADDITION:

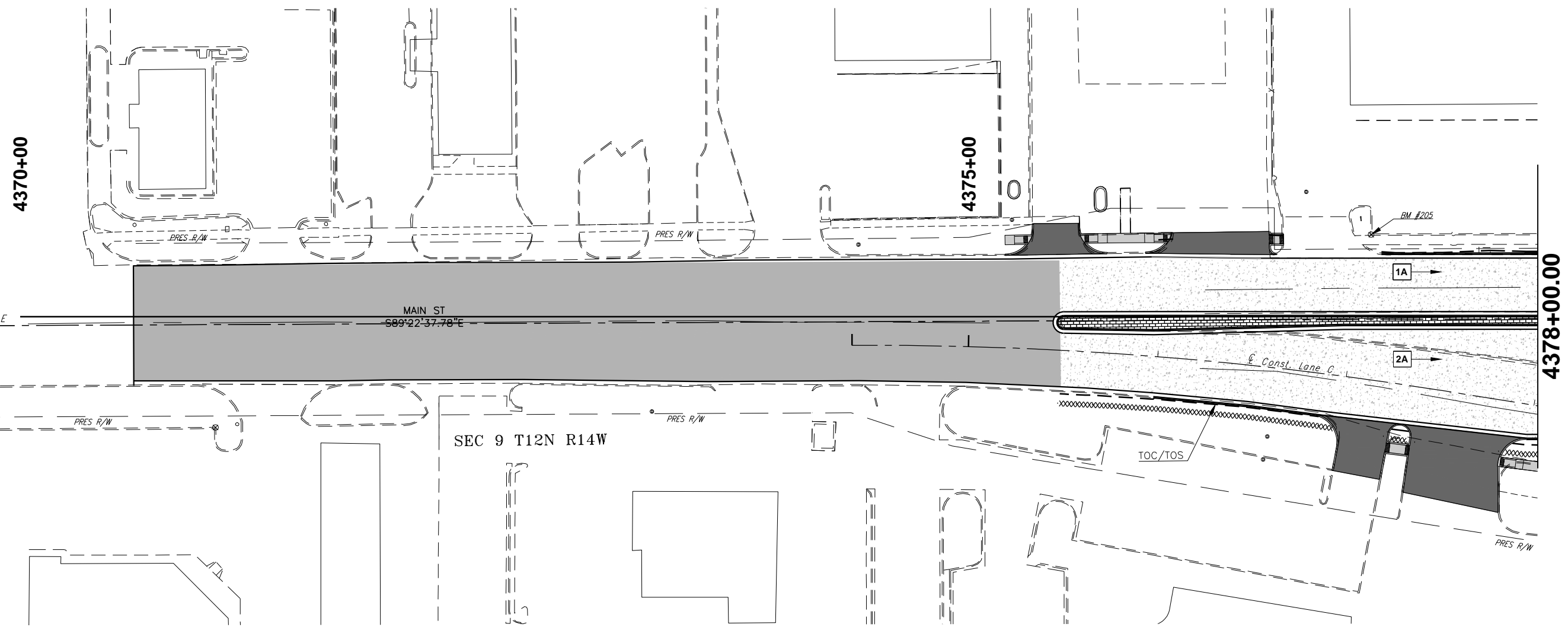
"ODEQ GENERAL PERMIT (OKR10) FOR STORM WATER DISCHARGES FROM CONSTRUCTION ACTIVITIES WITHIN THE STATE OF OKLAHOMA." ODEQ, WATER QUALITY DIVISION, SEPTEMBER 13, 2012.

MAIN ST. CUSTER COUNTY

STORM WATER MANAGEMENT PLAN

JOB PIECE NO. 27911(06) SHEET NO. 18


7/28/2016 11:35:36 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\1001.DWG - WEATHERFORD - L-40 - RAMP - PHASE 2\PROJECT DRAWINGS\EROSION\MAIN ST EROSION CONTROL.DWG




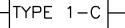
OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	1A	0.69 AC.
		1B	0.29 AC.
TOTAL			0.98 AC.


OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	2A	0.69 AC.
TOTAL			0.69 AC.

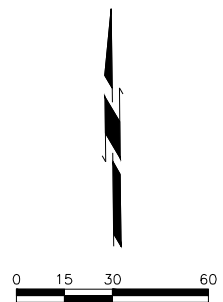
LEGEND

TEMPORARY SILT DIKE 

TEMPORARY SILT FENCE 

SEDIMENT FILTER 

RIP RAP 

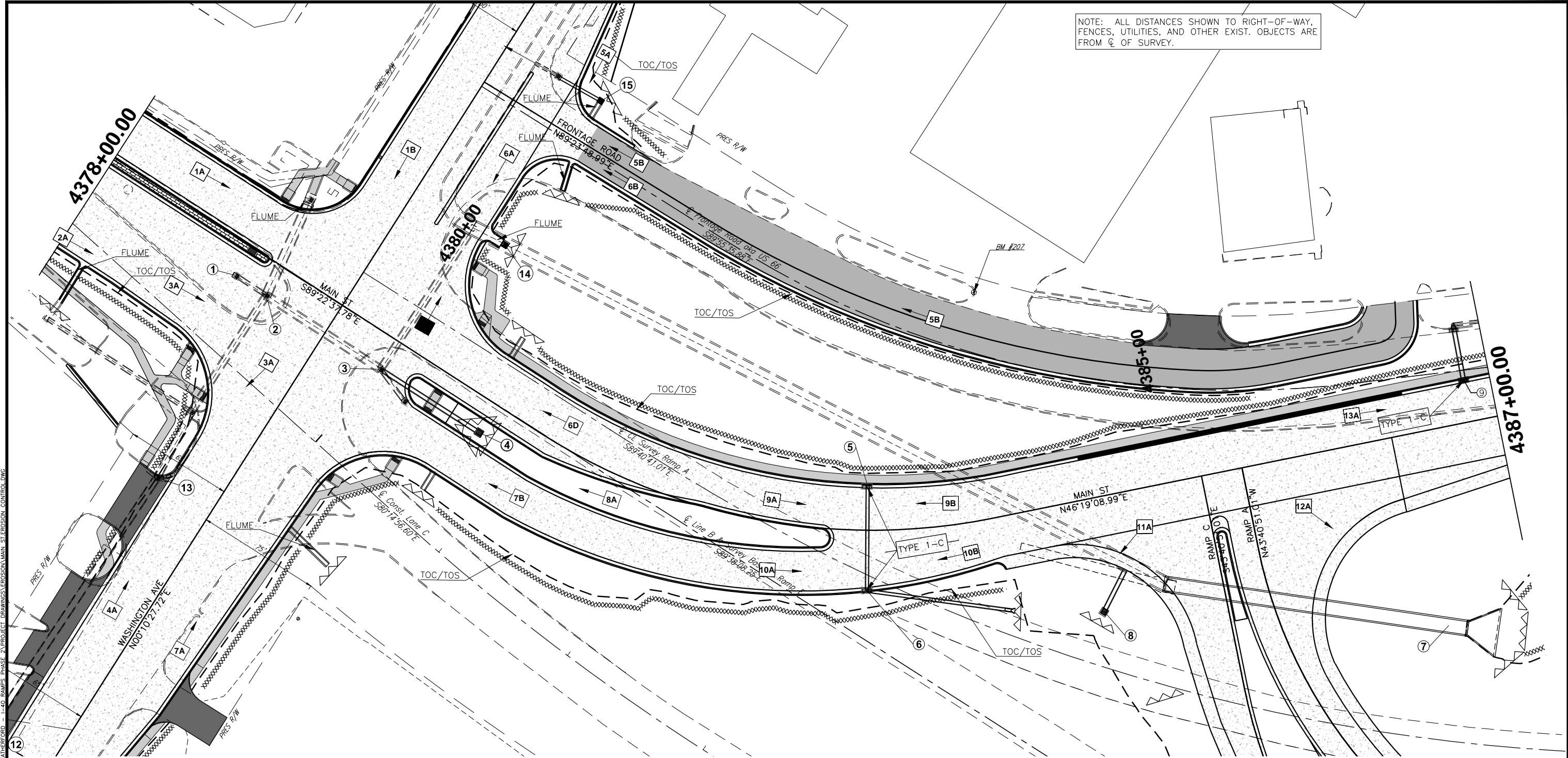


MAIN ST. CUSTER COUNTY

TEMPORARY EROSION CONTROL
SHEET 1 OF 8

JOB PIECE NO. 27911(06) SHEET NO. 19

NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM \bar{C} OF SURVEY.



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OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	1A	0.69 AC.
		1B	0.29 AC.
TOTAL			0.98 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	2A	0.69 AC.
			0.69 AC.
TOTAL			0.69 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	3A	0.27 AC.
			0.27 AC.
TOTAL			0.27 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 13	4A	0.19 AC.
			0.19 AC.
TOTAL			0.19 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 15	5A	0.02 AC.
		5B	0.40 AC.
TOTAL			0.42 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 14	6A	0.11 AC.
		6B	0.04 AC.
		6C	0.18 AC.
		6D	0.11 AC.
TOTAL			0.44 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
OFF PROJECT		7A	0.45 AC.
		7B	0.12 AC.
TOTAL			0.57 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 4	8A	0.08 AC.
			0.08 AC.
TOTAL			0.08 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 5	9A	0.12 AC.
		9B	0.24 AC.
TOTAL			0.36 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 6	10A	0.14 AC.
		10B	0.07 AC.
TOTAL			0.21 AC.

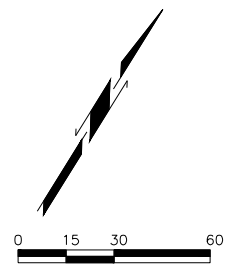
OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 8	11A	0.39 AC.
			0.39 AC.
TOTAL			0.39 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	DITCH	12A	3.25 AC.
		12B	0.20 AC.
TOTAL			3.45 AC.

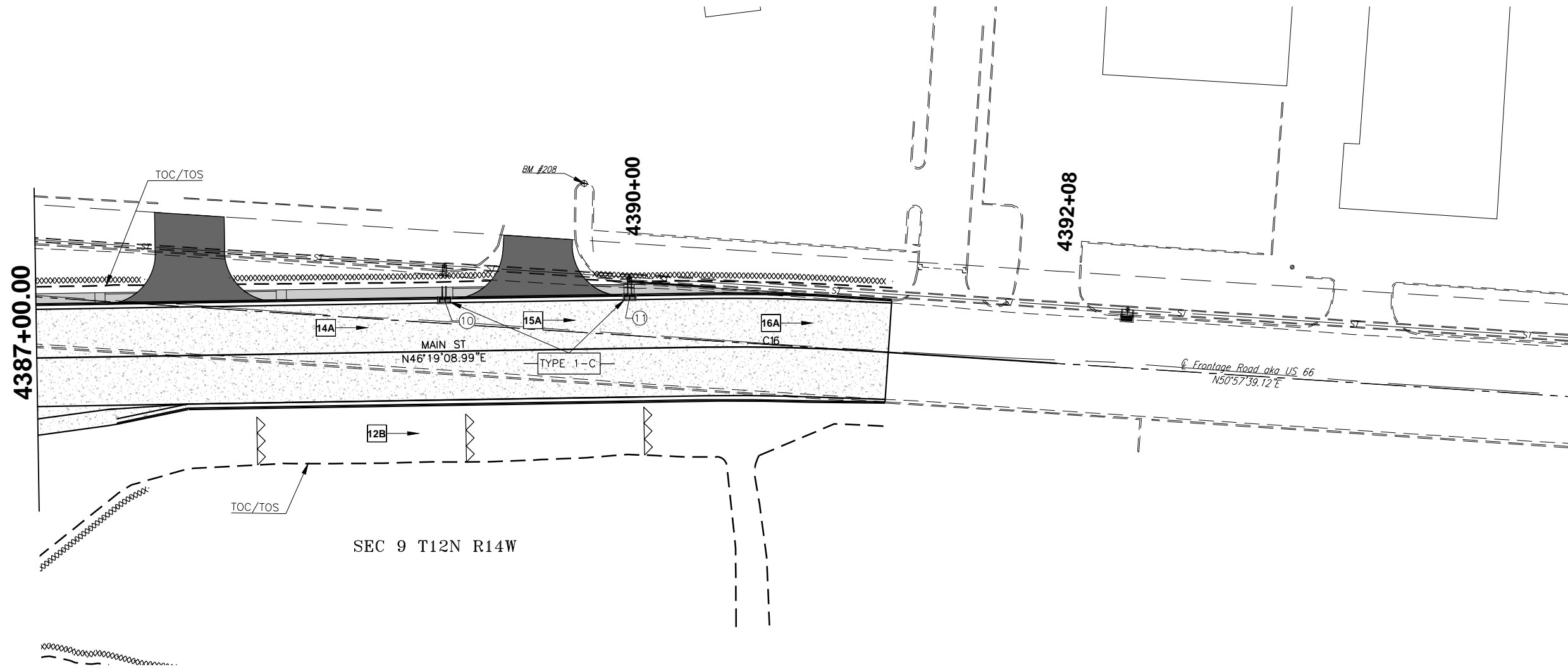
OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
OFF PROJECT	STR. 9	13A	0.08 AC.
TOTAL			0.08 AC.

LEGEND

- TEMPORARY SILT DIKE
- TEMPORARY SILT FENCE
- SEDIMENT FILTER
- RIP RAP



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OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	DITCH	12A	3.25 AC.
		12B	0.20 AC.
	TOTAL		3.45 AC.

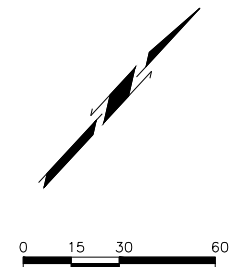
OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
OFF PROJECT	STR. 10	14A	0.31 AC.
TOTAL			0.31 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
OFF PROJECT	STR. 11	15B	0.12 AC.
TOTAL			0.12 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
OFF PROJECT		16A	0.16 AC.
TOTAL			0.16 AC.

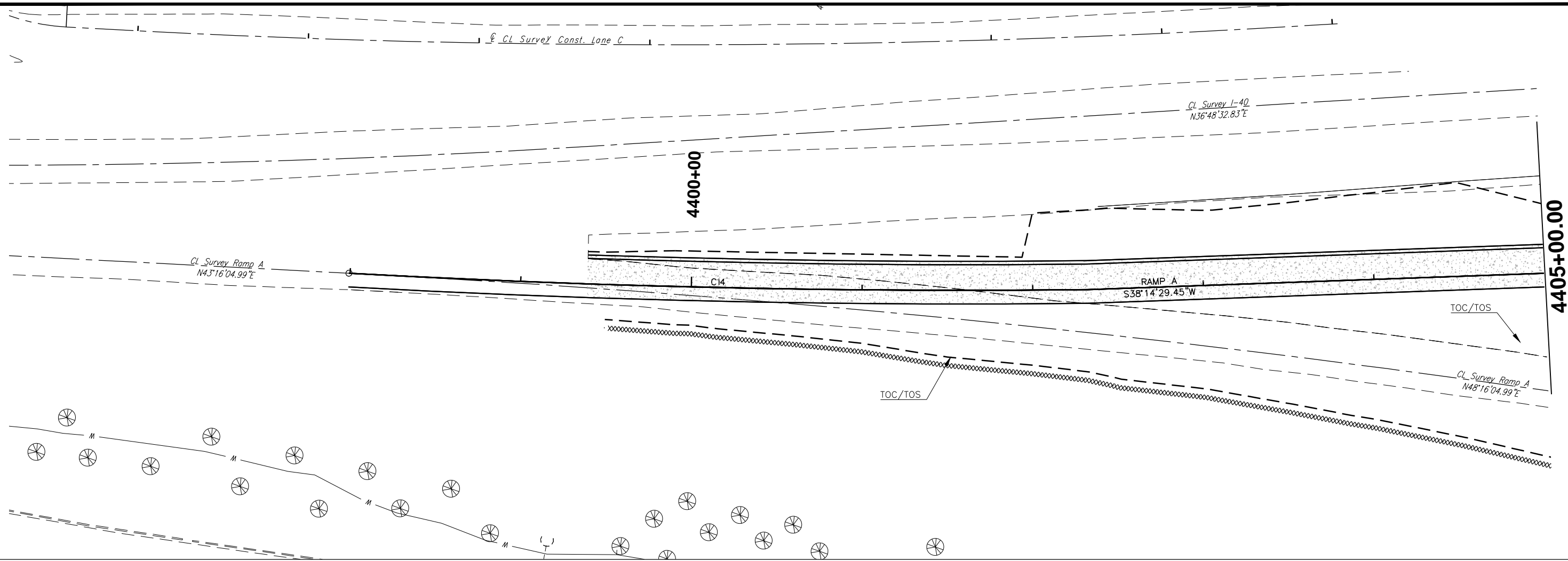
LEGEND

- TEMPORARY SILT DIKE
- TEMPORARY SILT FENCE
- SEDIMENT FILTER
- RIP RAP

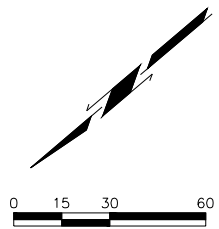


MAIN ST. CUSTER COUNTY
TEMPORARY EROSION CONTROL SHEET 3 OF 8
 JOB PIECE NO. 27911(06) SHEET NO. 21

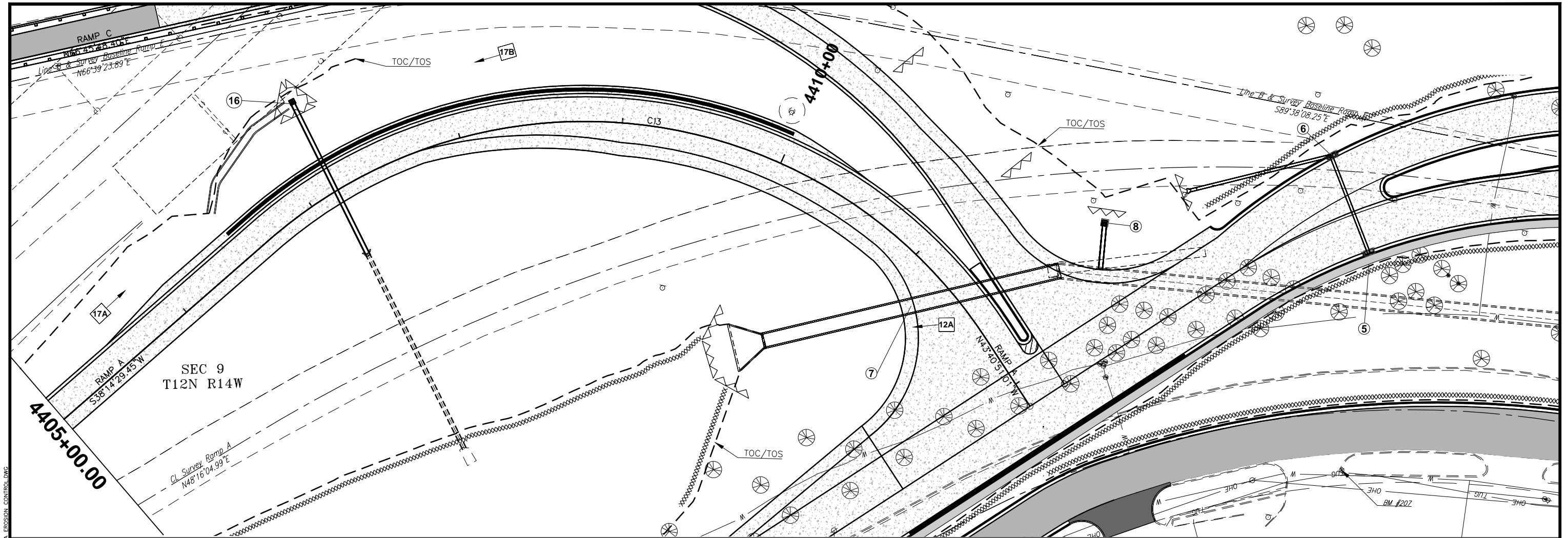
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LEGEND	
TEMPORARY SILT DIKE	
TEMPORARY SILT FENCE	
SEDIMENT FILTER	
RIP RAP	



MAIN ST. CUSTER COUNTY
TEMPORARY EROSION CONTROL
SHEET 4 OF 8
 JOB PIECE NO. 27911(06) SHEET NO. 22



7/8/2016 11:37:10 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\DOT\LOD10.DWG - WEATHERFORD - I-40 RAMP5 PHASE 2\PROJECT DRAWINGS\EROSION\RAMP A - EROSION CONTROL.DWG

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	DITCH	12A	3.25 AC.
		12B	0.20 AC.
TOTAL			3.45 AC.



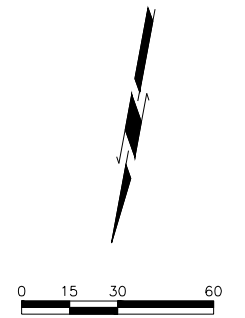
LEGEND

TEMPORARY SILT DIKE

TEMPORARY SILT FENCE

SEDIMENT FILTER

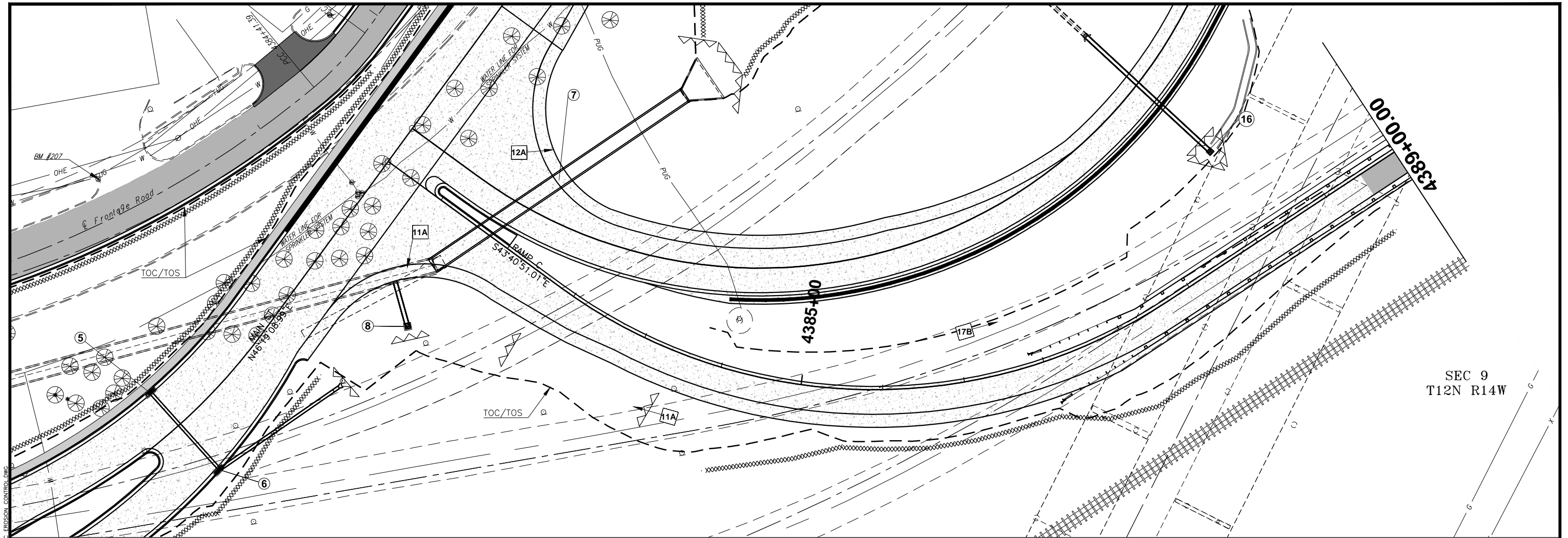
RIP RAP



MAIN ST. CUSTER COUNTY

TEMPORARY EROSION CONTROL
SHEET 5 OF 8

JOB PIECE NO. 27911(06) SHEET NO. 23



SEC 9
T12N R14W

7/28/2016 11:38:05 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\1010.DWG - WEATHERFORD - I-40 RAMP'S PHASE 2\PROJECT DRAWINGS\EROSION\BMP_C_EROSION_CONTROL.DWG

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 8	11A	0.39 AC.
TOTAL			0.39 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	DITCH	12A	3.25 AC.
		12B	0.20 AC.
TOTAL			3.45 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 16	DITCH	17A	0.26 A.C.
		17B	0.45 A.C.
TOTAL			0.71 A.C.

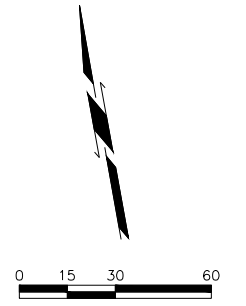
LEGEND

TEMPORARY SILT DIKE

TEMPORARY SILT FENCE

SEDIMENT FILTER

RIP RAP



MAIN ST. CUSTER COUNTY

TEMPORARY EROSION CONTROL SHEET 6 OF 8

JOB PIECE NO. 27911(06) SHEET NO. 24

7/28/2016 11:38:23 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\06071\06071.dwg - WEATHERFORD - L-40 - RAMP - PHASE 2\PROJECT DRAWINGS\EROSION\BMP_C - EROSION CONTROL.dwg

4389+00.00

TOC/TOS

Cl. Survey L-40
N36°48'32.83"E

4390+00

Line B & Survey Baseline Ramp E
N66°39'23.89"E

RAMP C
N66°45'48.40"E

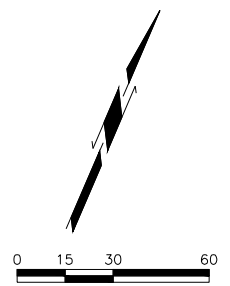
Const. Lane C
N66°45'48.40"E

SEC 9
T12N R14W

BM #211

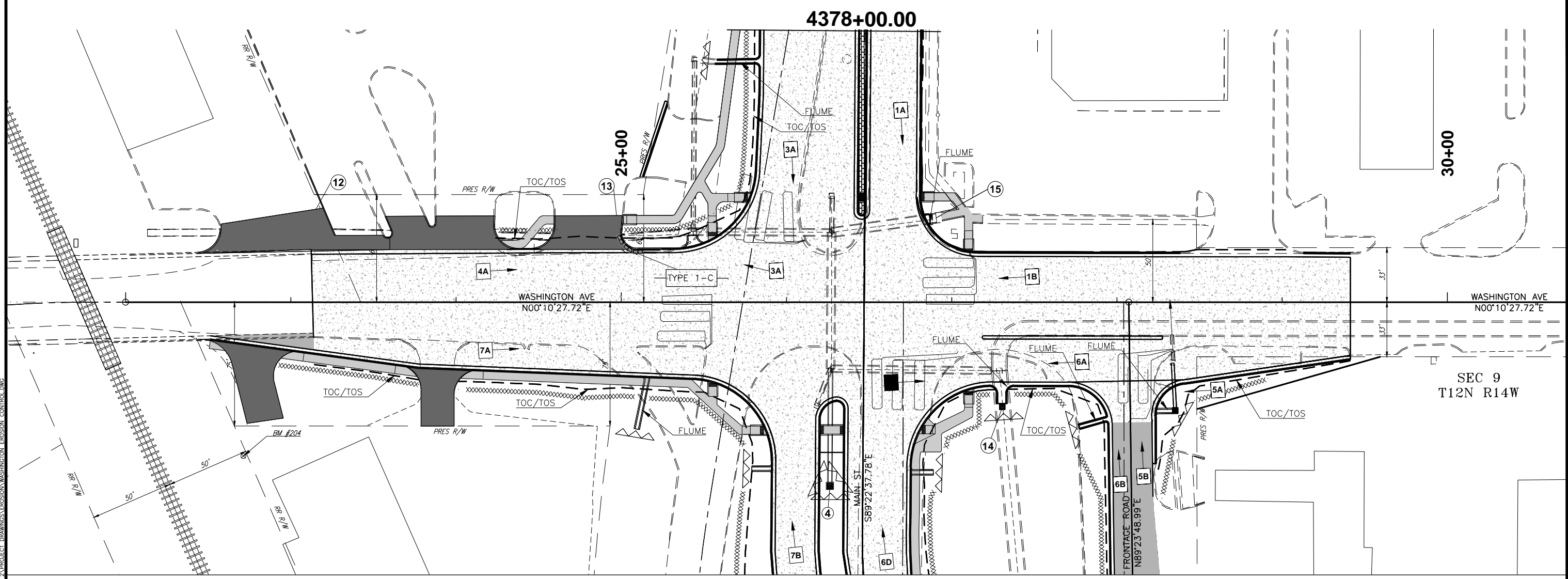
BM #212

LEGEND	
TEMPORARY SILT DIKE	
TEMPORARY SILT FENCE	
SEDIMENT FILTER	
RIP RAP	



MAIN ST. CUSTER COUNTY
TEMPORARY EROSION CONTROL
SHEET 7 OF 8
 JOB PIECE NO. 27911(06) SHEET NO. 25

7/8/2016 11:39:42 AM N:\KOC\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\10019_04 - WEATHERFORD - I-40 RAMP\PHASE 2\PROJECT DRAWINGS\EROSION\WASHINGTON_EROSION_CONTROL.DWG



OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	1A	0.69 AC.
	EXISTING INLET	1B	0.29 AC.
	TOTAL		0.98 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 15	5A	0.02 AC.
	STR. 15	5B	0.40 AC.
	TOTAL		0.42 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	EXISTING INLET	3A	0.27 AC.
	TOTAL		0.27 AC.

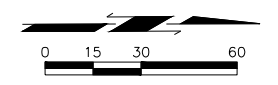
OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 14	6A	0.11 AC.
	STR. 14	6B	0.04 AC.
	STR. 14	6C	0.18 AC.
	STR. 14	6D	0.11 AC.
	TOTAL		0.44 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
STR. 7	STR. 13	4A	0.19 AC.
	TOTAL		0.19 AC.

OUTFALL	LOCAL COLLECTION	D.A.	DISTURBED AREA
OFF PROJECT	7A		0.45 AC.
	7B		0.12 AC.
	TOTAL		0.57 AC.

LEGEND

- TEMPORARY SILT DIKE
- TEMPORARY SILT FENCE
- SEDIMENT FILTER
- RIP RAP



MAIN ST. CUSTER COUNTY
TEMPORARY EROSION CONTROL
SHEET 8 OF 8
 JOB PIECE NO. 27911(06) SHEET NO. 26

7/8/2016 11:39:47 AM N:\GCS\TRANSPORTATION\HIGHWAY\PRODUCTION\DDOT\10119.04 - WEATHERFORD - I-40 RAMP\PHASE 2\PROJECT DRAWINGS\GEOMETRY\SURVEY CONTROL DATA.DWG

CONTROL POINT TABLE				
Point #	Raw Description	Northing	Easting	Elevation
1	CP 1 5/8IN IP 24IN W/ ALUM CAP HERE	190981.2158	1760565.8292	1640.48
2	CP 2 SET 5/8IN IP 24IN W/ALUM CAP	194302.3247	1765185.5835	1602.09
3	CP 3 SET 5/8IN IP W/CAP	191234.0471	1761915.4595	1618.22
4	CP 4 SET 5/8IN IP W/CAP	191302.5765	1762243.6990	1628.99

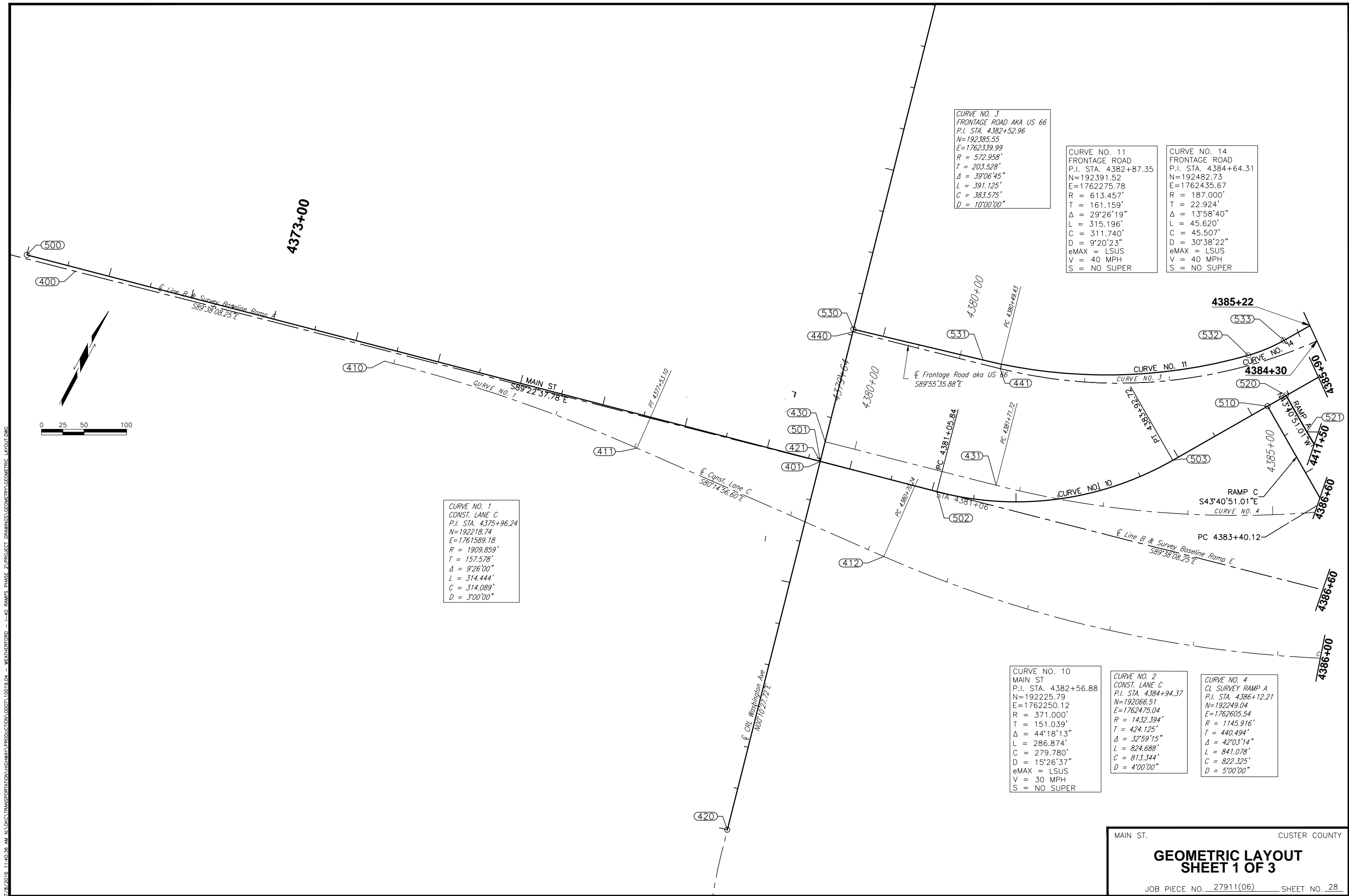
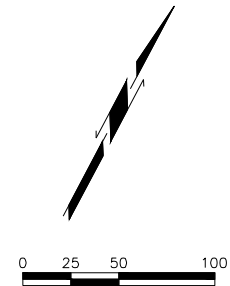
BENCHMARK TABLE				
Point #	Raw Description	Northing	Easting	Elevation
201	BM 201 CUT X IN ECD	190933.7	1760825.6	1635.87
202	BM 202 SET 5/8IN IP W/CAP	191069.8	1761325.9	1627.67
203	BM 203 X CUT IN CONC PART OF INLET	191461.3	1761705.6	1623.86
204	BM 204 CUT X IN TOW	191853.0	1762049.3	1624.36
205	BM 205 X CUT IN BC	192274.9	1761705.9	1625.06
206	BM 206 X CUT IN BC	192180.0	1761095.7	1631.32
207	BM 207 X CUT IN BC	192439.5	1762276.5	1624.45
208	BM 208 CUT X IN BC	192792.1	1762724.7	1623.33
209	BM 209 CUT X IN BC	193174.6	1763167.6	1616.45
210	BM 210 SET 5/8IN IP W/CAP	192070.0	1762549.9	1625.59
211	BM 211 SET 5/8IN IP W/CAP	192332.3	1763170.0	1618.11
212	BM 212 SET 5/8IN IP W/CAP	192558.8	1763718.6	1609.69
213	BM 213 SET RR SPIKE IN COR POST	192995.2	1764040.7	1607.18
214	BM 214 SET RR SPIKE IN PP	193510.8	1764266.9	1602.90
215	BM 215 CUT X IN BC	194038.3	1764762.4	1599.65
216	BM 216 CUT X IN BC	191126.5	1762855.3	1622.60

MAIN ST. CUSTER COUNTY

SURVEY CONTROL DATA

JOB PIECE NO. 27911(06) SHEET NO. 27

7/8/2016 11:40:36 AM N:\CNC\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\10019.04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\GEOMETRY\GEOMETRIC LAYOUT.DWG



CURVE NO. 3
FRONTAGE ROAD AKA US 66
P.I. STA. 4382+52.96
N=192385.55
E=1762339.99
R = 572.958'
T = 203.528'
Δ = 39°06'45"
L = 391.125'
C = 383.575'
D = 10'00'00"

CURVE NO. 11
FRONTAGE ROAD
P.I. STA. 4382+87.35
N=192391.52
E=1762275.78
R = 613.457'
T = 161.159'
Δ = 29°26'19"
L = 315.196'
C = 311.740'
D = 9°20'23"
eMAX = LSUS
V = 40 MPH
S = NO SUPER

CURVE NO. 14
FRONTAGE ROAD
P.I. STA. 4384+64.31
N=192482.73
E=1762435.67
R = 187.000'
T = 22.924'
Δ = 13°58'40"
L = 45.620'
C = 45.507'
D = 30°38'22"
eMAX = LSUS
V = 40 MPH
S = NO SUPER

CURVE NO. 1
CONST. LANE C
P.I. STA. 4375+96.24
N=192218.74
E=1761589.18
R = 1909.859'
T = 157.578'
Δ = 9°26'00"
L = 314.444'
C = 314.089'
D = 3'00'00"

CURVE NO. 10
MAIN ST
P.I. STA. 4382+56.88
N=192225.79
E=1762250.12
R = 371.000'
T = 151.039'
Δ = 44°18'13"
L = 286.874'
C = 279.780'
D = 15°26'37"
eMAX = LSUS
V = 30 MPH
S = NO SUPER

CURVE NO. 2
CONST. LANE C
P.I. STA. 4384+94.37
N=192066.51
E=1762475.04
R = 1432.394'
T = 424.125'
Δ = 32°59'15"
L = 824.688'
C = 813.344'
D = 4'00'00"

CURVE NO. 4
CL SURVEY RAMP A
P.I. STA. 4386+12.21
N=192249.04
E=1762605.54
R = 1145.916'
T = 440.494'
Δ = 42°03'14"
L = 841.078'
C = 822.325'
D = 5'00'00"

MAIN ST. CUSTER COUNTY
**GEOMETRIC LAYOUT
SHEET 1 OF 3**
JOB PIECE NO. 27911(06) SHEET NO. 28

CURVE NO. 15
 FRONTAGE ROAD
 P.I. STA. 4385+20.44
 N=192521.65
 E=1762476.43
 R = 292432.960'
 T = 33.433"
 Δ = 0°00'47"
 L = 66.865'
 C = 66.865'
 D = 0°01'11"
 eMAX = LSUS
 V = 40 MPH
 S = NO SUPER

CURVE NO. 16
 FRONTAGE ROAD
 P.I. STA. 4385+63.73
 N=192551.56
 E=1762507.73
 R = 11.992'
 T = 9.858"
 Δ = 78°50'49"
 L = 16.502'
 C = 15.230'
 D = 477°47'56"
 eMAX = LSUS
 V = 40 MPH
 S = NO SUPER

CURVE NO. 12
 MAIN ST
 P.I. STA. 4390+75.60
 N=192801.73
 E=1762853.21
 R = 1039.000'
 T = 42.109'
 Δ = 4°38'30"
 L = 84.173'
 C = 84.150'
 D = 5°30'52"
 eMAX = LSUS
 V = 40 MPH
 S = NO SUPER

CURVE NO. 27
 CL SURVEY I-40
 P.I. STA. 4401+62.56
 N=193508.26
 E=1763898.52
 R = 5729.587'
 T = 712.600"
 Δ = 14°10'45"
 L = 1417.919'
 C = 1414.303'
 D = 1°00'00"

CURVE NO. 13
 RAMP A
 P.I. STA. 4400+15.80
 N=192929.95
 E=1763352.31
 R = 6480.000'
 T = 216.432"
 Δ = 3°49'33"
 L = 432.703'
 C = 432.622'
 D = 0°53'03"
 eMAX = 6%
 V = 25 MPH
 S = 4.6%

CURVE NO. 8
 RAMP A
 P.I. STA. 4410+03.03
 N=192160.45
 E=1762733.68
 R = 292.000'
 T = 332.113"
 Δ = 97°21'18"
 L = 496.156'
 C = 438.587'
 D = 19°37'19"
 eMAX = LSUS
 V = 25 MPH
 S = 5.8%

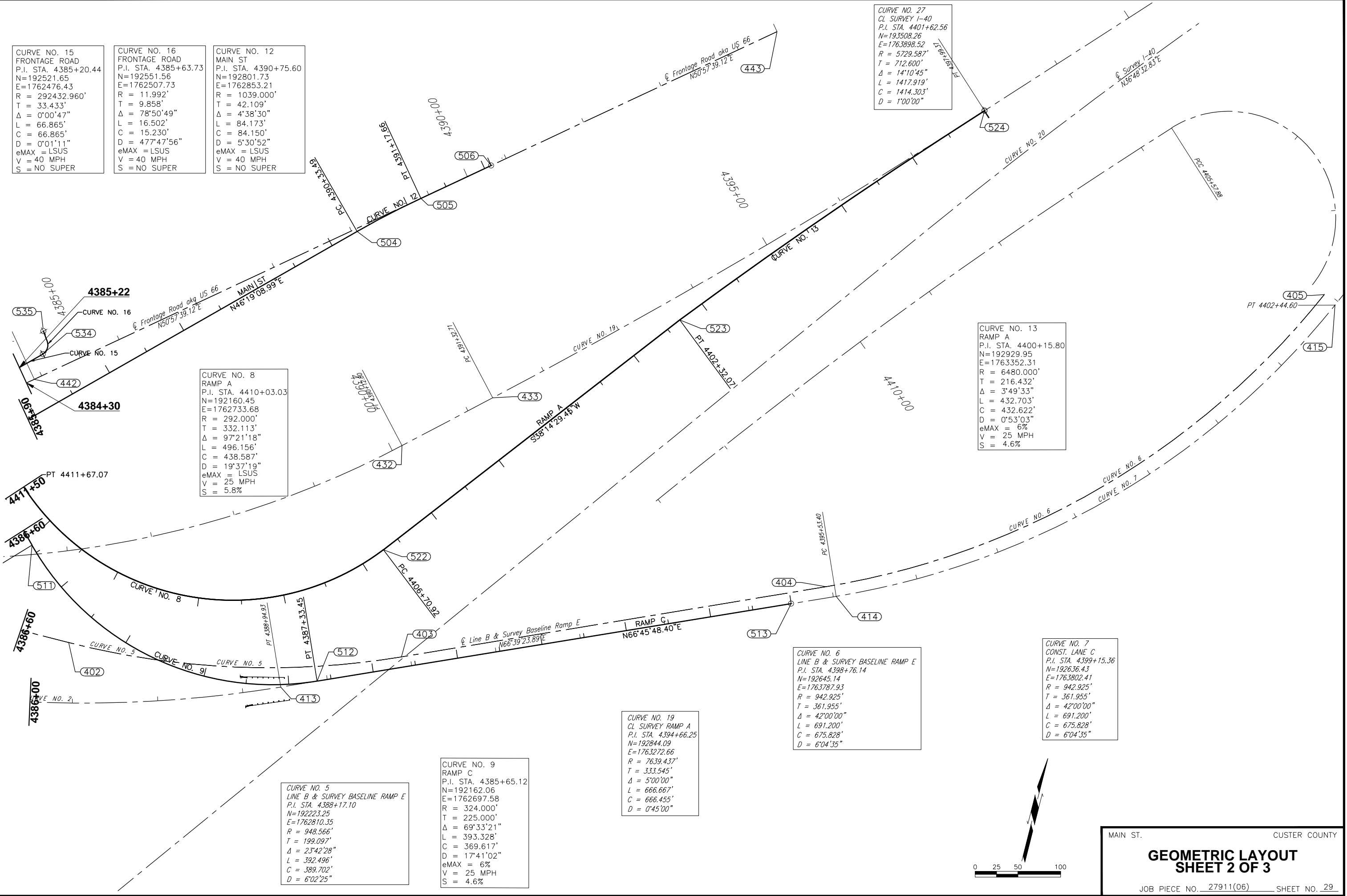
CURVE NO. 6
 LINE B & SURVEY BASELINE RAMP E
 P.I. STA. 4398+76.14
 N=192645.14
 E=1763787.93
 R = 942.925'
 T = 361.955"
 Δ = 42°00'00"
 L = 691.200'
 C = 675.828'
 D = 6°04'35"

CURVE NO. 7
 CONST. LANE C
 P.I. STA. 4399+15.36
 N=192636.43
 E=1763802.41
 R = 942.925'
 T = 361.955"
 Δ = 42°00'00"
 L = 691.200'
 C = 675.828'
 D = 6°04'35"

CURVE NO. 19
 CL SURVEY RAMP A
 P.I. STA. 4394+66.25
 N=192844.09
 E=1763272.66
 R = 7639.437'
 T = 333.545"
 Δ = 5°00'00"
 L = 666.667'
 C = 666.455'
 D = 0°45'00"

CURVE NO. 9
 RAMP C
 P.I. STA. 4385+65.12
 N=192162.06
 E=1762697.58
 R = 324.000'
 T = 225.000"
 Δ = 69°33'21"
 L = 393.328'
 C = 369.617'
 D = 17°41'02"
 eMAX = 6%
 V = 25 MPH
 S = 4.6%

CURVE NO. 5
 LINE B & SURVEY BASELINE RAMP E
 P.I. STA. 4388+17.10
 N=192223.25
 E=1762810.35
 R = 948.566'
 T = 199.097"
 Δ = 23°42'28"
 L = 392.496'
 C = 389.702'
 D = 6°02'25"



7/8/2016 11:41:00 AM N:\C:\TRANSPORTATION\HIGHWAY PRODUCTION\0001\10019.04 - WEATHERFORD - I-40 RAMP5 PHASE 2\PROJECT DRAWINGS\GEOMETRY\GEOMETRIC LAYOUT.DWG

CL LINE B & SURVEY BASELINE RAMP E			
POINT NO.	STATION	NORTHING	EASTING
400	4370+60	192234.4283	1761053.3099
401	4379+64	192228.6785	1761957.4283
402	4386+18	192224.5203	1762611.2600
403	4390+11	192302.1443	1762993.1525
404	4395+14	192501.7227	1763455.6047
405	4402+05	192974.0973	1763938.9326

CRL MAIN ST.			
POINT NO.	STATION	NORTHING	EASTING
500	4370+00	192239.4577	1760993.3174
501	4379+64	192228.9768	1761957.4292
502	4381+06	192227.4368	1762099.0951
503	4383+93	192330.1083	1762359.3557
504	4390+33	192772.6518	1762822.7612
505	4391+18	192828.2567	1762885.9215
506	4392+08	192885.4595	1762956.4625

CL CONST. LANE C			
POINT NO.	STATION	NORTHING	EASTING
410	4374+39	192219.6163	1761431.6076
411	4377+53	192192.0544	1761744.4854
412	4380+70	192138.3418	1762057.0438
413	4388+95	192233.8387	1762864.7624
414	4395+53	192493.6238	1763469.8199
415	4402+45	192965.0965	1763954.0275

CRL RAMP C			
POINT NO.	STATION	NORTHING	EASTING
510	4382+10	192418.8810	1762452.3132
511	4383+40	192324.7760	1762542.1817
512	4387+33	192250.8252	1762904.3251
513	4392+94	192472.5552	1763420.7497

CL SURVEY WASHINGTON AVE.			
POINT NO.	STATION	NORTHING	EASTING
420	22+00	191781.9537	1761956.0688
421	26+47	192228.9768	1761957.4292

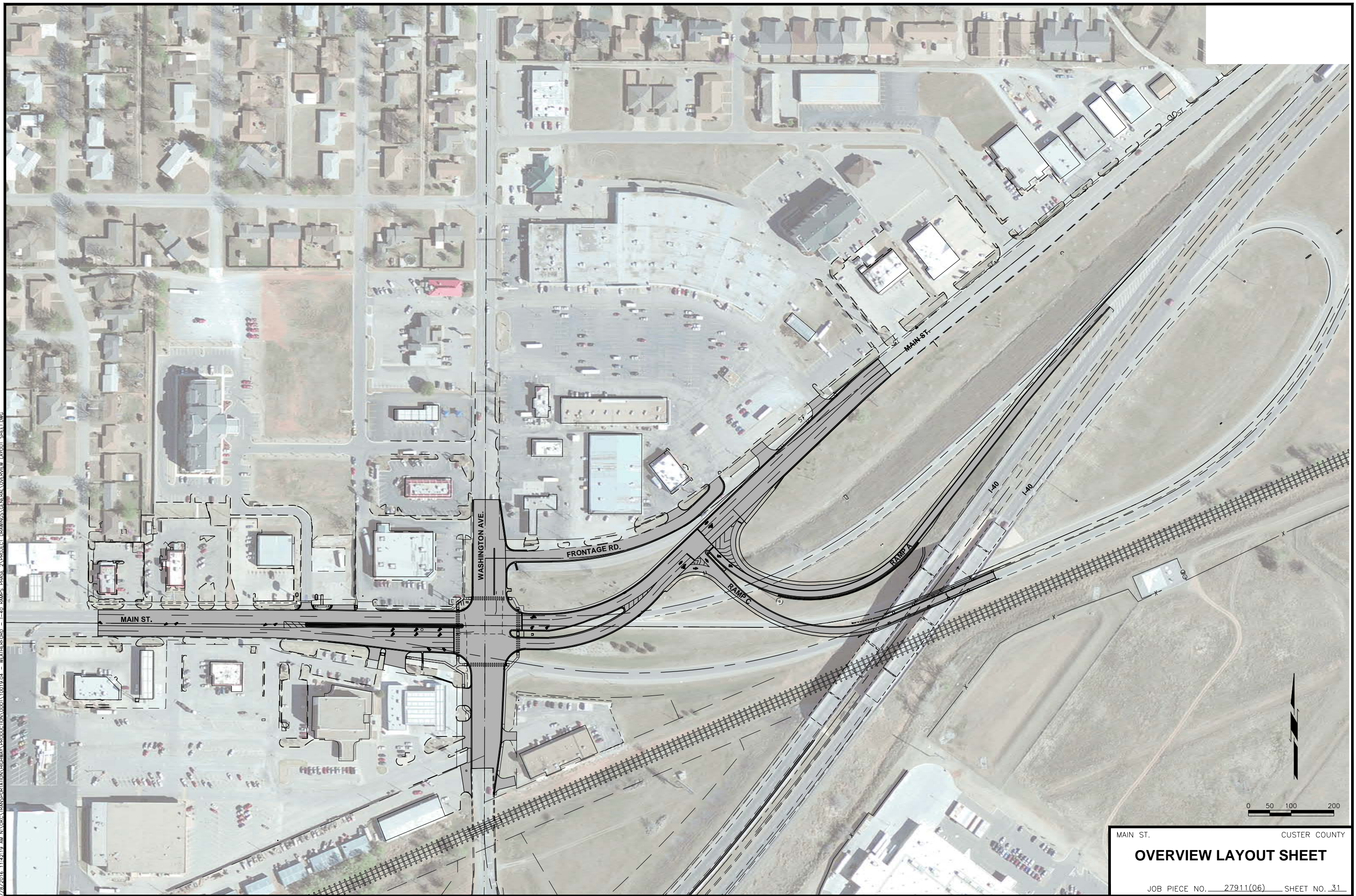
CRL RAMP A			
POINT NO.	STATION	NORTHING	EASTING
520	4412+16	192436.1470	1762470.3932
521	4411+67	192400.6346	1762504.3068
522	4406+71	192418.6821	1762942.5222
523	4402+32	192763.3528	1763214.1559
524	4397+99	193086.9638	1763501.2762

CL SURVEY RAMP A			
POINT NO.	STATION	NORTHING	EASTING
430	4379+64	192252.6784	1761957.5014
431	4381+72	192251.5121	1762165.0481
432	4390+13	192542.2505	1762934.2617
433	4391+33	192622.0682	1763023.7466

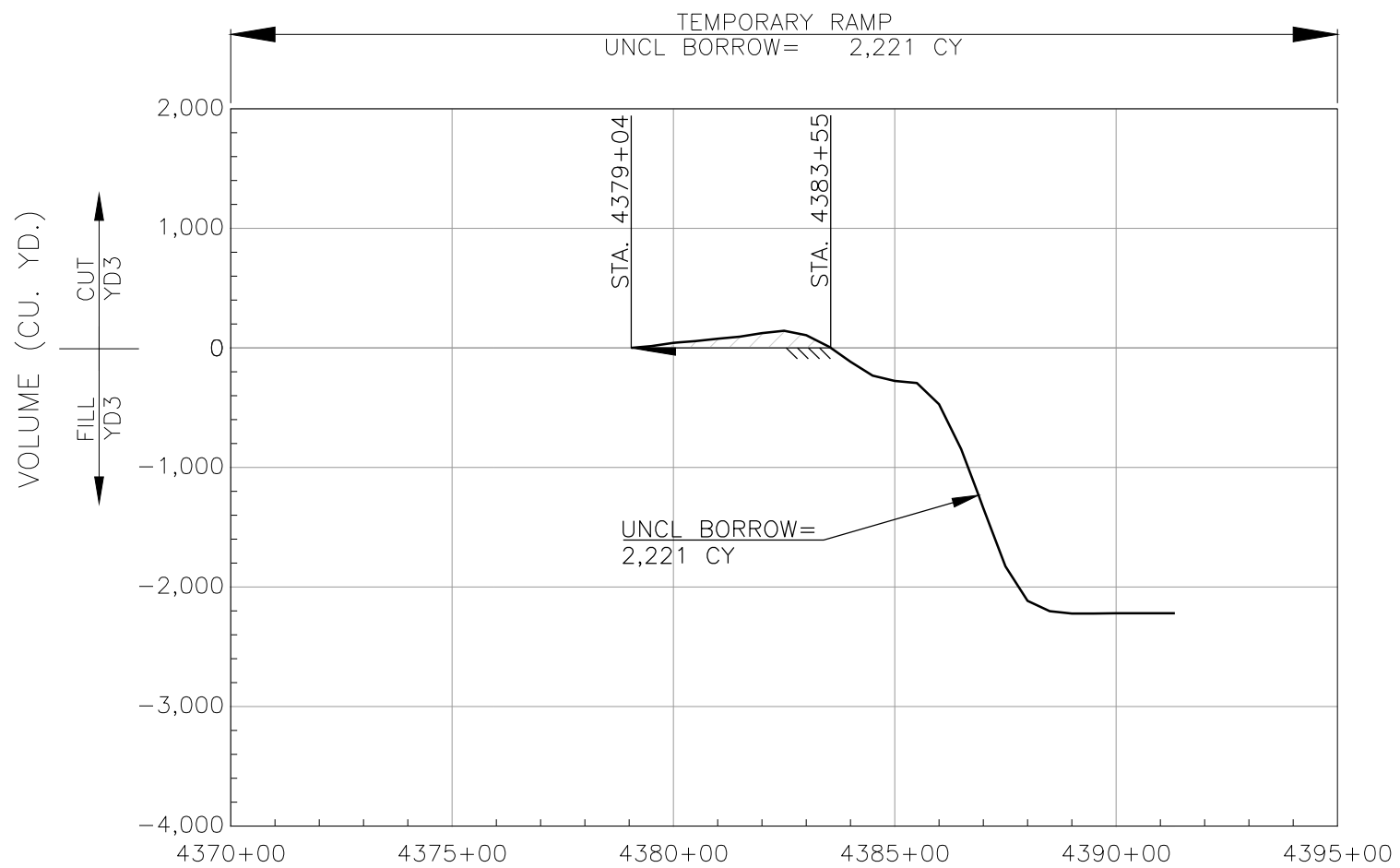
CRL FRONTAGE ROAD			
POINT NO.	STATION	NORTHING	EASTING
530	4379+64	192389.1266	1761957.9166
531	4381+21	192390.7760	1762114.6219
532	4384+41	192471.3753	1762415.7626
533	4384+87	192498.5661	1762452.2526
534	4385+54	192544.7486	1762500.6066
535	4385+84	192570.9008	1762494.8195

CL SURVEY US-66			
POINT NO.	STATION	NORTHING	EASTING
440	4378+71	192386.0377	1761957.9072
441	4380+49	192385.8091	1762136.4571
442	4384+41	192513.7410	1762498.0688
443	4394+00	193118.0453	1763243.2811

7/8/2016 11:42:19 AM N:\000\TRANSPORTATION\HIGHWAY\PRODUCTION\000A10019.04 - WEATHERFORD - I-40 RAMP PHASE 2 PROJECT DRAWINGS\GENERAL\OVERVIEW LAYOUT SHEET.DWG

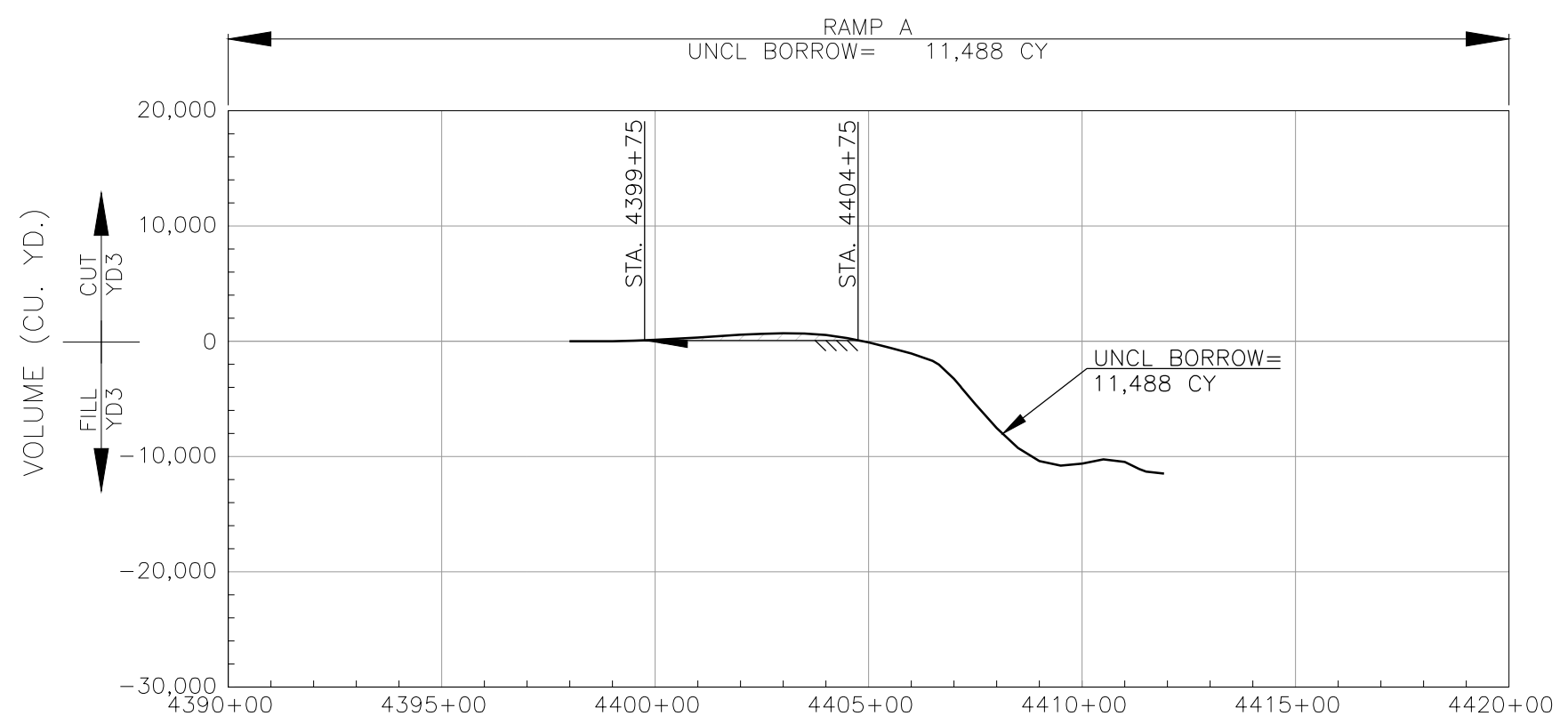


MAIN ST. CUSTER COUNTY
OVERVIEW LAYOUT SHEET
JOB PIECE NO. 27911(06) SHEET NO. 31



SCHEDULE OF EARTHWORK				
ALIGNMENT	UNCLASSIFIED EXCAVATION	EMBANKMENT +15%	UNCLASSIFIED BORROW	EXCESS EXCAVATION
	202(A) C.Y.	C.Y.	202(D) C.Y.	C.Y.
TEMP. RAMP	-	2,847.00	2,847.00	-
MAIN STREET	4,394.00	1,847.00		2,547.00
WASHINGTON AVE.	5,579.00	149.00		5,430.00
RAMP C	1,356.00	1,756.00	400.00	-
RAMP A*	8,934.00	23,343.00	23,343.00	8,934.00
FRONTAGE ROAD	214.00	9.00		205.00
TOTALS:	20,477.00	29,951.00	26,590.00	17,116.00

* UNCLASSIFIED EXCAVATION MAY BE UNSUITABLE FOR EMBANKMENT.

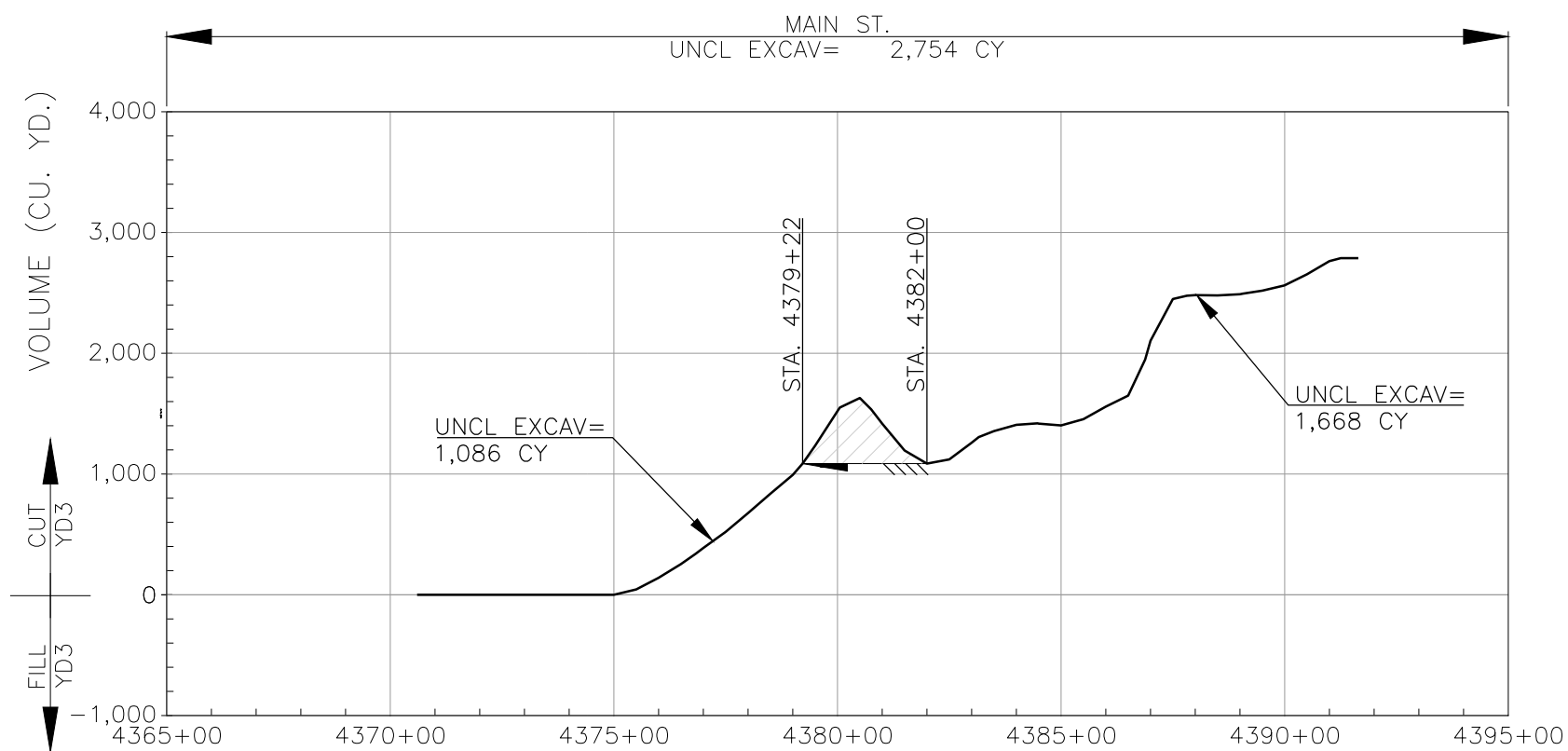
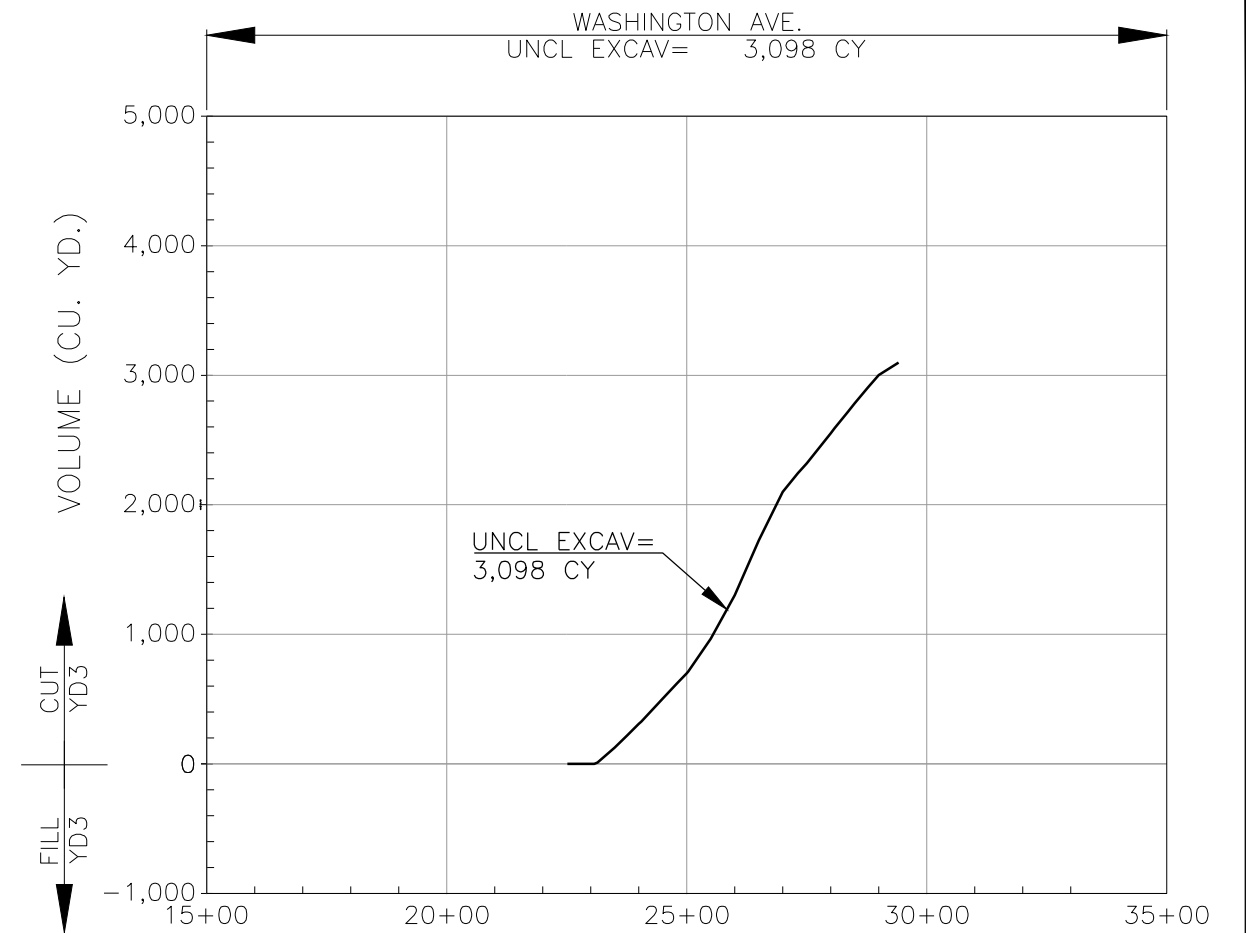
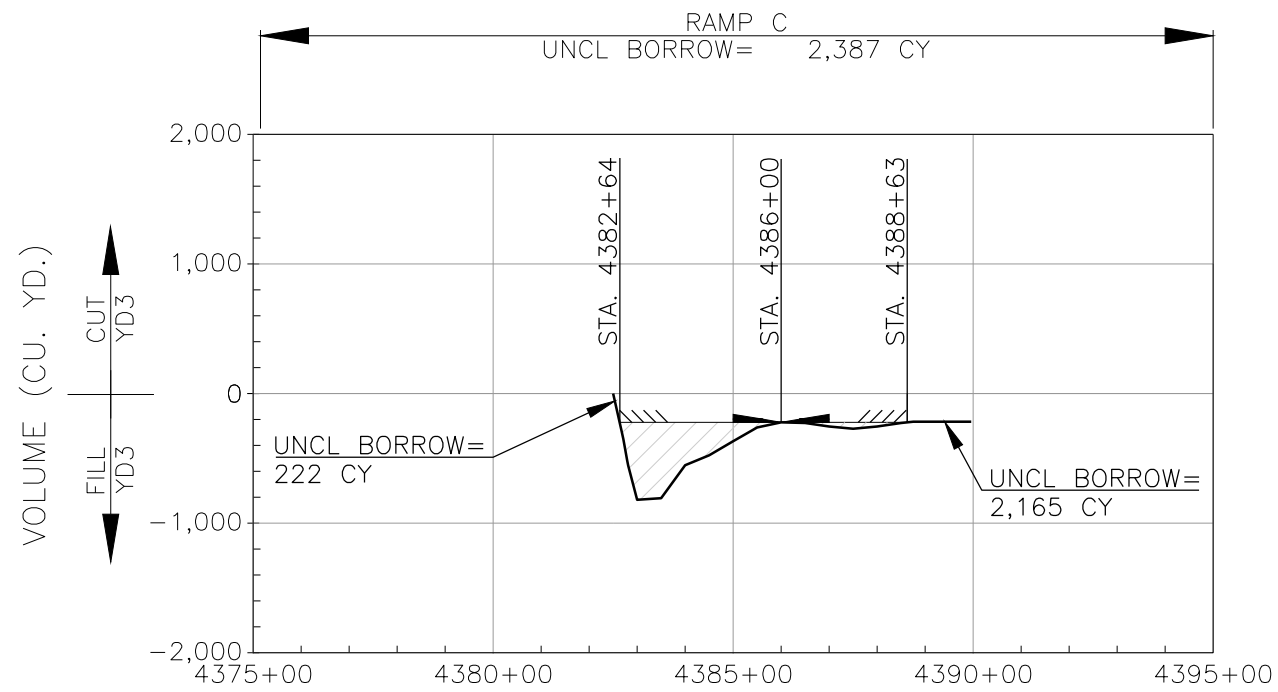


MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTION FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.

WEATHERFORD I40 RAMPS PHASE 2 CUSTER COUNTY

**MASS HAUL DIAGRAMS
SHEET 1 OF 2**

JOB PIECE NO. 27911(06) SHEET NO. 32



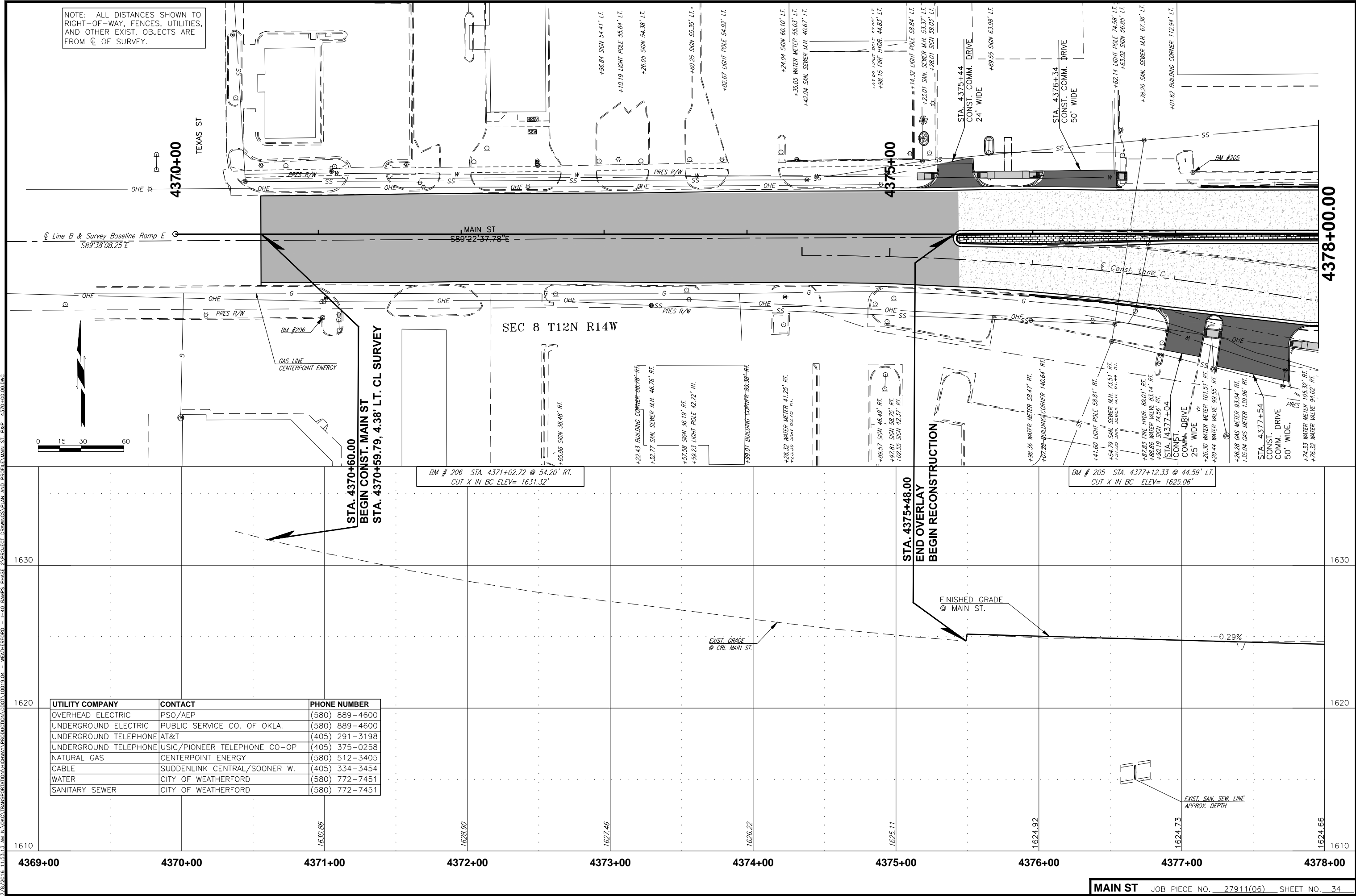
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTION FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.

WEATHERFORD I40 RAMPS PHASE 2 CUSTER COUNTY

**MASS HAUL DIAGRAMS
SHEET 2 OF 2**

JOB PIECE NO. 27911(06) SHEET NO. 33

NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM CL OF SURVEY.



7/8/2016 11:55:13 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\04 - WEATHERFORD - L40 - RAMP5 PHASE 2\PROJECT DRAWINGS\PLAN AND PROFILE\MAIN ST - P&P - 4370-00.DWG

UTILITY COMPANY	CONTACT	PHONE NUMBER
OVERHEAD ELECTRIC	PSO/AEP	(580) 889-4600
UNDERGROUND ELECTRIC	PUBLIC SERVICE CO. OF OKLA.	(580) 889-4600
UNDERGROUND TELEPHONE	AT&T	(405) 291-3198
UNDERGROUND TELEPHONE	USIC/PIONEER TELEPHONE CO-OP	(405) 375-0258
NATURAL GAS	CENTERPOINT ENERGY	(580) 512-3405
CABLE	SUDDENLINK CENTRAL/SOONER W.	(405) 334-3454
WATER	CITY OF WEATHERFORD	(580) 772-7451
SANITARY SEWER	CITY OF WEATHERFORD	(580) 772-7451

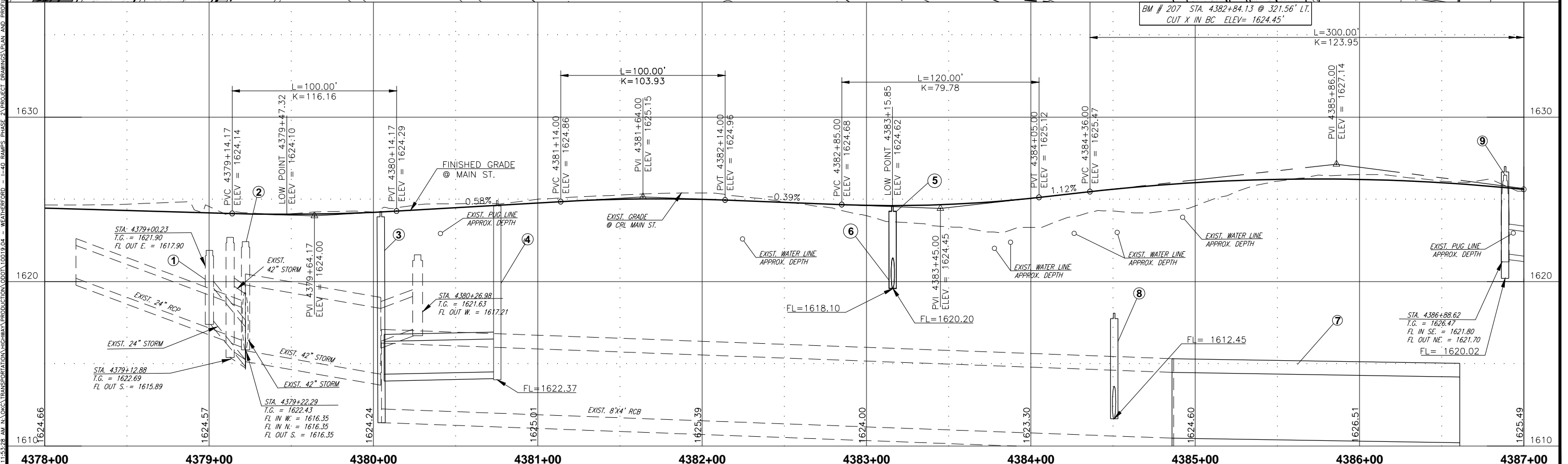
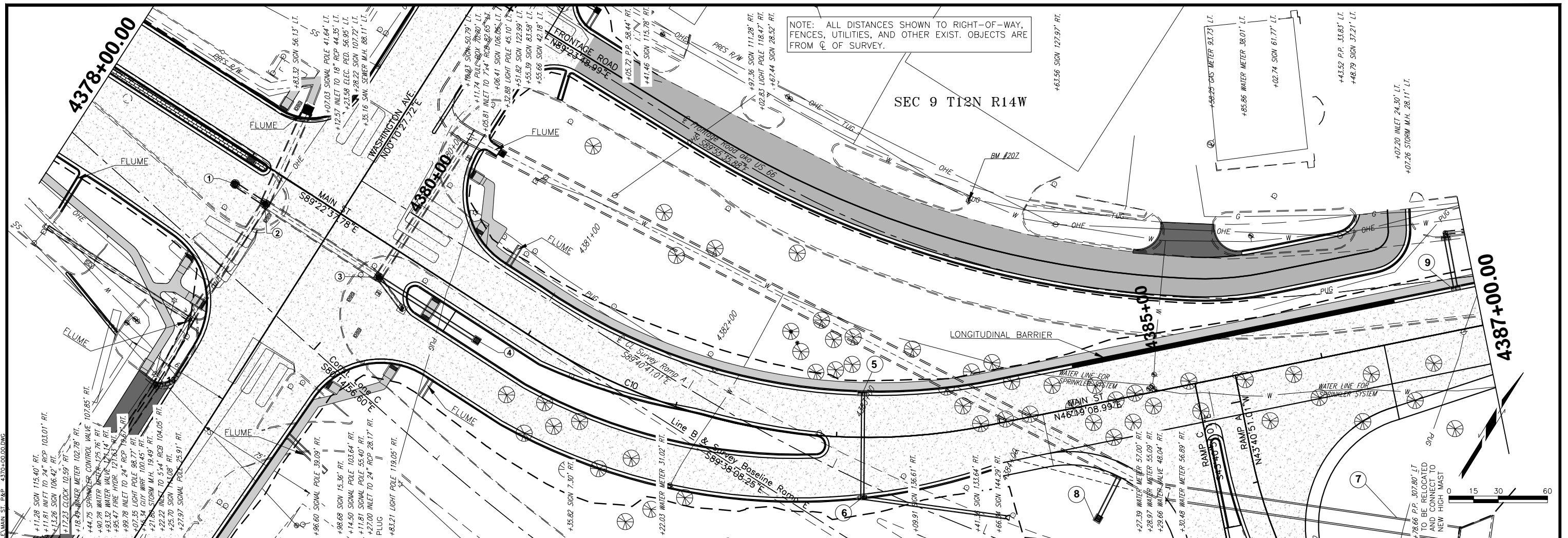
STA. 4370+60.00
BEGIN CONST. MAIN ST
STA. 4370+59.79, 4.38' LT. CL SURVEY

BM # 206 STA. 4371+02.72 @ 54.20' RT.
CUT X IN BC ELEV= 1631.32'

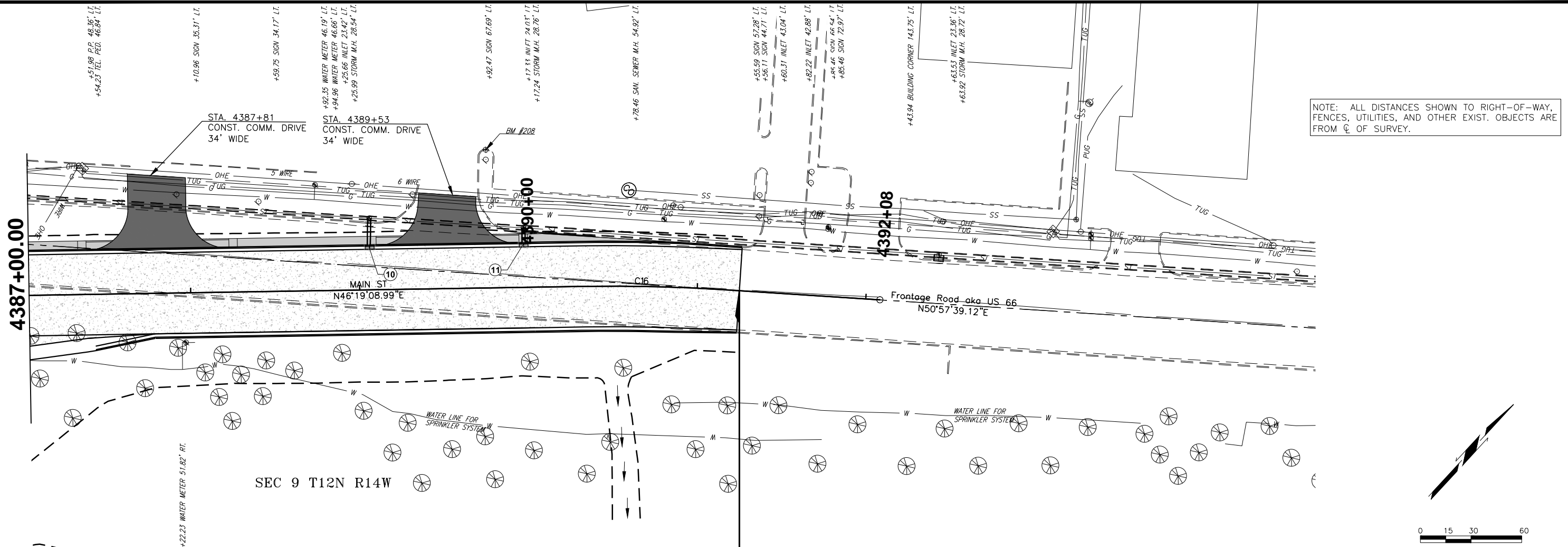
STA. 4375+48.00
END OVERLAY
BEGIN RECONSTRUCTION

BM # 205 STA. 4377+12.33 @ 44.59' LT.
CUT X IN BC ELEV= 1625.06'

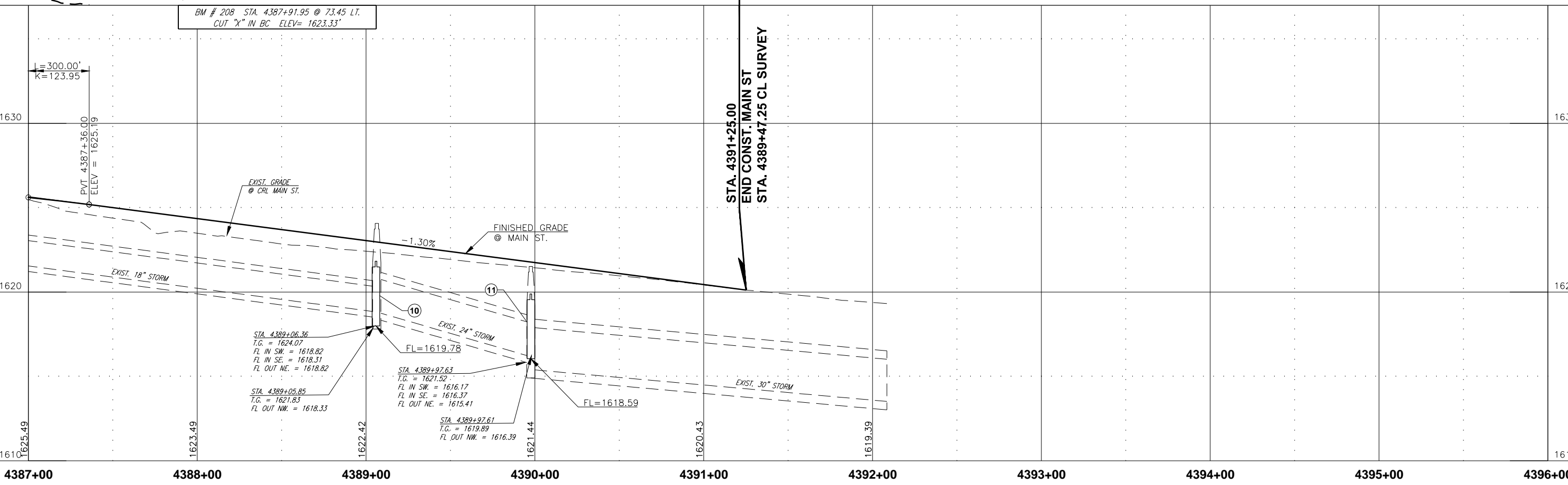
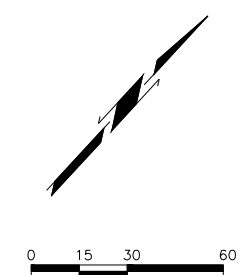
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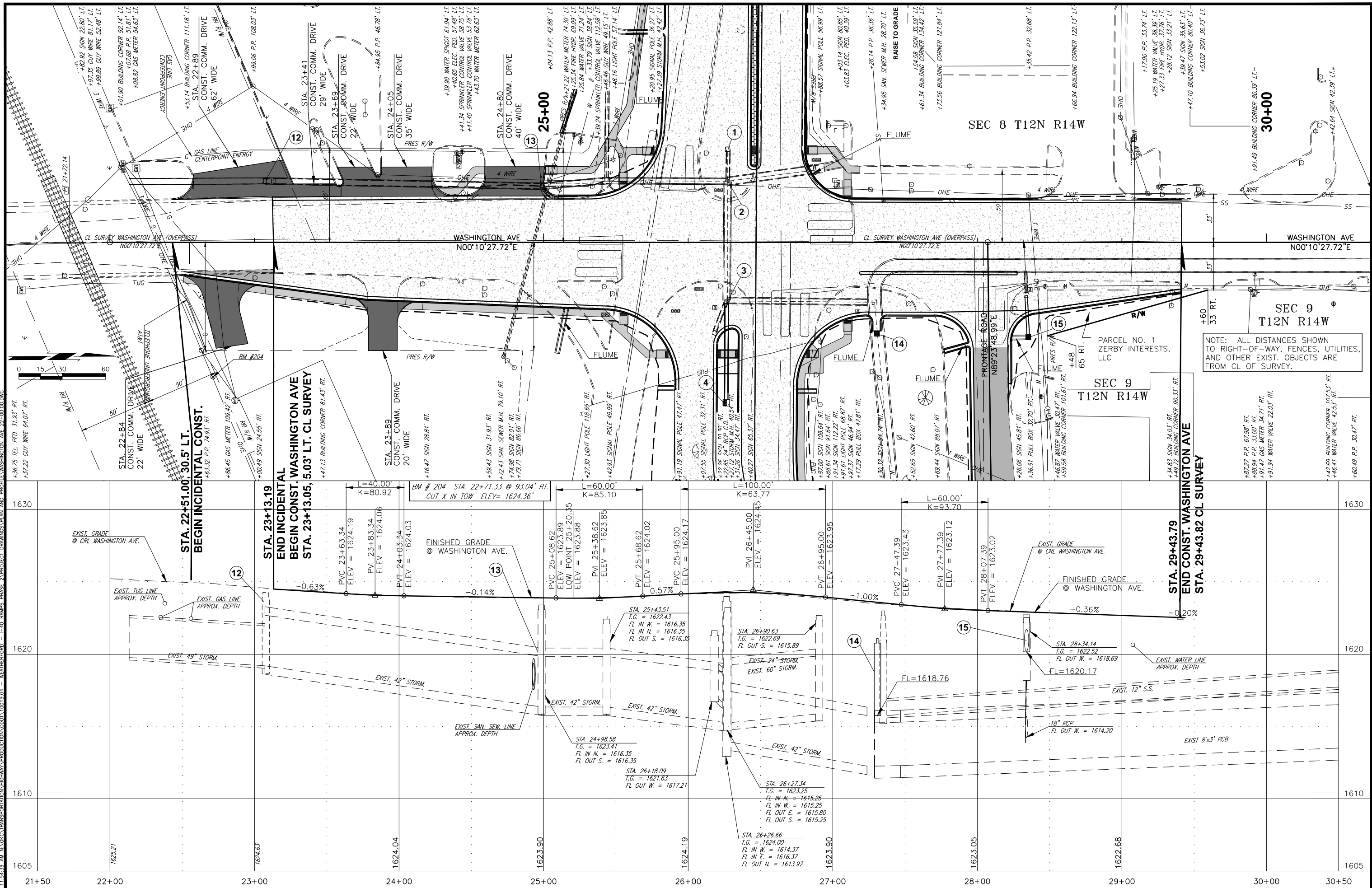
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NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM C/L OF SURVEY.

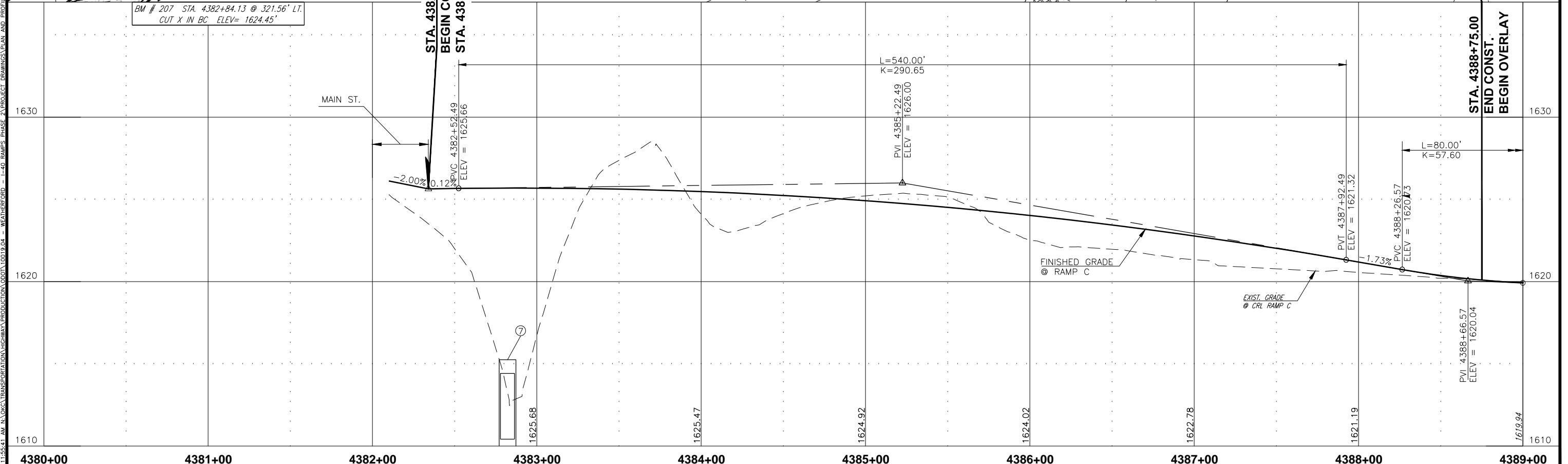
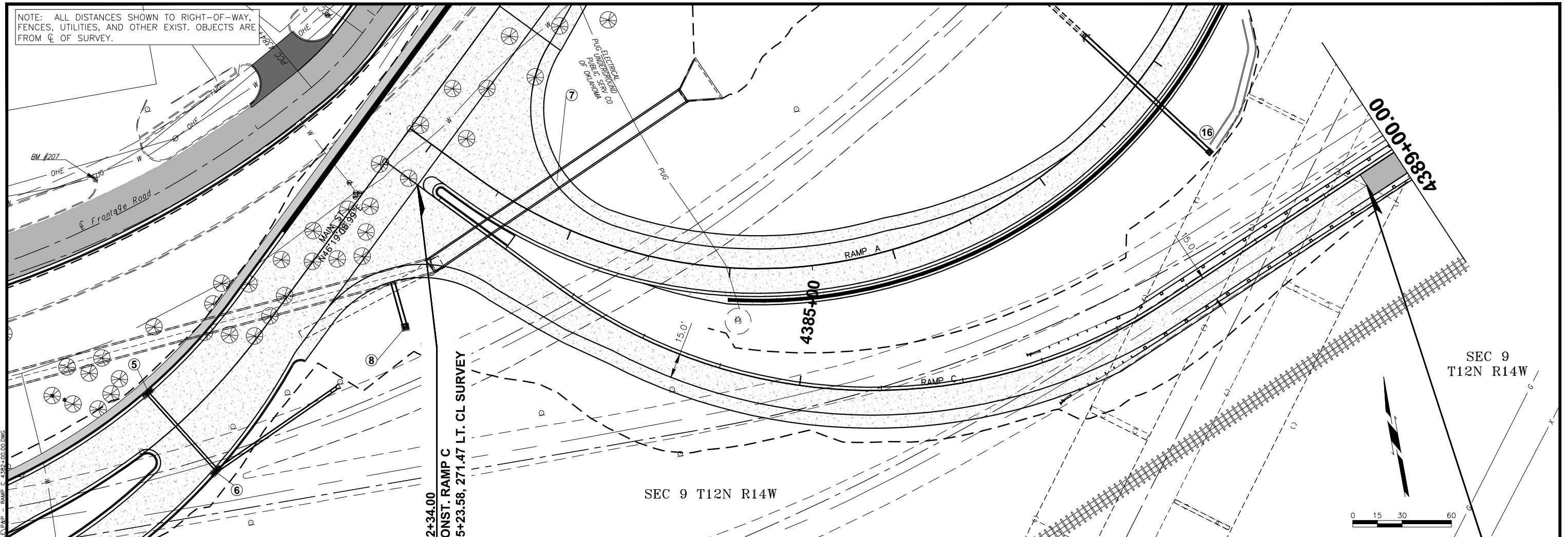


7/8/2016 11:56:39 AM N:\KOKC\TRANSPORTATION\HIGHWAY\PRODUCTION\DOTA10019.04 - WEATHERED - 1-40 RAMP, PHASE 2 PROJECT DRAWINGS\PLAN AND PROFILE\WASHINGTON_AVE_22+00.00.DWG



NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM CL OF SURVEY.

NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM ϕ OF SURVEY.



NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM C OF SURVEY.

4389+00.00

4390+00

SEC 9
T12N R14W

0 15 30 60

BM # 211 STA. 4392+14.29 @ 29.98' RT.
SET 3/8" IRON PIN W/CAP ELEV= 1618.11"

BM # 212 STA. 4397+92.53 @ 70.69' RT.
SET 3/8" IRON PIN W/CAP ELEV= 1609.69"

STA. 4389+95.00
END CONST. RAMP C
STA. 4392+00.00, 0.0' RT. CL SURVEY

EXIST. GRADE
@ CRL RAMP C

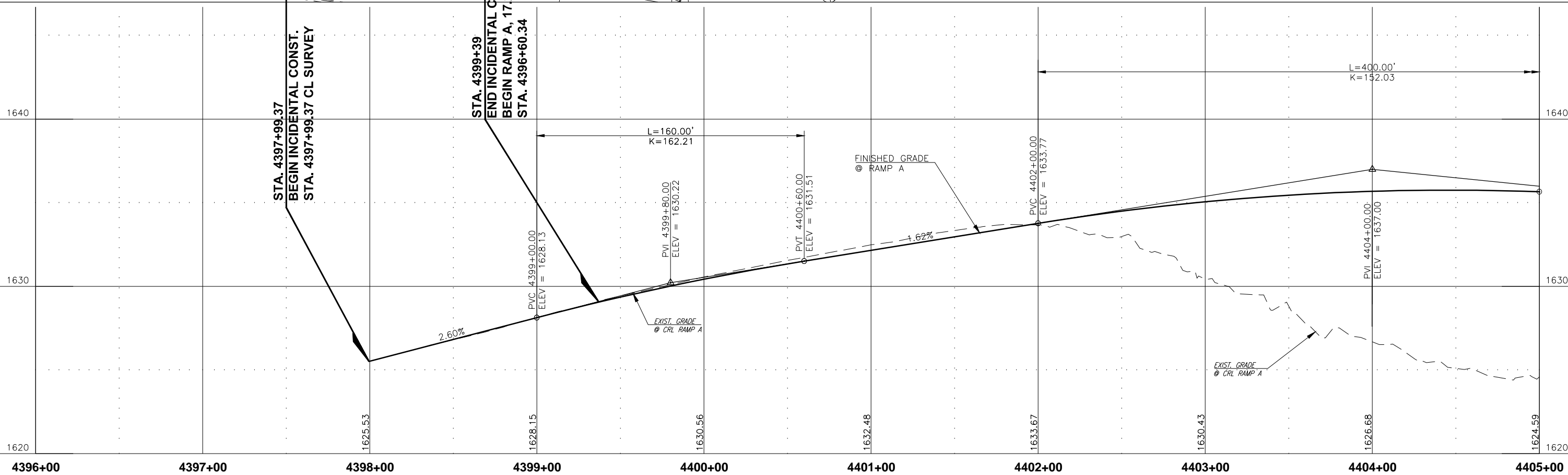
GAS LINE CENTERPOINT ENERGY

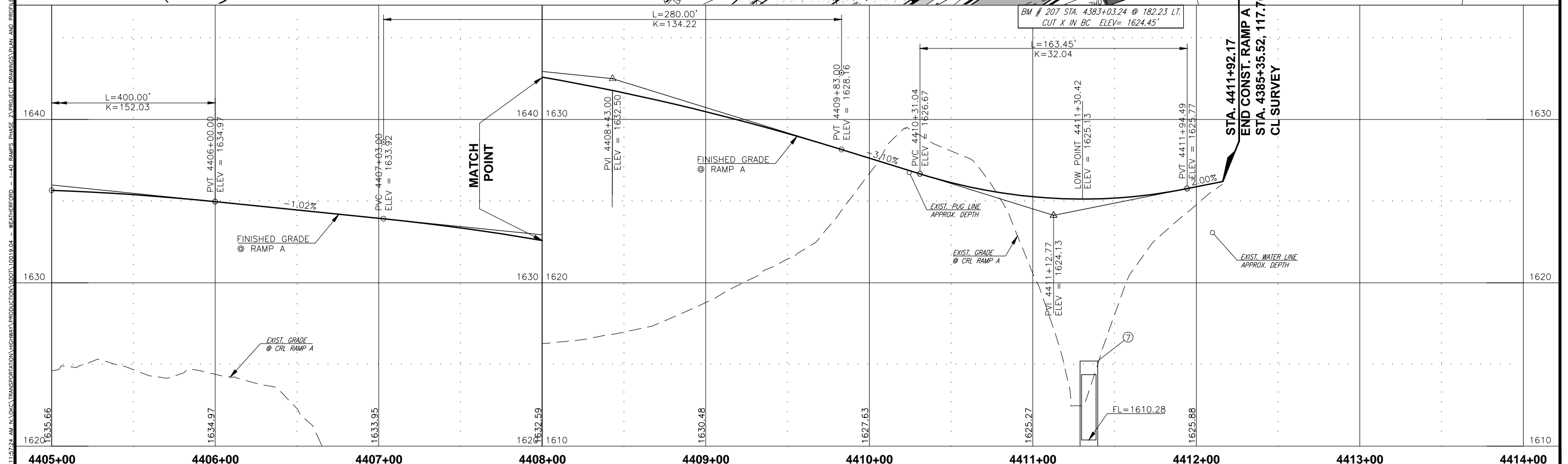
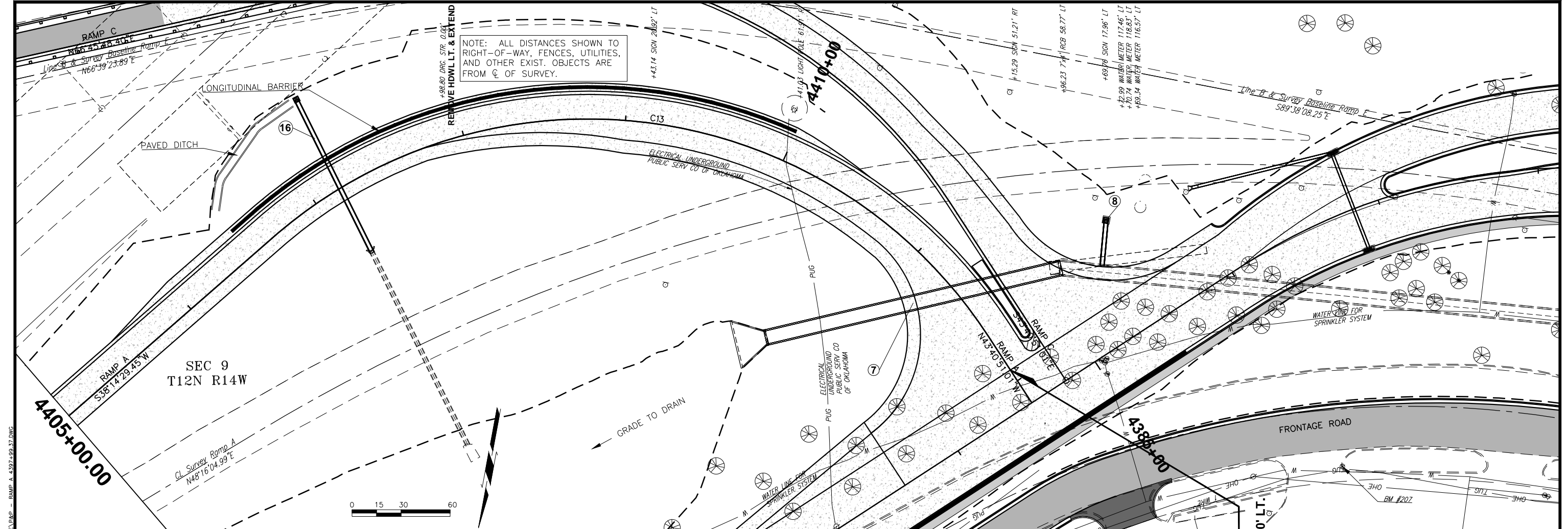
1610 1610 1610 1610 1610 1610 1610 1610 1610 1610

4389+00 4390+00 4391+00 4392+00 4393+00 4394+00 4395+00 4396+00 4397+00 4398+00

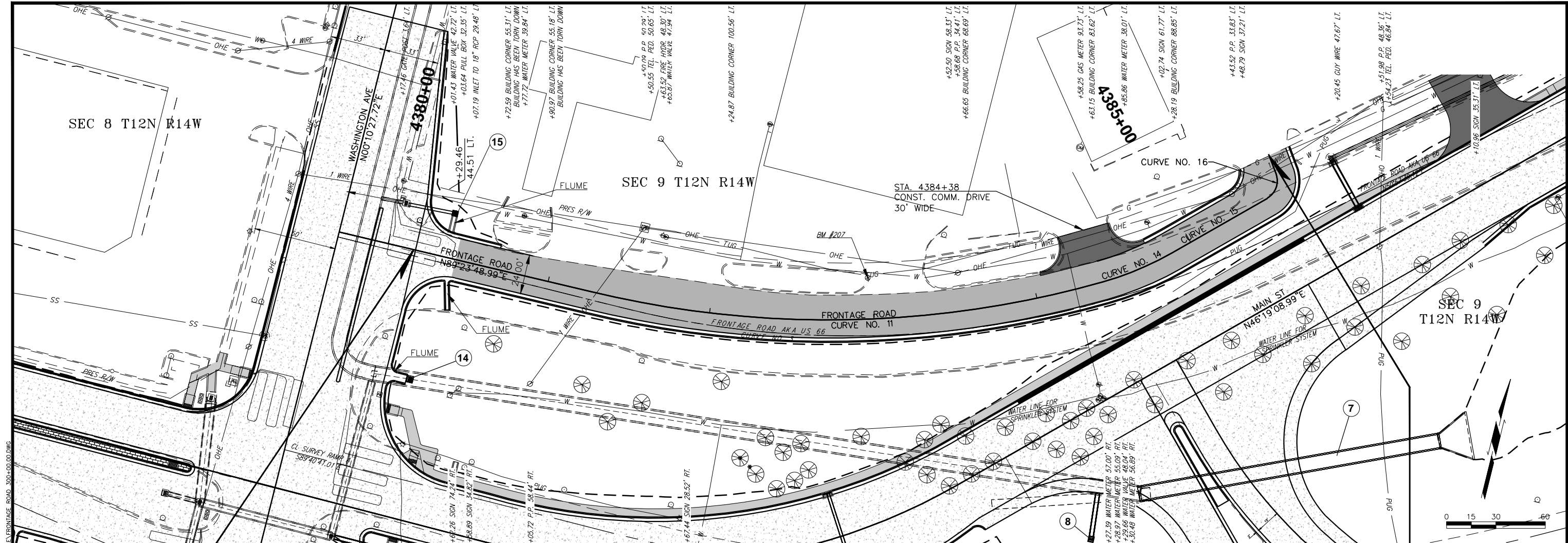
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7/28/2016 11:57:06 AM NA/CIVIL/TRANSPORTATION/HIGHWAY/PRODUCTION/0001/1010/04 - WEATHERFORD - I-40 RAMP A - PHASE 2 - PROJECT DRAWINGS/PLAN AND PROFILE/PLAN - RAMP A 4397+99.37.DWG



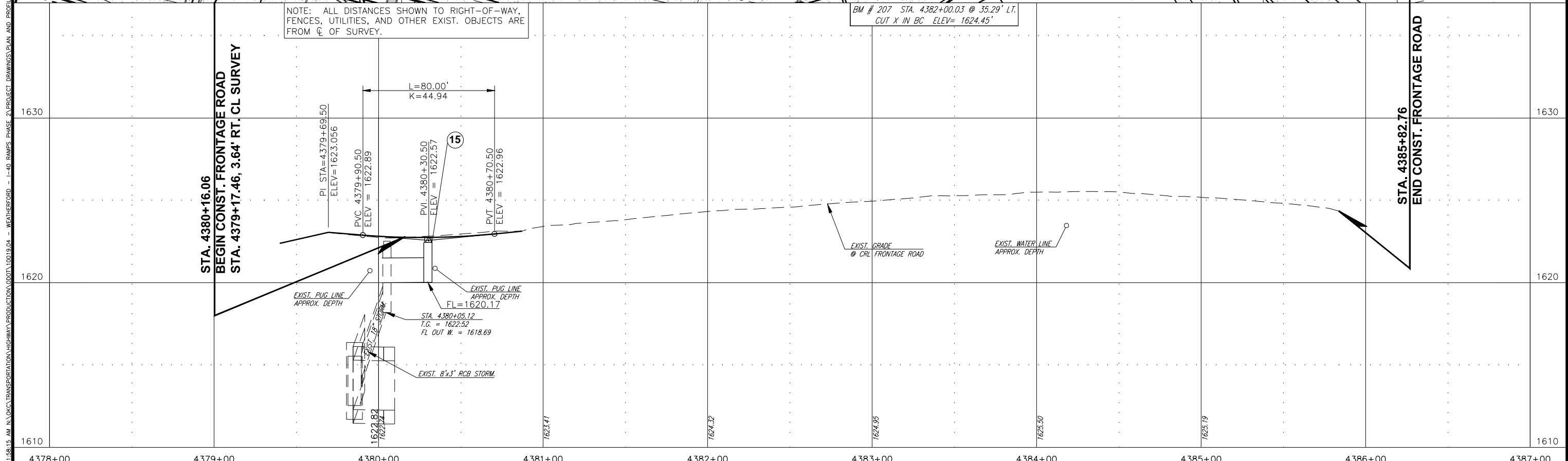


7/28/2016 11:52:24 AM NA/CIVIL/TRANSPORTATION/INCH/PRODUCTION/0001/0404 - WEATHERFORD - L-40 RAMP A PHASE 2 PROJECT DRAWINGS/PLAN AND PROFILE/06 - RAMP A 4397+00.00 TO 4405+00.00



NOTE: ALL DISTANCES SHOWN TO RIGHT-OF-WAY, FENCES, UTILITIES, AND OTHER EXIST. OBJECTS ARE FROM ϕ OF SURVEY.

BM # 207 STA. 4382+00.03 @ 35.29' LT.
CUT X IN BC ELEV= 1624.45'

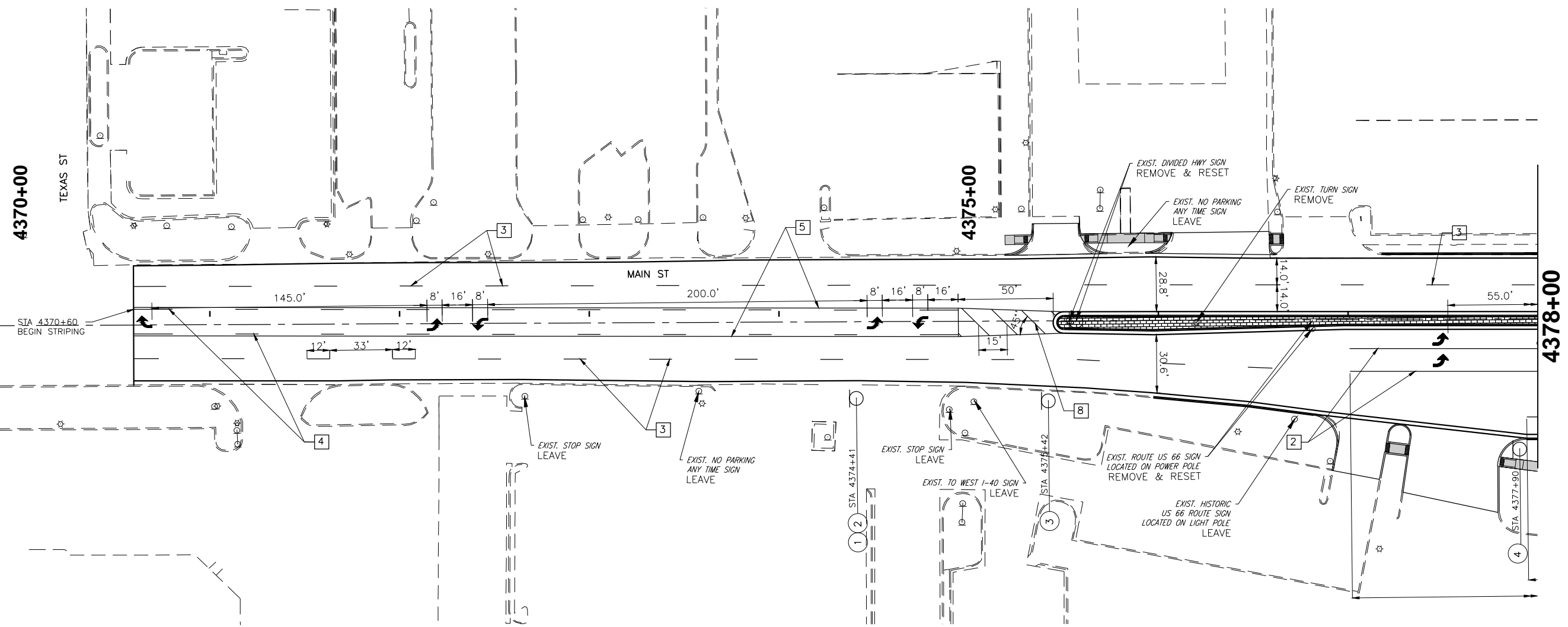


STA. 4380+16.06
BEGIN CONST. FRONTAGE ROAD
STA. 4379+17.46, 3.64' RT. CL SURVEY

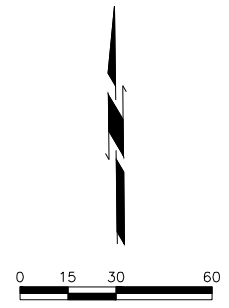
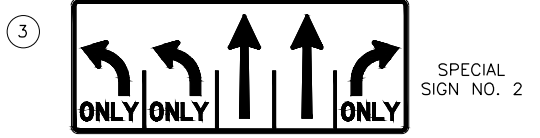
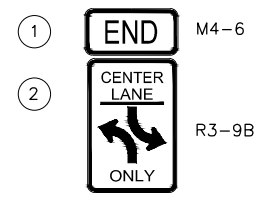
STA. 4385+82.76
END CONST. FRONTAGE ROAD

7/8/2016 11:58:15 AM N:\KOC\TRANSPORTATION\HIGHWAY\PRODUCTION\DOTA\10019.04 - WEATHERFORD - I-40 RAMP\PHASE 2\PROJECT DRAWINGS\PLAN AND PROFILE\FRONTAGE ROAD_300-400.DWG

7/28/2016 11:59:01 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\001.DWG - WEATHERFORD - L-40 RAMP'S PHASE 2\PROJECT DRAWINGS\SIGNING & STRIPING\MAIN ST. SIGNING & STRIPING.DWG

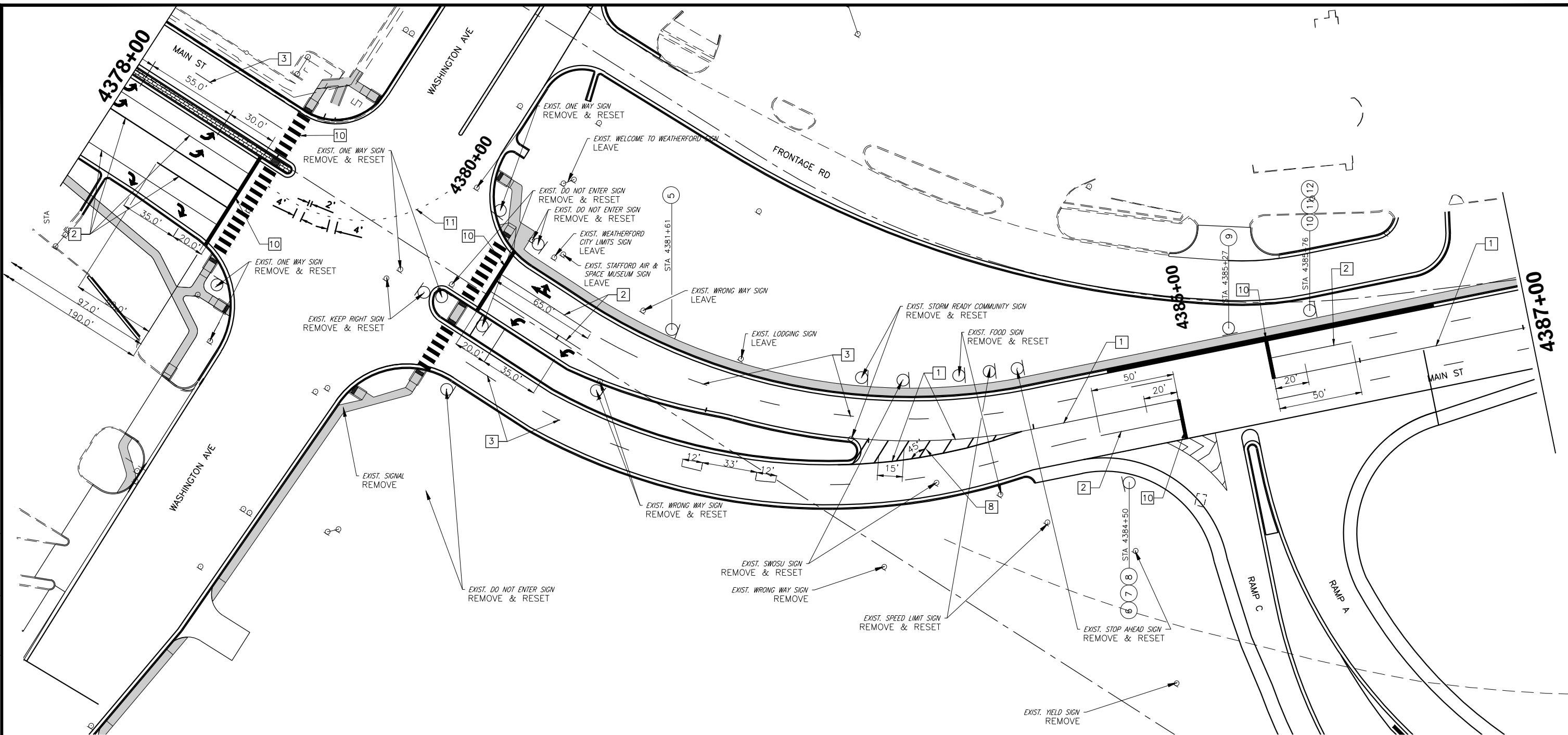


LEGEND	
1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN

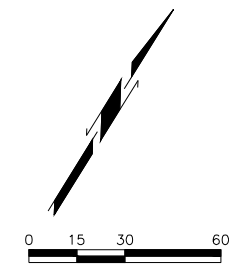
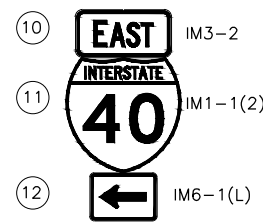
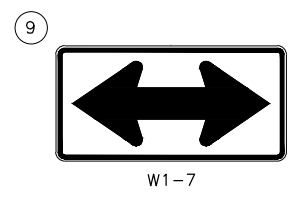
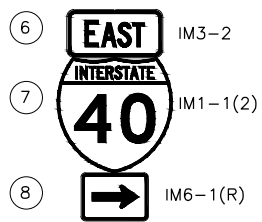
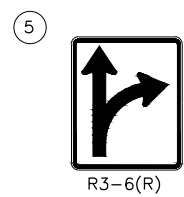


MAIN ST. CUSTER COUNTY
SIGNING AND STRIPING
SHEET 1 OF 10
 JOB PIECE NO. 27911(06) SHEET NO. 43

7/28/2016 11:59:09 AM NA/CIVIL/TRANSPORTATION/HIGHWAY/PRODUCTION/0001/LOD1/04 - WEATHERFORD - L-40 RAMP PHASE 2/PROJECT DRAWINGS/SIGNING & STRIPING/MAIN ST SIGNING & STRIPING.DWG

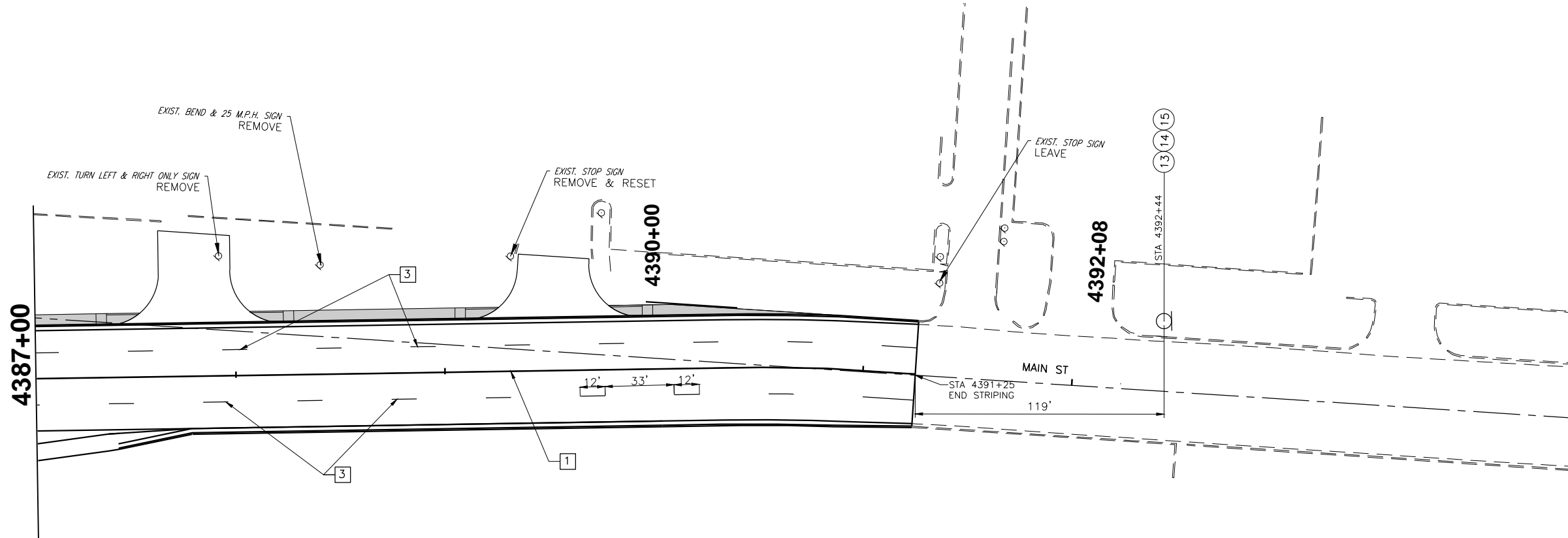


LEGEND	
1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	4" DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN



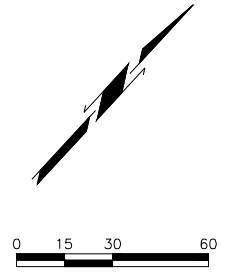
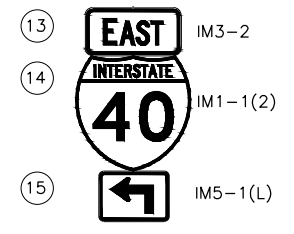
MAIN ST. CUSTER COUNTY
SIGNING AND STRIPING
SHEET 2 OF 10
 JOB PIECE NO. 27911(06) SHEET NO. 44

7/28/2016 11:59:18 AM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\001.DWG - WEATHERFORD - I-40 RAMP'S PHASE 2\PROJECT DRAWINGS\SIGNING & STRIPING\MAIN SET - SIGNING & STRIPING.DWG



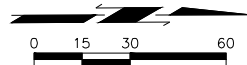
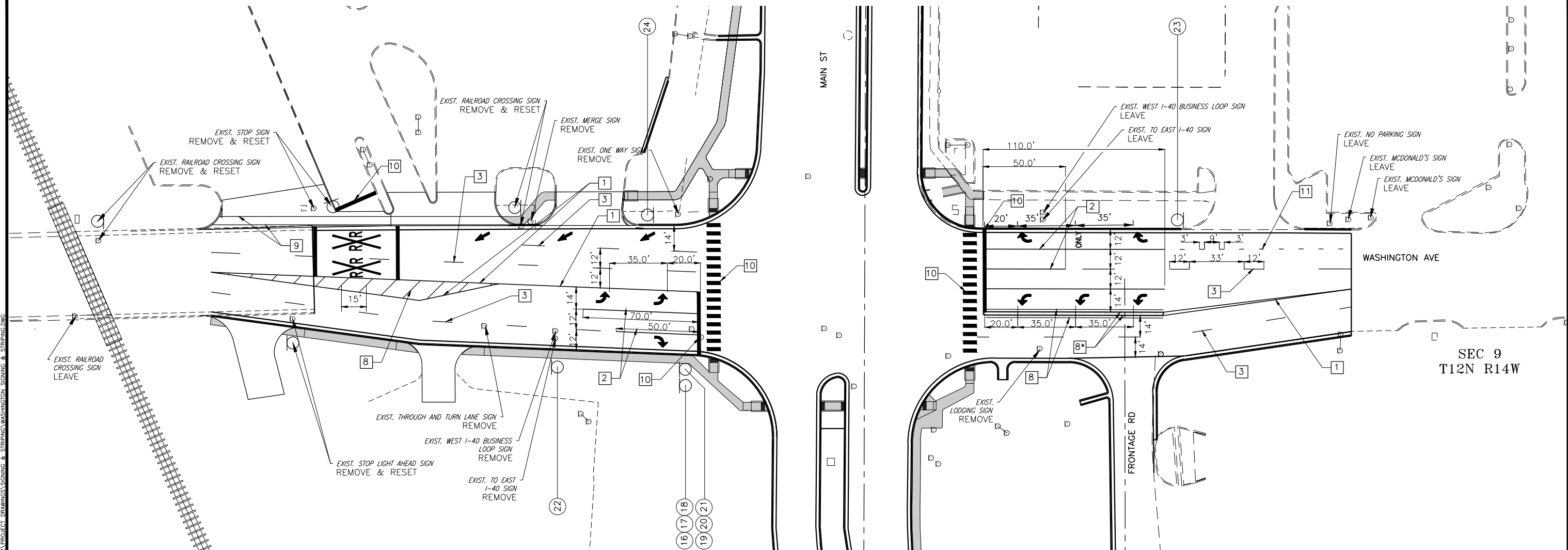
LEGEND

1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN

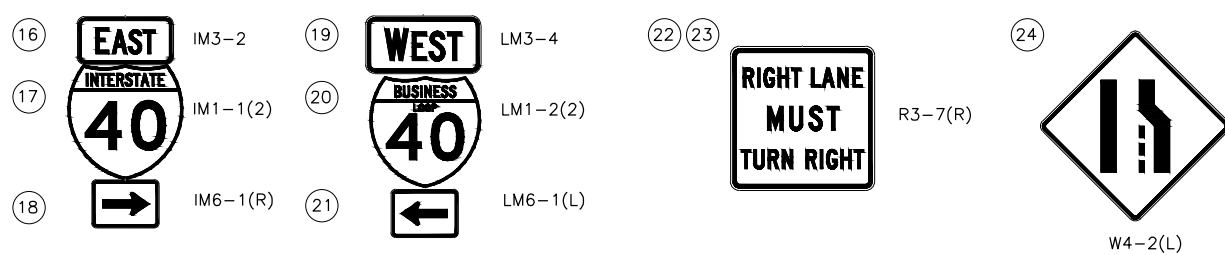


MAIN ST. CUSTER COUNTY
SIGNING AND STRIPING
SHEET 3 OF 10
 JOB PIECE NO. 27911(06) SHEET NO. 45

7/28/2016 12:00:09 PM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\101910_04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\SIGNING & STRIPING.DWG

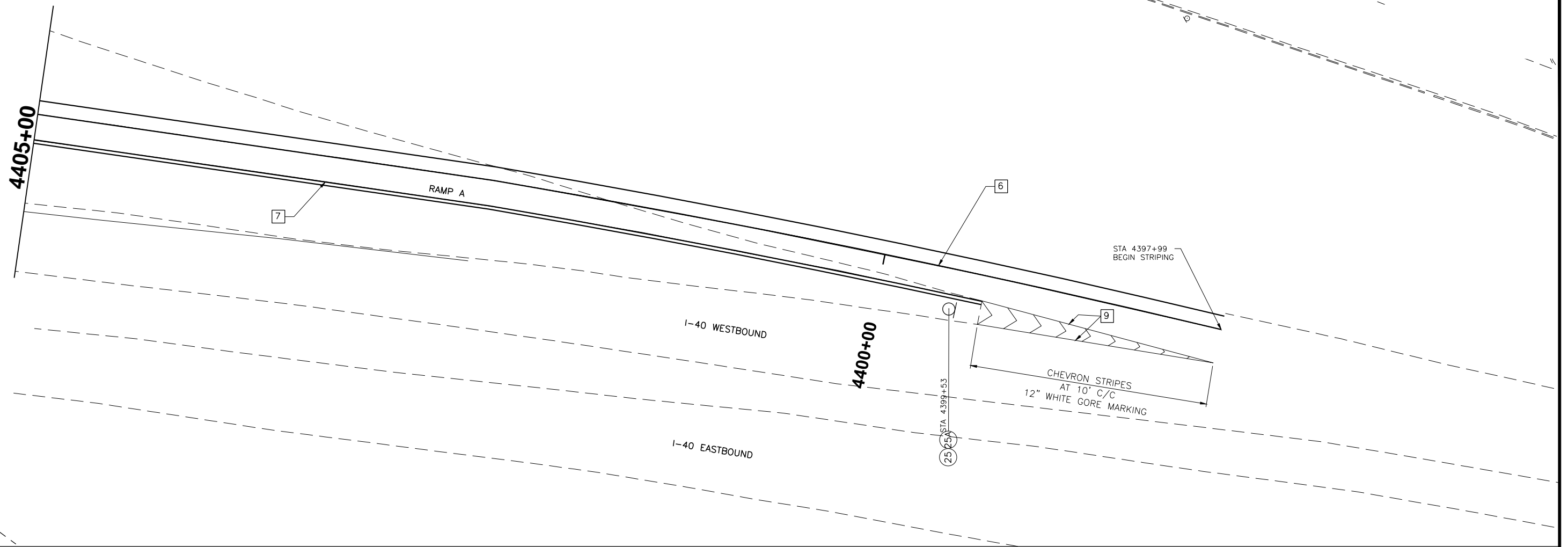


LEGEND	
1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN



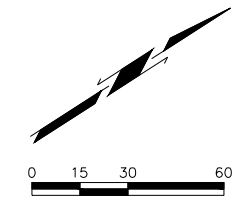
* PAINT CURB ALONG DIVIDING STRIPE WITH SOLID YELLOW LINE (PLASTIC)

7/28/2016 12:01:03 PM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\1019\04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\SIGNING & STRIPING\RAMP A SIGNING & STRIPING.DWG



LEGEND

1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN

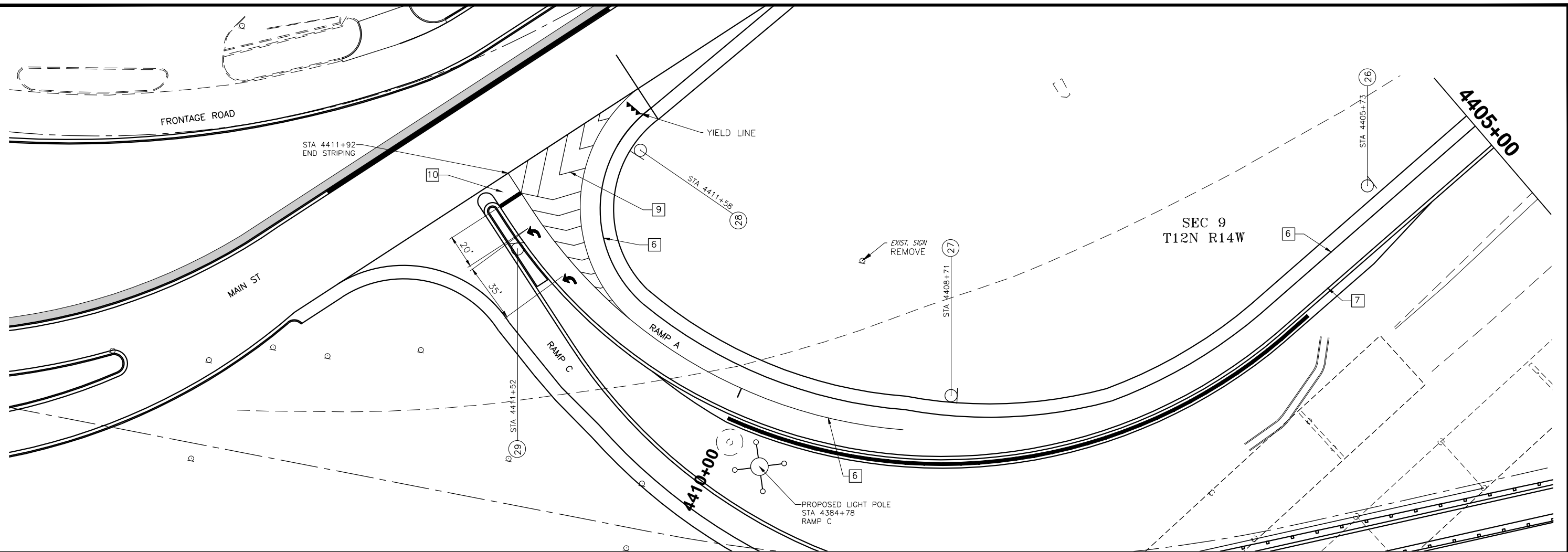


MAIN ST. CUSTER COUNTY

**SIGNING AND STRIPING
SHEET 5 OF 10**

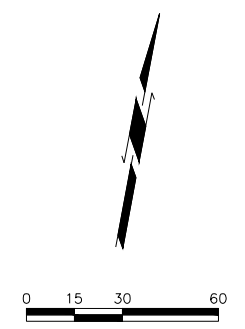
JOB PIECE NO. 27911(06) SHEET NO. 47

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LEGEND

1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN

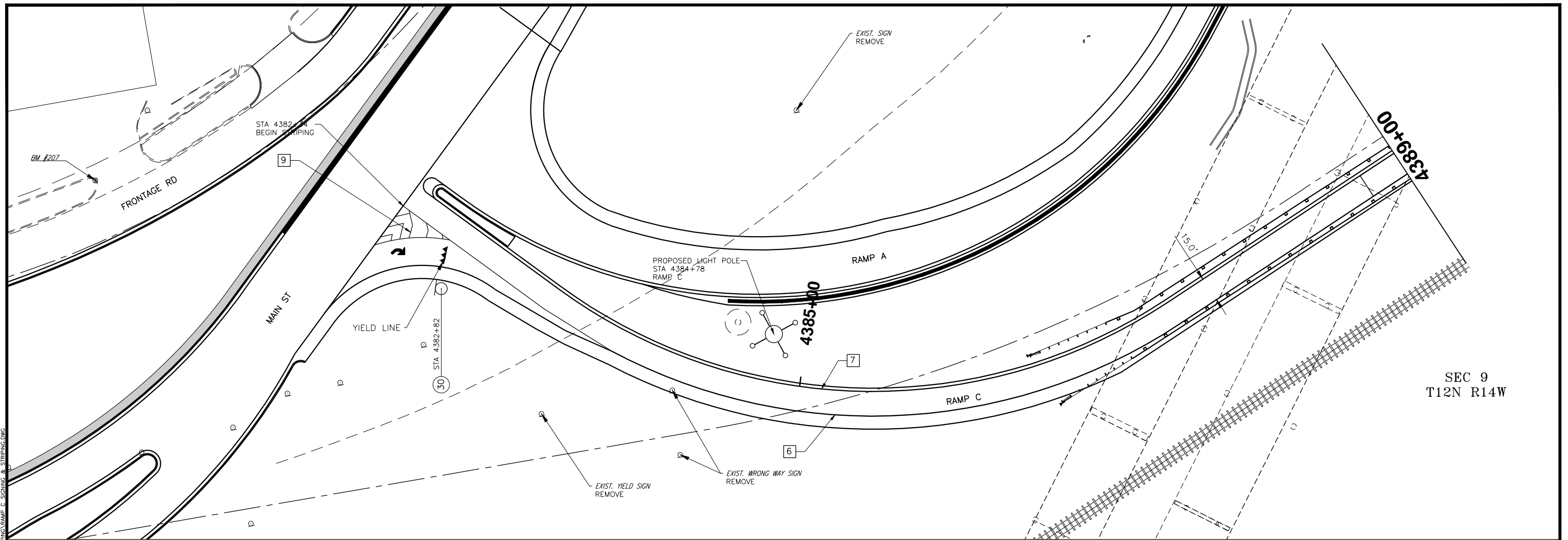


MAIN ST. CUSTER COUNTY

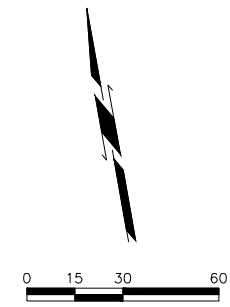
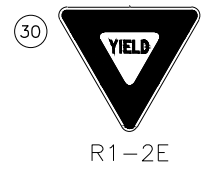
**SIGNING AND STRIPING
SHEET 6 OF 10**

JOB PIECE NO. 27911(06) SHEET NO. 48

7/28/2016 12:02:07 PM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\1019104 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\SIGNING & STRIPING\RAMP C SIGNING & STRIPING.DWG



SEC 9
T12N R14W



LEGEND

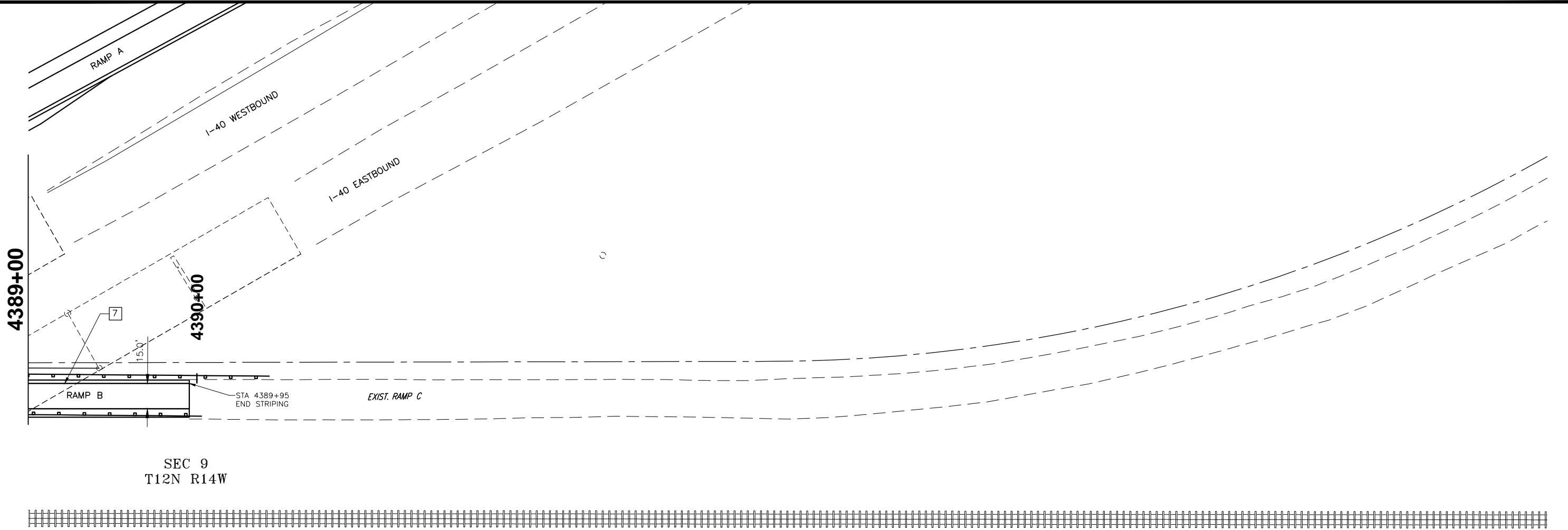
1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN

MAIN ST. CUSTER COUNTY

**SIGNING AND STRIPING
SHEET 7 OF 10**

JOB PIECE NO. 27911(06) SHEET NO. 49

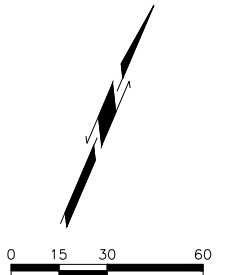
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SEC 9
T12N R14W

LEGEND

1	4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
2	4" SOLID WHITE LINE (PLASTIC)
3	4" DASHED WHITE LANE LINE (PLASTIC)
4	4" DASHED YELLOW LANE LINE (PLASTIC)
5	4" SOLID YELLOW BARRIER LINE (PLASTIC)
6	6" SOLID WHITE LINE (PLASTIC)
7	6" SOLID YELLOW EDGE LINE (PLASTIC)
8	8" SOLID YELLOW LINE (PLASTIC)
9	8" SOLID WHITE LINE (PLASTIC)
10	24" SOLID WHITE LINE (PLASTIC)
11	DOTTED WHITE LANE LINE (PLASTIC)
XX	PROPOSED SIGN

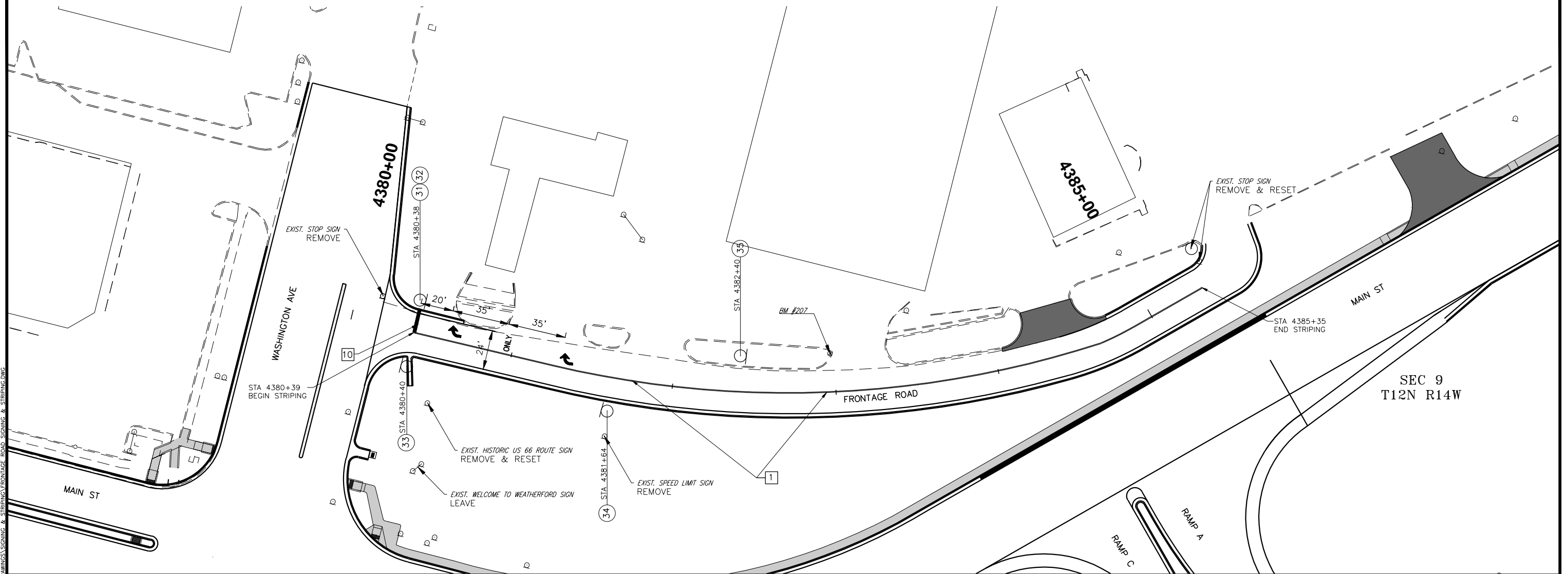


MAIN ST. CUSTER COUNTY

**SIGNING AND STRIPING
SHEET 8 OF 10**

JOB PIECE NO. 27911(06) SHEET NO. 50

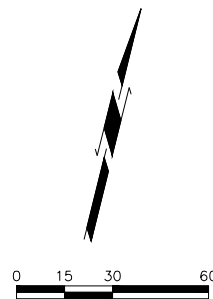
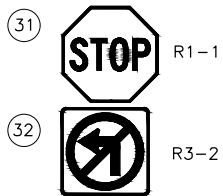
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SEC 9
T12N R14W

LEGEND

- 1 4" SOLID DOUBLE YELLOW BARRIER LINE (PLASTIC)
- 2 4" SOLID WHITE LINE (PLASTIC)
- 3 4" DASHED WHITE LANE LINE (PLASTIC)
- 4 4" DASHED YELLOW LANE LINE (PLASTIC)
- 5 4" SOLID YELLOW BARRIER LINE (PLASTIC)
- 6 6" SOLID WHITE LINE (PLASTIC)
- 7 6" SOLID YELLOW EDGE LINE (PLASTIC)
- 8 8" SOLID YELLOW LINE (PLASTIC)
- 9 8" SOLID WHITE LINE (PLASTIC)
- 10 24" SOLID WHITE LINE (PLASTIC)
- 11 DOTTED WHITE LANE LINE (PLASTIC)
- XX PROPOSED SIGN



MAIN ST. CUSTER COUNTY

**SIGNING AND STRIPING
SHEET 9 OF 10**

JOB PIECE NO. 27911(06) SHEET NO. 51

7/28/2016 12:04:17 PM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\10019.04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\SIGNING & STRIPING\1-40 SIGNING & STRIPING.DWG



LEGEND

(XX) PROPOSED SIGN

(36) **EXIT 82**
Washington Ave
E. Main St
1 MILE

SPECIAL SIGN NO. 7

(37) **EXIT 82**
Washington Ave ↗
E. Main St

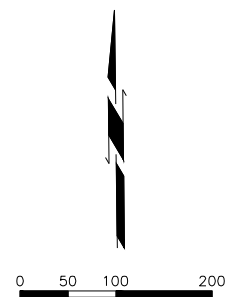
SPECIAL SIGN NO. 8

(38) ← **E. Main St**

SPECIAL SIGN NO. 9

(39) ← **Washington Ave**
 ← **E. Main St**

SPECIAL SIGN NO. 10

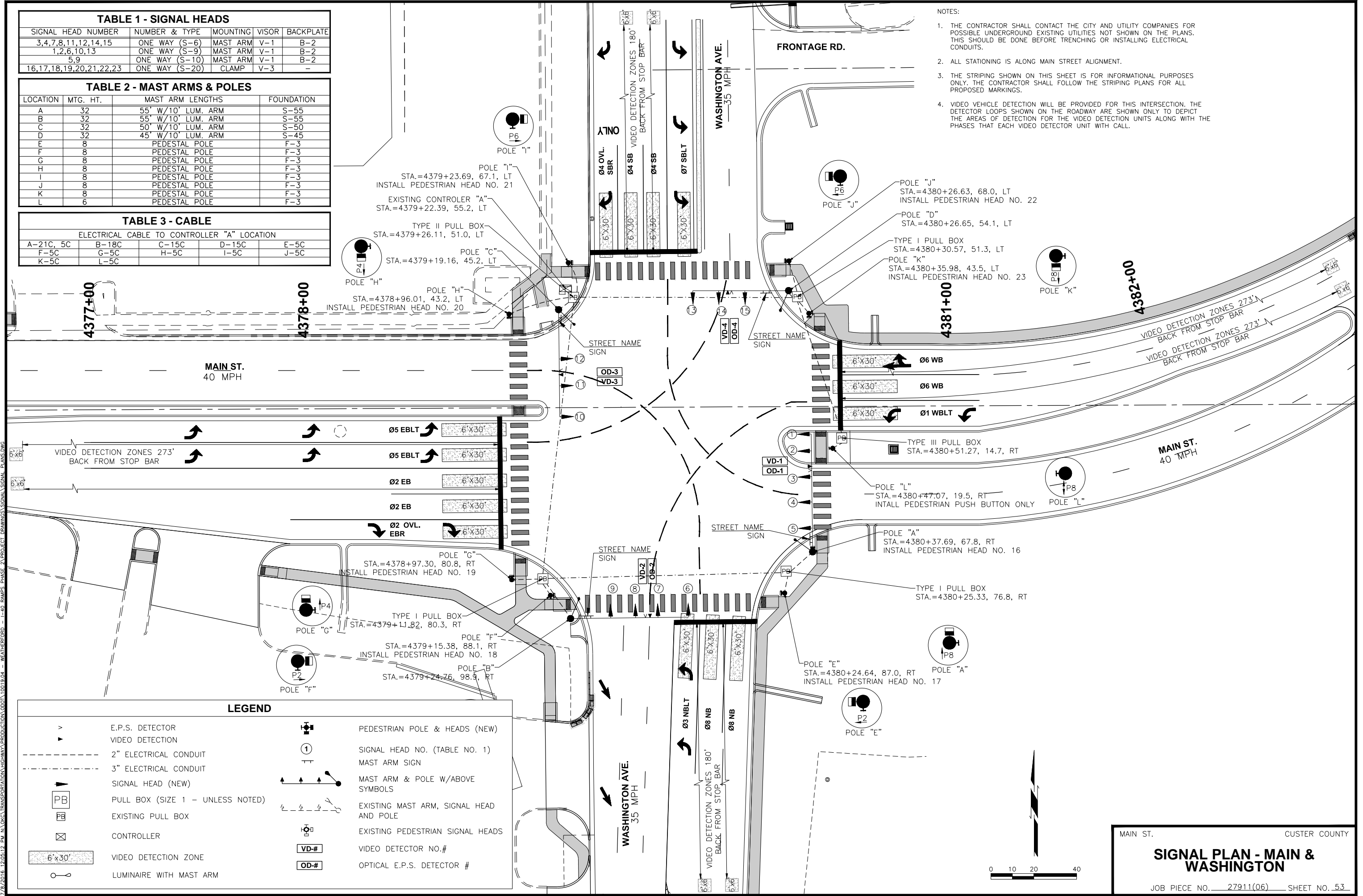


SIGNAL HEAD NUMBER	NUMBER & TYPE	MOUNTING	VISOR	BACKPLATE
3,4,7,8,11,12,14,15	ONE WAY (S-6)	MAST ARM	V-1	B-2
1,2,6,10,13	ONE WAY (S-9)	MAST ARM	V-1	B-2
5,9	ONE WAY (S-10)	MAST ARM	V-1	B-2
16,17,18,19,20,21,22,23	ONE WAY (S-20)	CLAMP	V-3	-

LOCATION	MTG. HT.	MAST ARM LENGTHS	FOUNDATION
A	32	55' W/10' LUM. ARM	S-55
B	32	55' W/10' LUM. ARM	S-55
C	32	50' W/10' LUM. ARM	S-50
D	32	45' W/10' LUM. ARM	S-45
E	8	PEDESTAL POLE	F-3
F	8	PEDESTAL POLE	F-3
G	8	PEDESTAL POLE	F-3
H	8	PEDESTAL POLE	F-3
I	8	PEDESTAL POLE	F-3
J	8	PEDESTAL POLE	F-3
K	8	PEDESTAL POLE	F-3
L	6	PEDESTAL POLE	F-3

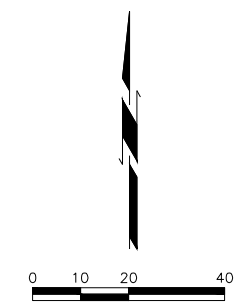
ELECTRICAL CABLE TO CONTROLLER "A" LOCATION				
A-21C_5C	B-18C	C-15C	D-15C	E-5C
F-5C	G-5C	H-5C	I-5C	J-5C
K-5C	L-5C			

- NOTES:
1. THE CONTRACTOR SHALL CONTACT THE CITY AND UTILITY COMPANIES FOR POSSIBLE UNDERGROUND EXISTING UTILITIES NOT SHOWN ON THE PLANS. THIS SHOULD BE DONE BEFORE TRENCHING OR INSTALLING ELECTRICAL CONDUITS.
 2. ALL STATIONING IS ALONG MAIN STREET ALIGNMENT.
 3. THE STRIPING SHOWN ON THIS SHEET IS FOR INFORMATIONAL PURPOSES ONLY. THE CONTRACTOR SHALL FOLLOW THE STRIPING PLANS FOR ALL PROPOSED MARKINGS.
 4. VIDEO VEHICLE DETECTION WILL BE PROVIDED FOR THIS INTERSECTION. THE DETECTOR LOOPS SHOWN ON THE ROADWAY ARE SHOWN ONLY TO DEPICT THE AREAS OF DETECTION FOR THE VIDEO DETECTION UNITS ALONG WITH THE PHASES THAT EACH VIDEO DETECTOR UNIT WITH CALL.



LEGEND

	E.P.S. DETECTOR		PEDESTRIAN POLE & HEADS (NEW)
	VIDEO DETECTION		SIGNAL HEAD NO. (TABLE NO. 1)
	2" ELECTRICAL CONDUIT		MAST ARM SIGN
	3" ELECTRICAL CONDUIT		MAST ARM & POLE W/ABOVE SYMBOLS
	SIGNAL HEAD (NEW)		EXISTING MAST ARM, SIGNAL HEAD AND POLE
	PULL BOX (SIZE 1 - UNLESS NOTED)		EXISTING PEDESTRIAN SIGNAL HEADS
	EXISTING PULL BOX		VIDEO DETECTOR NO.#
	CONTROLLER		OPTICAL E.P.S. DETECTOR #
	VIDEO DETECTION ZONE		
	LUMINAIRE WITH MAST ARM		



MAIN ST. CUSTER COUNTY
SIGNAL PLAN - MAIN & WASHINGTON
 JOB PIECE NO. 27911(06) SHEET NO. 53

7/28/2016 12:05:12 PM N:\CIVIL\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\0019.DWG - WEATHERFORD - I-40 RAMP/PHASE 2/PROJECT DRAWINGS/SEGMENTAL SIGNAL PLANS.DWG

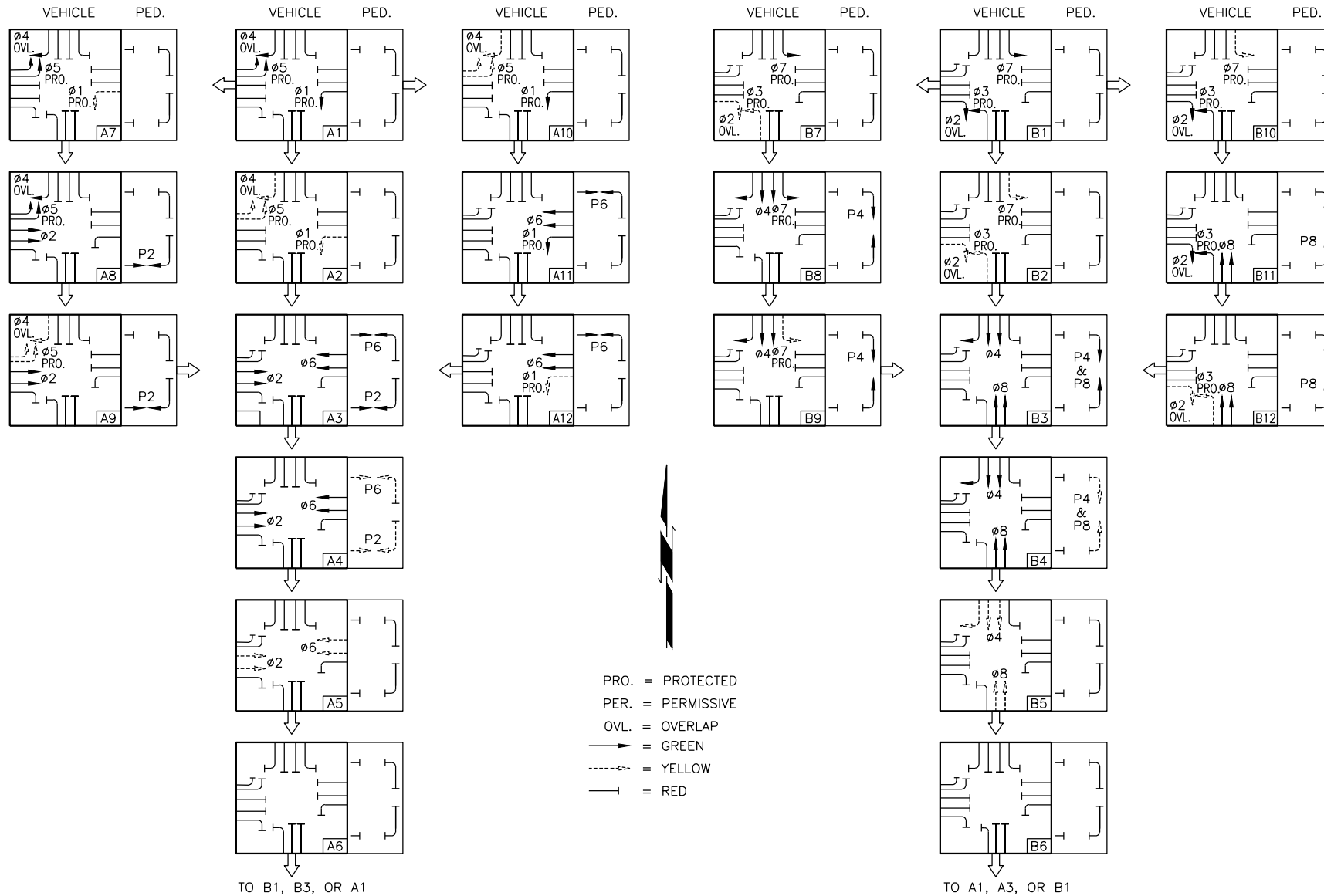
PHASING DIAGRAM

PHASE "A"

PHASE "B"

MAIN STREET

WASHINGTON AVENUE



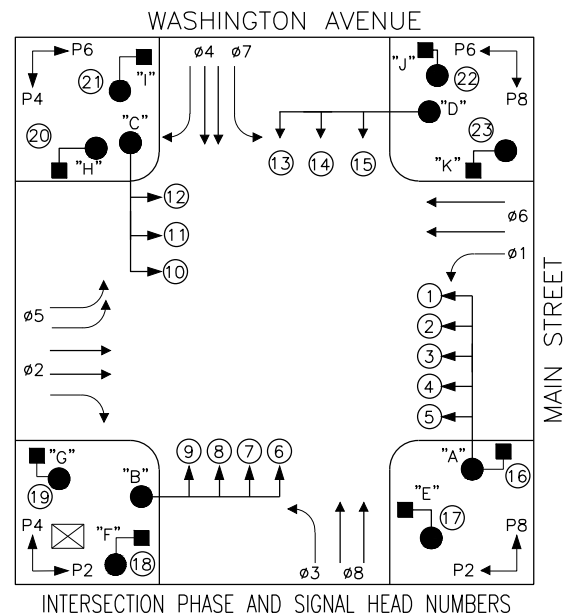
SEQUENCE CHART

DIRECTION	BLOCK NO.	PHASE																	
		5	2	7	4	1	6	3	8	P8	P2	P4	P6						
		SIGNAL HEAD NUMBER																	
		1,2	3,4	5	6	7,8	9	10	11,12	13	14,15	16,23	17,18	19,20	21,22				
05 & 01 PRO., 04 OVL.	A1	←G	R	←R	←R	R	←G	←G	R	←R	R	DW	DW	DW	DW				
05 & 01 CL., 04 OVL. CL.	A2	←Y	R	←R	←R	R	←Y	←Y	R	←R	R	DW	DW	DW	DW				
06 & 02 R.O.W., P6 & P2 R.O.W.	A3	←R	G	←R	←R	R	←R	←R	G	←R	R	DW	W	DW	W				
06 & 02 R.O.W., P6 & P2 CL.	A4	←R	G	←R	←R	R	←R	←R	G	←R	R	DW	FDW	DW	FDW				
06 & 02 CL.	A5	←R	Y	←R	←R	R	←R	←R	Y	←R	R	DW	DW	DW	DW				
ALL RED	A6	←R	R	←R	←R	R	←R	←R	R	←R	R	DW	DW	DW	DW				
05 PRO., 01 CL., 04 OVL.	A7	←G	R	←R	←R	R	←G	←Y	R	←R	R	DW	DW	DW	DW				
05 PRO., 02 R.O.W., 04 OVL., P2 R.O.W.	A8	←G	G	←R	←R	R	←G	←R	R	←R	R	DW	W	DW	DW				
05 CL., 04 OVL. CL., 02 R.O.W., P2 R.O.W.	A9	←Y	G	←R	←R	R	←Y	←R	R	←R	R	DW	W	DW	DW				
01 PRO., 05 CL., 04 OVL. CL.	A10	←Y	R	←R	←R	R	←Y	←G	R	←R	R	DW	DW	DW	DW				
01 PRO., 06 R.O.W., P6 R.O.W.	A11	←R	R	←R	←R	R	←R	←G	G	←R	R	DW	DW	DW	W				
01 CL., 06 R.O.W., P6 R.O.W.	A12	←R	R	←R	←R	R	←R	←Y	G	←R	R	DW	DW	DW	W				
03 & 07 PRO., 02 OVL.	B1	←R	R	←G	←G	R	←R	←R	R	←G	R	DW	DW	DW	DW				
03 & 07 CL., 02 OVL. CL.	B2	←R	R	←Y	←Y	R	←R	←R	R	←Y	R	DW	DW	DW	DW				
04 & 08 R.O.W., P4 & P8 R.O.W.	B3	←R	R	←R	←R	G	←G	←R	R	←R	G	W	DW	W	DW				
04 & 08 R.O.W., P4 & P8 CL.	B4	←R	R	←R	←R	G	←G	←R	R	←R	G	FDW	DW	FDW	DW				
04 & 08 CL.	B5	←R	R	←R	←R	Y	←Y	←R	R	←R	Y	DW	DW	DW	DW				
ALL RED	B6	←R	R	←R	←R	R	←R	←R	R	←R	R	DW	DW	DW	DW				
07 PRO., 03 CL., 02 OVL. CL.	B7	←R	R	←Y	←G	R	←R	←R	R	←Y	R	DW	DW	DW	DW				
07 PRO., 04 R.O.W., P4 R.O.W.	B8	←R	R	←R	←G	G	←G	←R	R	←R	R	DW	DW	W	DW				
07 CL., 04 R.O.W., P4 R.O.W.	B9	←R	R	←R	←Y	G	←G	←R	R	←R	R	DW	DW	W	DW				
03 PRO., 07 CL., 02 OVL.	B10	←R	R	←G	←Y	R	←R	←R	R	←G	R	DW	DW	DW	DW				
03 PRO., 08 R.O.W., P8 R.O.W., 02 OVL.	B11	←R	R	←G	←R	R	←R	←R	R	←G	G	W	DW	DW	DW				
03 CL., 08 R.O.W., P8 R.O.W., 02 OVL. CL.	B12	←R	R	←Y	←R	R	←R	←R	R	←Y	G	W	DW	DW	DW				
FLASHING OPERATION	⊗	FR	FR	FR	FR	FR	FR	FR	FR	FR	FR	-	-	-	-				

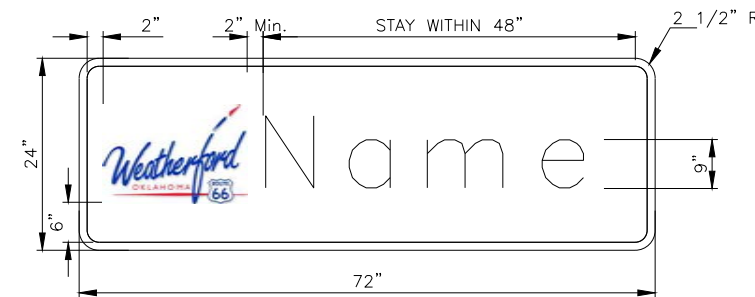
G = GREEN
 Y = YELLOW
 R = RED
 ←G = GREEN ARROW
 ←Y = YELLOW ARROW
 ←R = RED ARROW
 ←Y* = FLASHING YELLOW ARROW
 W = WALK
 DW = DON'T WALK
 FDW = FLASHING DON'T WALK
 FR = FLASHING RED

SUMMARY OF MAST ARM SIGNS

SER.	STREET NAME	POLE LOCATION	NO. OF SIGNS	L	W	SQ. FT. SIGN AREA	TOTAL AREA SQ. FT.
C	Main St	C,F	2	72	24	12.0	24.0
C	Washington Ave	D,H	2	72	24	12.0	24.0
						TOTAL SQ. FT.	48.0



MESSAGE	FONT	LOCATION	SIZE
STREET NAME	UPPERCASE 12" LOWERCASE 9"	CENTERED HORIZONTALLY BETWEEN BORDER AND CITY SEAL, 3.0" OFFSET FROM TOP OF SIGN BLANK	12.0"
CITY LOGO		2.0" OFFSET FROM THE LEFT, 5.8" OFFSET FROM THE TOP AND BOTTOM	18"X12"
BORDER			1.0"



MAST ARM STREET MARKER

NOT TO SCALE

NOTES:

STREET NAME SHOWN IS FOR ILLUSTRATION PURPOSES ONLY. FOR INDIVIDUAL SIGN NAMES SEE "SUMMARY OF MAST ARM SIGNS" TABLE THIS SHEET.

STREET NAME SIGNS SHALL BE MOUNTED ON MAST ARMS USING A MODIFIED MOUNTING BRACKET EQUIVALENT TO THOSE USED FOR MOUNTING SIGNAL HEADS. THESE SIGNS SHALL BE LOCATED 2'-0" FROM THE POLE ON THE MAST ARM. THICKNESS OF SIGN MATERIAL SHALL BE 1/16". SEE ODOT STD. SNS1-1 (LATEST REV.) FOR BRACKET DESIGN.

REFLECTORIZED GREEN - BACKGROUND
REFLECTORIZED WHITE - MESSAGE & BORDER

MAIN ST. CUSTER COUNTY

PHASE DIAGRAMS - MAIN & WASHINGTON

JOB PIECE NO. 27911(06) SHEET NO. 54

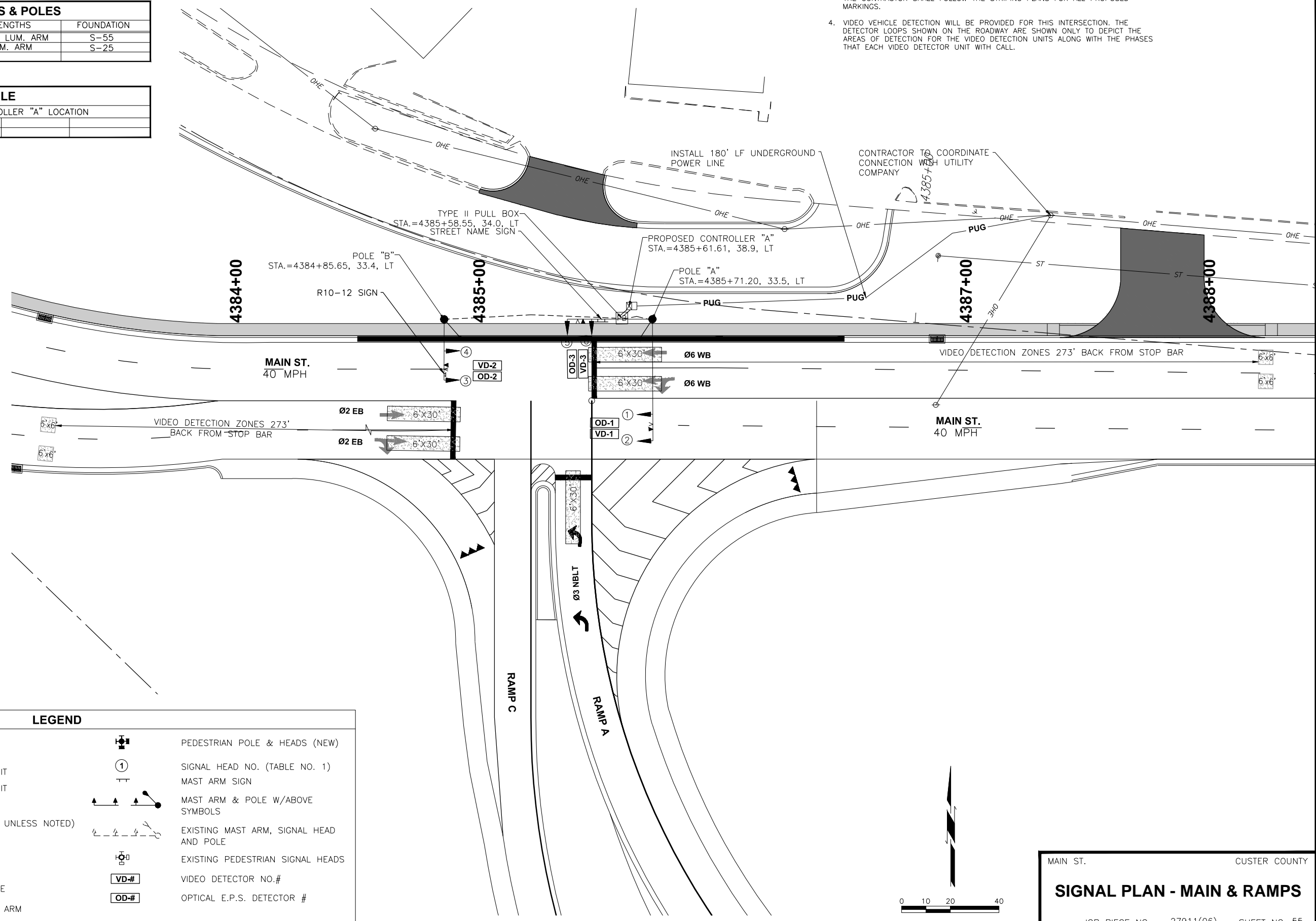
TABLE 1 - SIGNAL HEADS				
SIGNAL HEAD NUMBER	NUMBER & TYPE	MOUNTING	VISOR	BACKPLATE
1,2,3,4	ONE WAY (S-6)	MAST ARM	V-1	B-2
5,6	ONE WAY (S-9)	MAST ARM	V-1	B-2

TABLE 2 - MAST ARMS & POLES			
LOCATION	MTG. HT.	MAST ARM LENGTHS	FOUNDATION
A	32'	50' & 35' W/10' LUM. ARM	S-55
B	32'	25' W/10' LUM. ARM	S-25

TABLE 3 - CABLE			
ELECTRICAL CABLE TO CONTROLLER "A" LOCATION			
A-18C	B-9C		

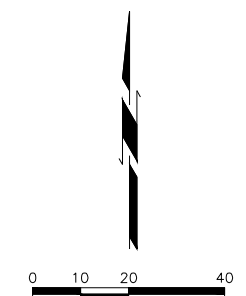
NOTES:

1. THE CONTRACTOR SHALL CONTACT THE CITY AND UTILITY COMPANIES FOR POSSIBLE UNDERGROUND EXISTING UTILITIES NOT SHOWN ON THE PLANS. THIS SHOULD BE DONE BEFORE TRENCHING OR INSTALLING ELECTRICAL CONDUITS.
2. ALL STATIONING IS ALONG MAIN STREET ALIGNMENT.
3. THE STRIPING SHOWN ON THIS SHEET IS FOR INFORMATIONAL PURPOSES ONLY. THE CONTRACTOR SHALL FOLLOW THE STRIPING PLANS FOR ALL PROPOSED MARKINGS.
4. VIDEO VEHICLE DETECTION WILL BE PROVIDED FOR THIS INTERSECTION. THE DETECTOR LOOPS SHOWN ON THE ROADWAY ARE SHOWN ONLY TO DEPICT THE AREAS OF DETECTION FOR THE VIDEO DETECTION UNITS ALONG WITH THE PHASES THAT EACH VIDEO DETECTOR UNIT WITH CALL.



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LEGEND			
>	E.P.S. DETECTOR		PEDESTRIAN POLE & HEADS (NEW)
▶	VIDEO DETECTION	①	SIGNAL HEAD NO. (TABLE NO. 1)
---	2" ELECTRICAL CONDUIT	—	MAST ARM SIGN
- - -	3" ELECTRICAL CONDUIT		MAST ARM & POLE W/ABOVE SYMBOLS
	SIGNAL HEAD (NEW)		EXISTING MAST ARM, SIGNAL HEAD AND POLE
	PULL BOX (SIZE 1 - UNLESS NOTED)		EXISTING PEDESTRIAN SIGNAL HEADS
	EXISTING PULL BOX	VD-#	VIDEO DETECTOR NO.#
	CONTROLLER	OD-#	OPTICAL E.P.S. DETECTOR #
	VIDEO DETECTION ZONE		
	LUMINAIRE WITH MAST ARM		



MAIN ST. CUSTER COUNTY

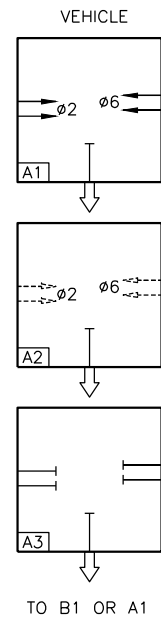
SIGNAL PLAN - MAIN & RAMPS

JOB PIECE NO. 27911(06) SHEET NO. 55

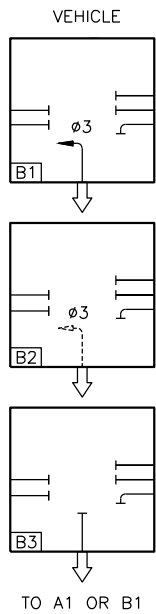
PHASING DIAGRAM

**PHASE "A"
MAIN STREET**

**PHASE "B"
I-40 RAMPS**



TO B1 OR A1



TO A1 OR B1



→ = GREEN
- - - = YELLOW
—| = RED

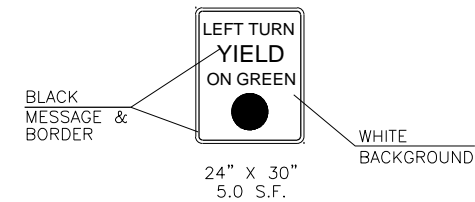
SEQUENCE CHART

DIRECTION	BLOCK No.	PHASE			
		SIGNAL HEAD NO.			
		5,6	1,2	3,4	
Ø2 & Ø6 R.O.W.	A1	→R	G	G	
Ø2 & Ø6 CL.	A2	→R	Y	Y	
ALL RED	A3	→R	R	R	
Ø8 R.O.W.	B1	→G	R	R	
Ø8 CL.	B2	→Y	R	R	
ALL RED	B3	→R	R	R	
FLASHING OPERATION	⊗	FR	FR	FR	

G = GREEN
Y = YELLOW
R = RED

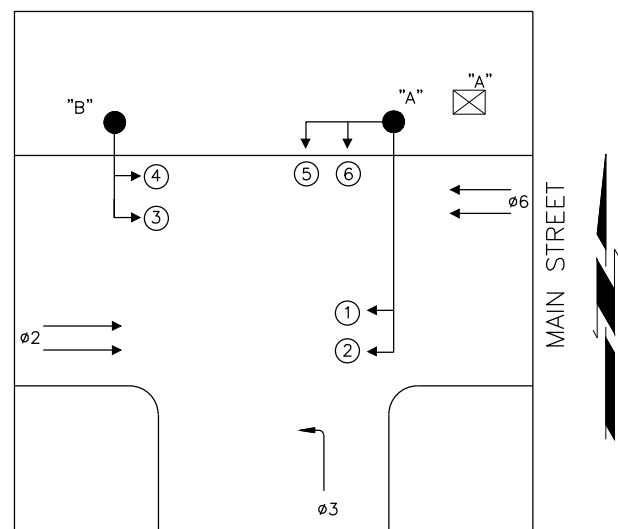
→G = GREEN ARROW
→Y = YELLOW ARROW
→R = RED ARROW

W = WALK
DW = DON'T WALK
FDW = FLASHING DON'T WALK
FR = FLASHING RED



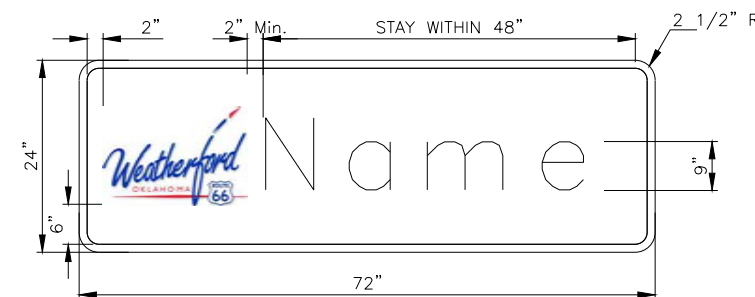
R10-12 SIGN DETAILS

SUMMARY OF MAST ARM SIGNS							
SER.	STREET NAME	POLE LOCATION	NO. OF SIGNS	L	W	SQ. FT. SIGN AREA	TOTAL AREA SQ. FT.
C	Main St	A	1	72	24	12.0	12.0
C	R10-12	B	1	24	30	5.0	5.0
						TOTAL SQ. FT.	17.0



INTERSECTION PHASE AND SIGNAL HEAD NUMBERS

MESSAGE	FONT	LOCATION	SIZE
STREET NAME	UPPERCASE 12" LOWERCASE 9"	CENTERED HORIZONTALLY BETWEEN BORDER AND CITY SEAL, 3.0" OFFSET FROM TOP OF SIGN BLANK	12.0"
CITY LOGO		2.0" OFFSET FROM THE LEFT, 5.8" OFFSET FROM THE TOP AND BOTTOM	18"X12"
BORDER			1.0"



MAST ARM STREET MARKER

NOT TO SCALE

NOTES:

STREET NAME SHOWN IS FOR ILLUSTRATION PURPOSES ONLY. FOR INDIVIDUAL SIGN NAMES SEE "SUMMARY OF MAST ARM SIGNS" TABLE THIS SHEET.

STREET NAME SIGNS SHALL BE MOUNTED ON MAST ARMS USING A MODIFIED MOUNTING BRACKET EQUIVALENT TO THOSE USED FOR MOUNTING SIGNAL HEADS. THESE SIGNS SHALL BE LOCATED 2'-0" FROM THE POLE ON THE MAST ARM. THICKNESS OF SIGN MATERIAL SHALL BE 1/16". SEE ODOT STD. SNS1-1 (LATEST REV.) FOR BRACKET DESIGN.

REFLECTORIZED GREEN - BACKGROUND
REFLECTORIZED WHITE - MESSAGE & BORDER

MAIN ST. CUSTER COUNTY

PHASE DIAGRAMS - MAIN & RAMPS

JOB PIECE NO. 27911(06) SHEET NO. 56

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 1

CONSTRUCT TEMPORARY RAMP AND WIDENING FOR RAMP A.

REMOVE EXISTING RAISED MEDIAN ALONG MAIN STREET FROM STA. 4375+48.07 TO STA. 4379+35.97.

CONSTRUCT STRUCTURES 7, 8, 14, 15 AND 16.

INSTALL TEMPORARY TRAFFIC SIGNAL.

PHASE 2

PHASE 2A

ROUTE I-40 EAST BOUND OFF RAMP TO NEW EXIT 82.

ROUTE I-40 WEST BOUND OFF RAMP (RAMP A) TRAFFIC TO TEMPORARY RAMP.

MAIN ST:

SHIFT TRAFFIC TO EXIST EASTBOUND LANES.

CONSTRUCT STRUCTURES 1, 2, 3, 5, 6, 12 AND 13.

CONSTRUCT NEW WESTBOUND LANES WEST OF WASHINGTON AVE FROM STA. 4375+48 TO STA. 4379+50.

CONSTRUCT ALL NEW LANES EAST OF WASHINGTON AVE FROM STA. 4379+93 TO 4385+46.

CONSTRUCT NEW EASTBOUND LANES FROM STA. 4385+46 TO 4391+25.

WASHINGTON AVE:

MERGE TRAFFIC AND SHIFT SOUTHBOUND TRAFFIC TO ONE LANE.

CONSTRUCT LEFT SOUTHBOUND LANE FROM STA 23+13 TO 25+32 AND FROM STA. 26+27 TO STA. 29+41.

CONSTRUCT LANE WIDENING OF NORTHBOUND LANES FROM STA. 22+51 TO STA. 25+43 AND FROM STA. 25+91 TO STA. 29+41.

RAMPS:

CONSTRUCT RAMP A.

CONSTRUCT RAMP C FROM BOP TO STA. 4376+04.

MILL AND OVERLAY MAIN STREET UNDER EXISTING TRAFFIC.

PHASE 2B

WASHINGTON AVE:

MERGE TRAFFIC AND SHIFT SOUTHBOUND TRAFFIC TO NEW SOUTHBOUND ONE LANE.

CONSTRUCT SOUTHBOUND LANE FROM STA 23+13 TO 25+32 AND FROM STA. 26+27 TO STA. 29+41.

PHASE 3

MAIN ST:

MERGE AND SHIFT TRAFFIC TO NEW WESTBOUND LANES FROM STA. 4375+48 TO STA. 4384+50, THEN SHIFT TRAFFIC TO NEW EASTBOUND LANES FROM STA. 4384+50 TO STA. 4391+25.

CONSTRUCT STRUCTURES 9, 10 AND 11.

CONSTRUCT NEW EASTBOUND LANES FROM STA. 4378+48 TO 4384+50.

CONSTRUCT NEW WESTBOUND LANES FROM 4385+46 TO 4391+25.

WASHINGTON AVE:

CONSTRUCT SOUTHBOUND LANES FROM STA. 25+32 TO 26+27.

RAMPS:

COMPLETE RAMP C CONSTRUCTION INCLUDING MILL AND OVERLAY SECTION FROM STA. 4388+75 TO STA. 4389+47.

FRONTAGE ROAD:

RECONSTRUCT TO 2 LANE CONFIGURATION.

PHASE 4

PHASE 4A

MAIN ST:

SHIFT TRAFFIC TO NEW LANES. CLOSE EASTBOUND RIGHT – LEFT TURN LANE AND RIGHT THROUGH LANE.

WASHINGTON AVE:

MERGE NORTHBOUND TRAFFIC TO ONE LEFT

CONSTRUCT NORTHBOUND LANES FROM STA. 23+13 TO STA. 26+05 AND FROM STA. 26+60 TO STA. 29+41.

PHASE 4B

WASHINGTON AVE:

MERGE NORTHBOUND TRAFFIC RIGHT.

CONSTRUCT NORTHBOUND LANES FROM STA. 23+13 TO STA. 26+05 AND FROM STA. 26+60 TO STA. 29+41.

INSTALL PERMANENT SIGNALS.

PHASE 4C

WASHINGTON AVE:

COMPLETE CONSTRUCTION OF WASHINGTON AVE.

REMOVE TEMPORARY PAVING.

COMPLETE ANY MISCELLANEOUS CONSTRUCTION OPERATIONS.

PERFORM FINAL SIGNING AND STRIPING OPERATIONS.

REMOVE ALL TRAFFIC CONTROL.

NOTE: THE CONTRACTOR MAY SUBMIT A WRITTEN ALTERNATE SEQUENCE OF CONSTRUCTION OPERATION TO THE ENGINEER FOR APPROVAL.

ALL CONSTRUCTION SIGNING AND STRIPING IN ADVANCE OF, AND THROUGH THE PROJECT LIMITS WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH THE MUTCD (LATEST EDITION), AND APPLICABLE O.D.O.T. STANDARDS.

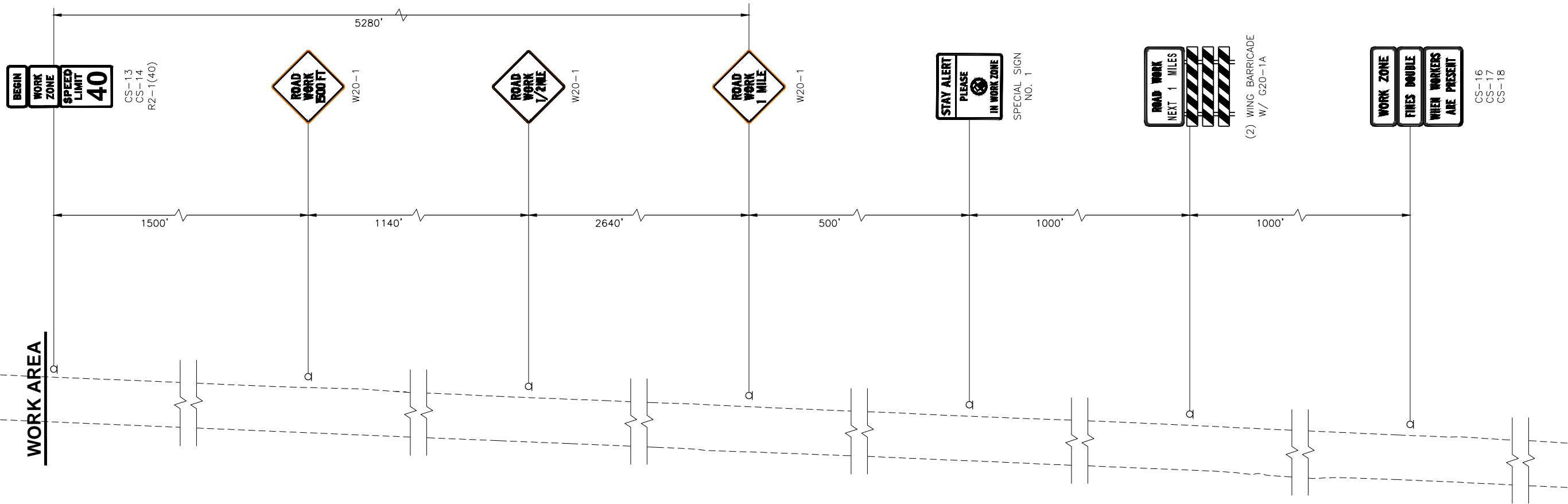
MAIN ST.

CUSTER COUNTY

SUGGESTED SEQUENCE OF CONSTRUCTION

JOB PIECE NO. 27911(06) SHEET NO. 57

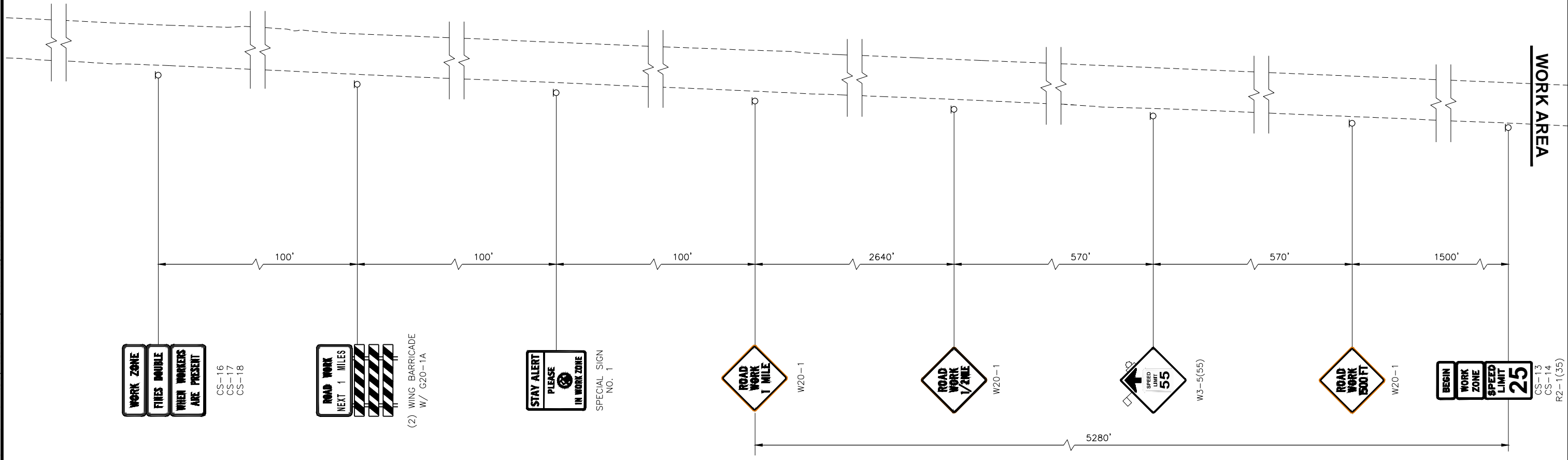
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I-40 SECTION 1



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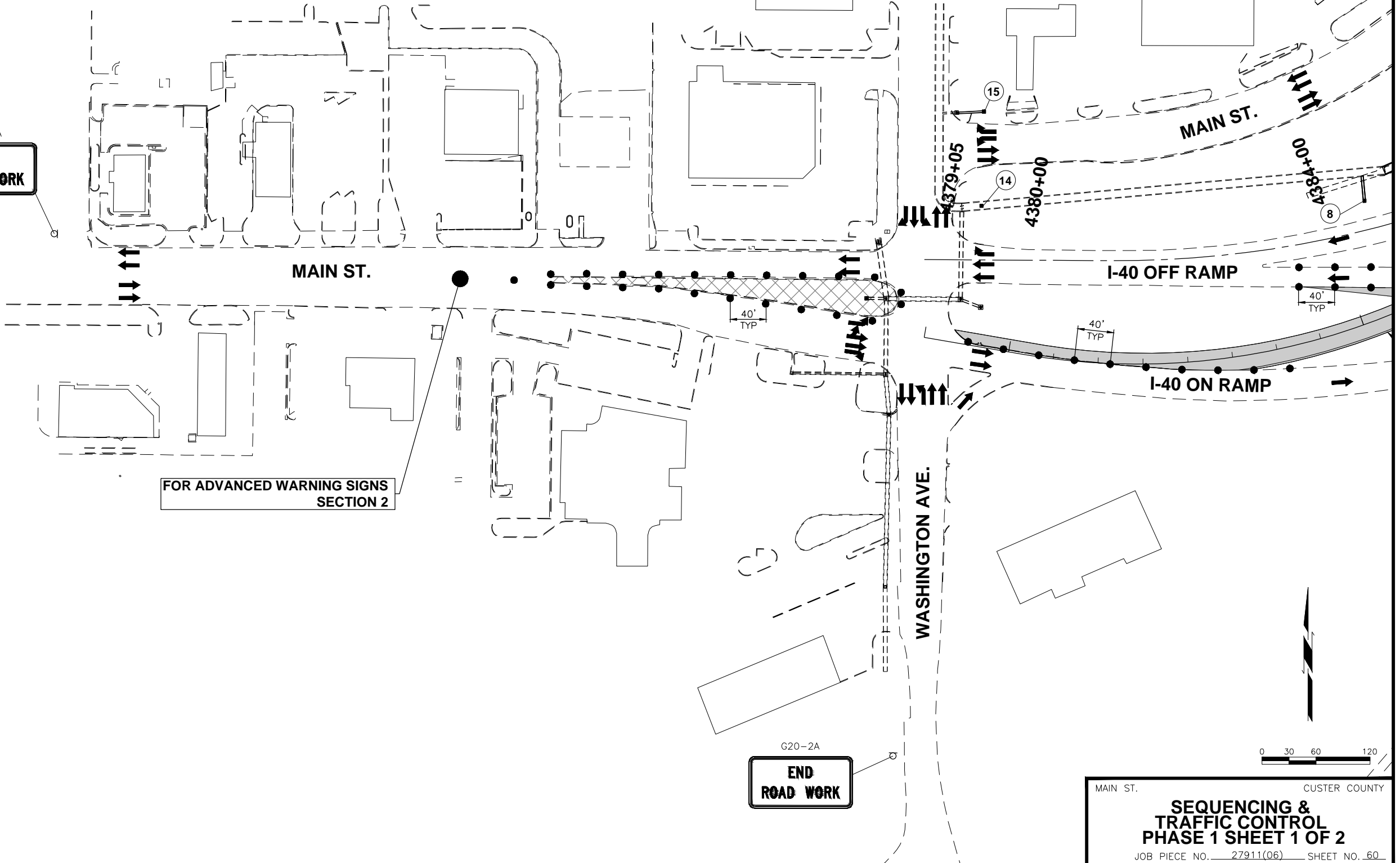
MAIN STREET SECTION 2

WORK AREA

7/29/2016 12:07:34 PM N:\OKC\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\10019.04 - WEATHERFORD - I-49 RAMP PHASE 2\PROJECT DRAWINGS\TRAFFIC CONTROL\10019.04 PHASE 1 BASEMAP.DWG

LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
▤ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
← FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
■ CONST. THIS PHASE	⚡ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	

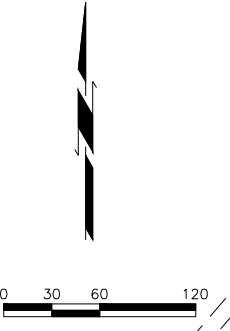
G20-2A
END ROAD WORK



G20-2A
END ROAD WORK

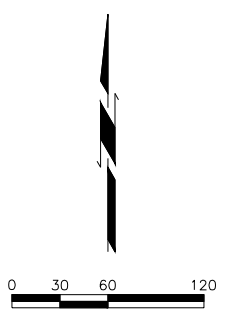
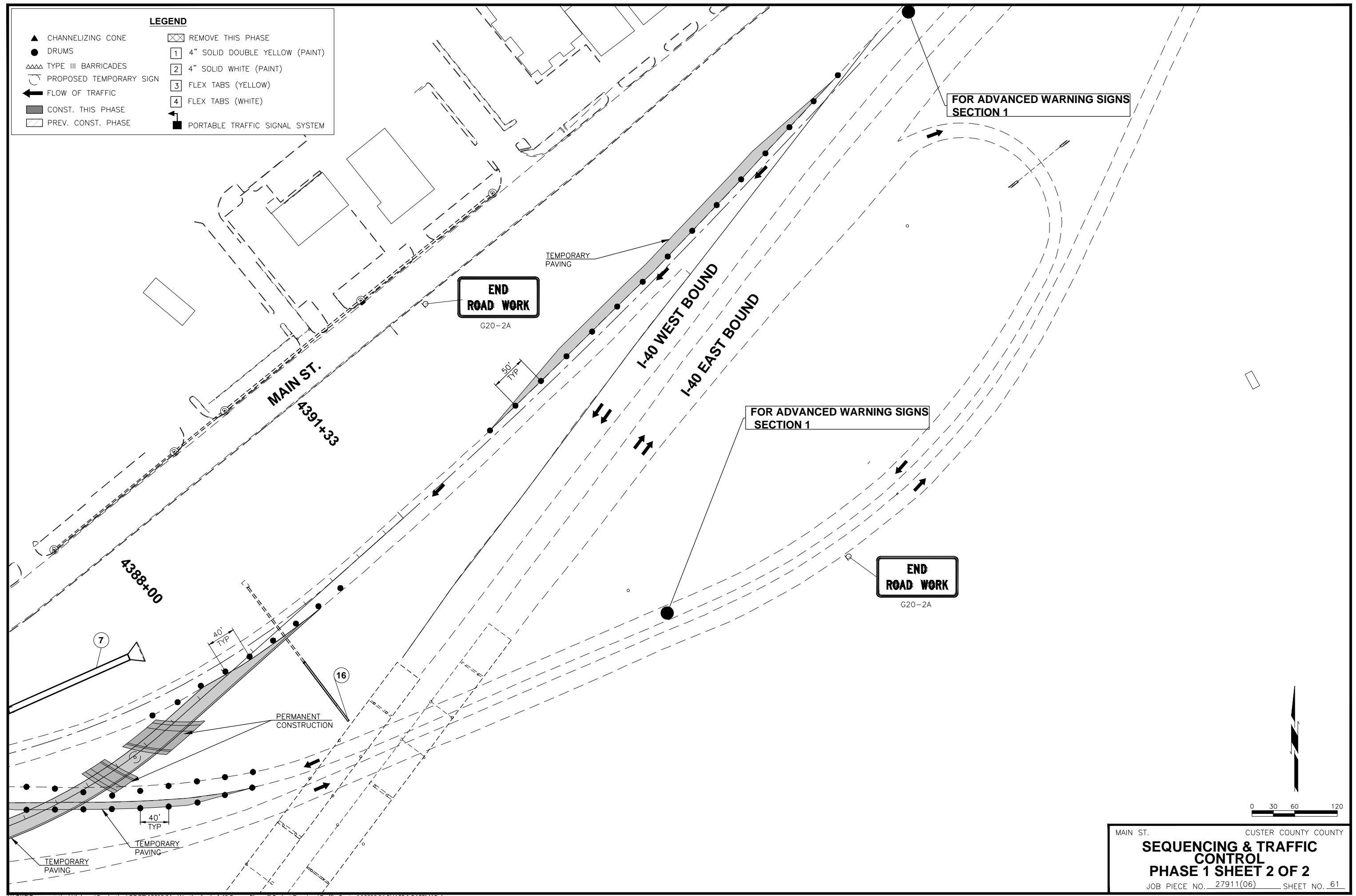
FOR ADVANCED WARNING SIGNS
SECTION 2

G20-2A
END ROAD WORK



MAIN ST. CUSTER COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 1 SHEET 1 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 60

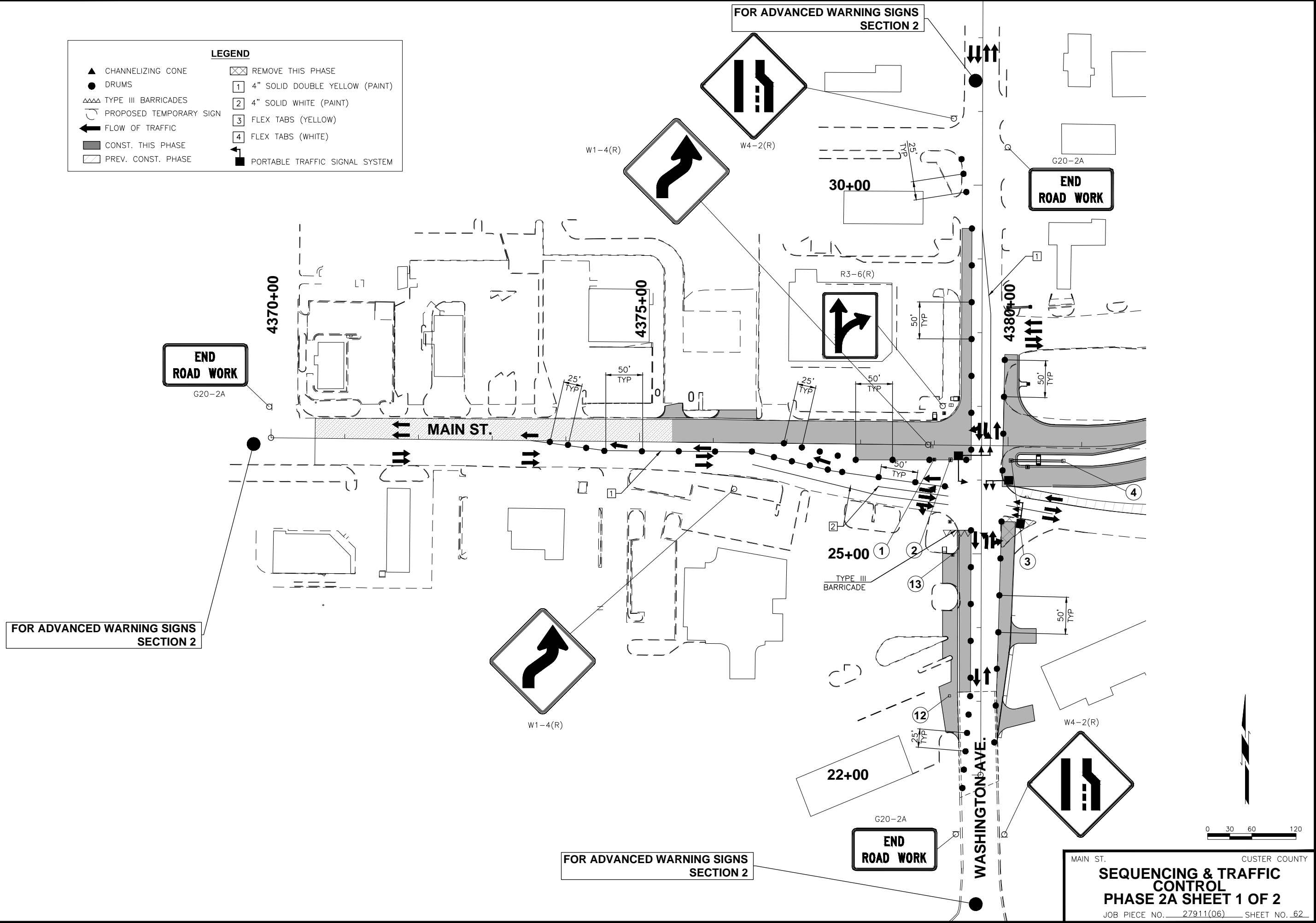
LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
≡≡≡ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
← FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
■ CONST. THIS PHASE	⬇ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	



MAIN ST. CUSTER COUNTY COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 1 SHEET 2 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 61

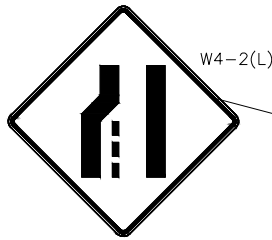
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LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
← FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▒ CONST. THIS PHASE	⬇ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	



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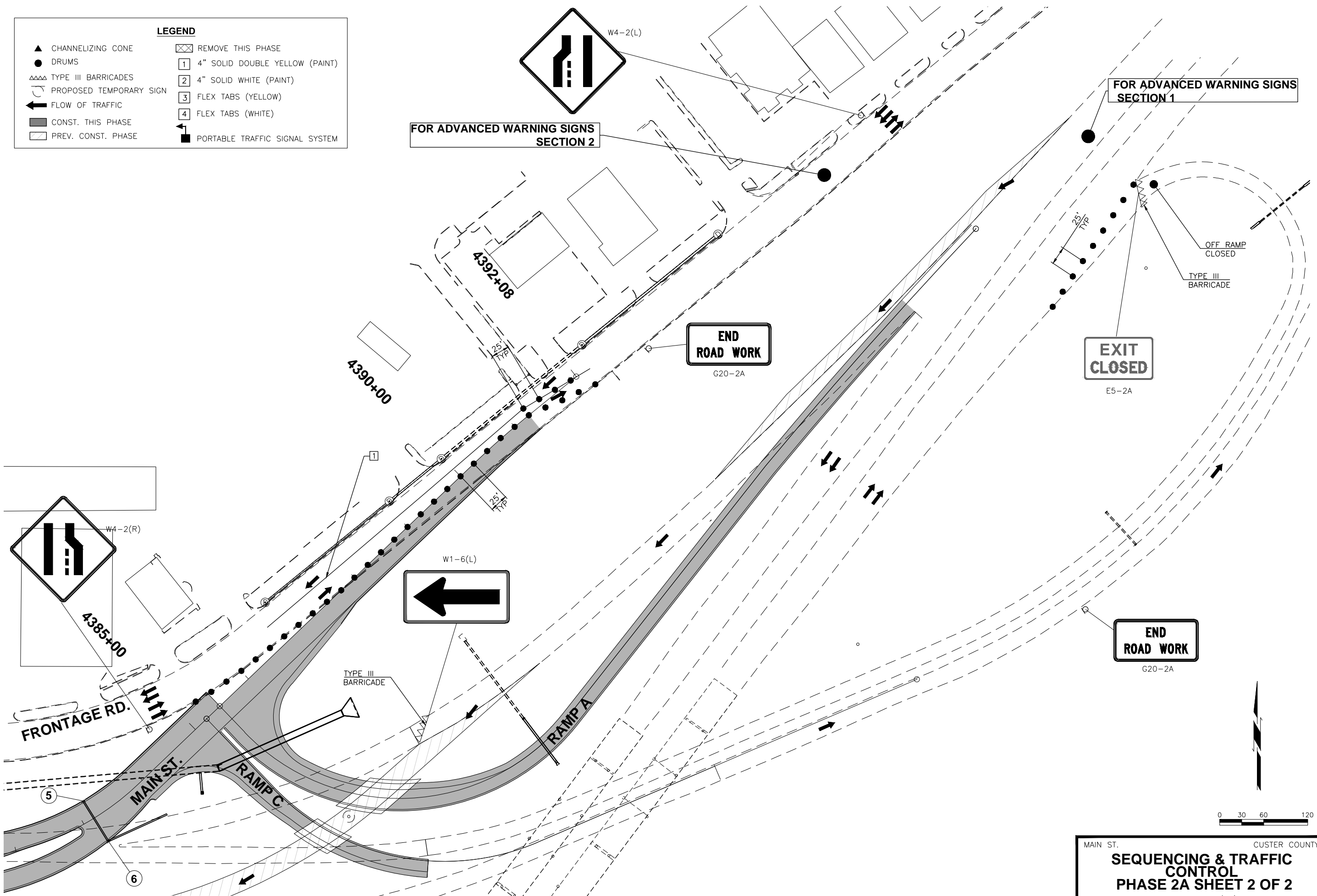
LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
➔ FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▒ CONST. THIS PHASE	⬅ PORTABLE TRAFFIC SIGNAL SYSTEM
▒ PREV. CONST. PHASE	



W4-2(L)

FOR ADVANCED WARNING SIGNS SECTION 2

FOR ADVANCED WARNING SIGNS SECTION 1



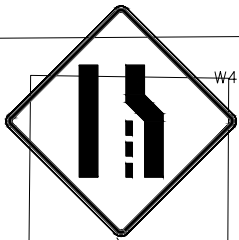
END ROAD WORK
G20-2A

EXIT CLOSED
E5-2A

END ROAD WORK
G20-2A



W1-6(L)



W4-2(R)

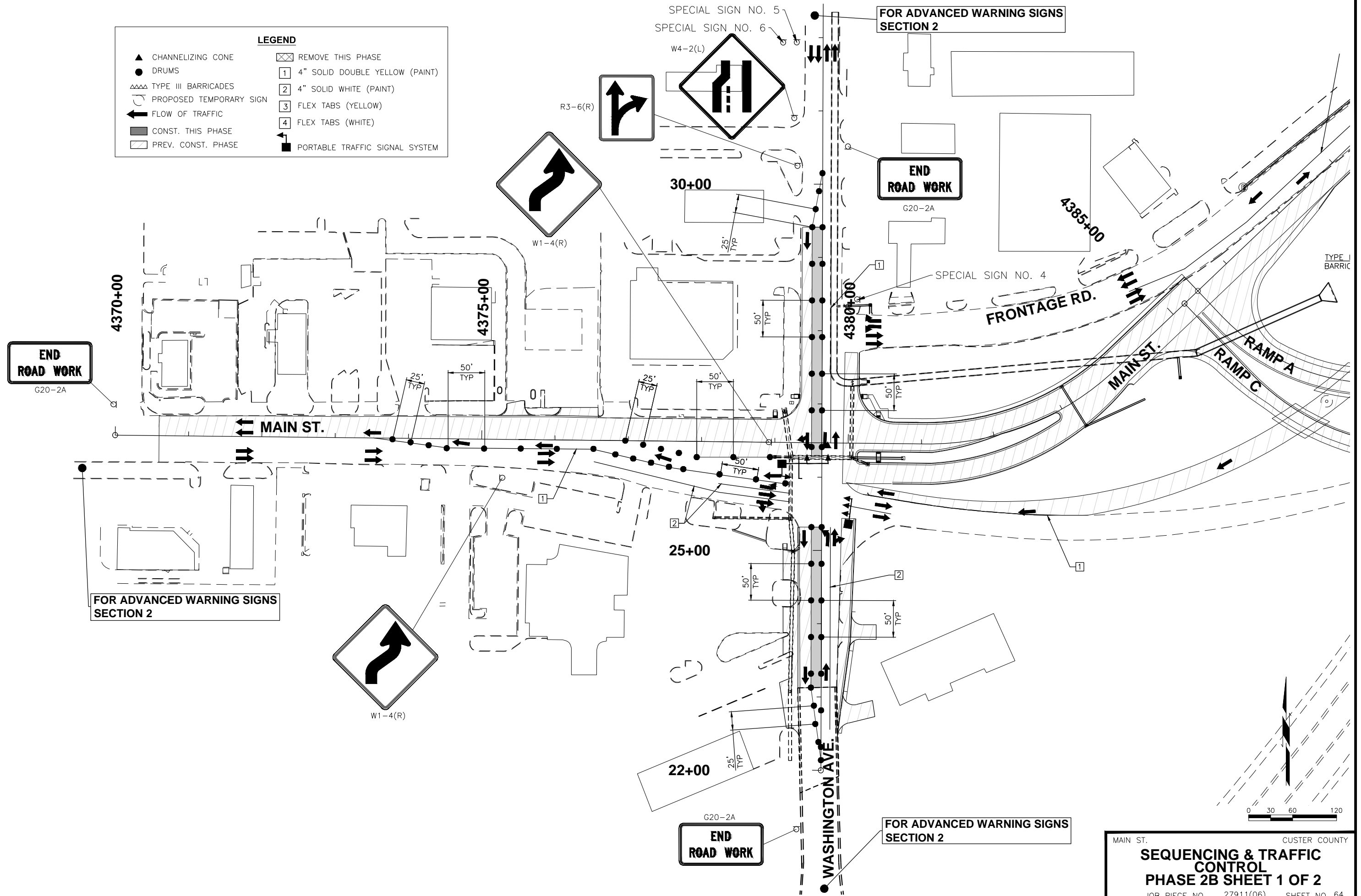


0 30 60 120

MAIN ST. CUSTER COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 2A SHEET 2 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 63

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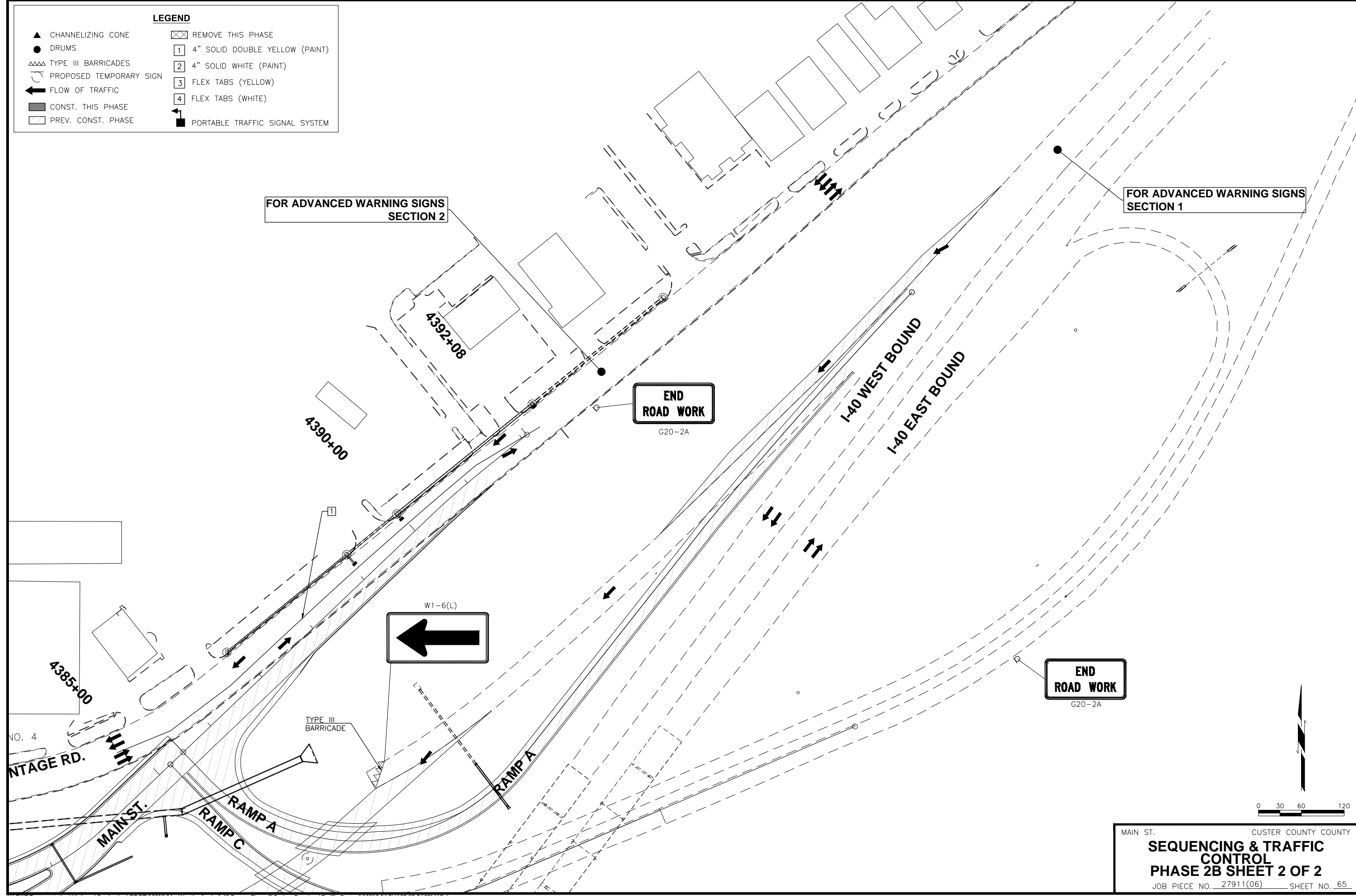
LEGEND	
▲ CHANNELIZING CONE	⊠ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
→ FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	⬇ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	



0 30 60 120

MAIN ST. CUSTER COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 2B SHEET 1 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 64

LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
≡≡≡ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
➔ FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	⬛ PORTABLE TRAFFIC SIGNAL SYSTEM
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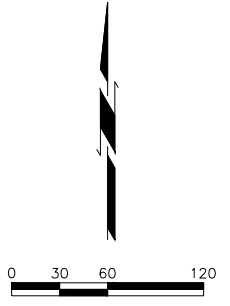
FOR ADVANCED WARNING SIGNS
SECTION 2

FOR ADVANCED WARNING SIGNS
SECTION 1

END
ROAD WORK
G20-2A

END
ROAD WORK
G20-2A

W1-6(L)
←



MAIN ST. CUSTER COUNTY COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 2B SHEET 2 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 65

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LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⏏ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
↩ FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	🚦 PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	

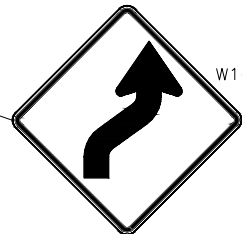
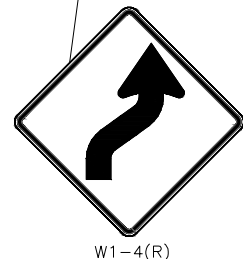
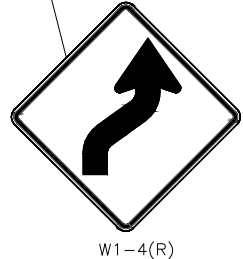
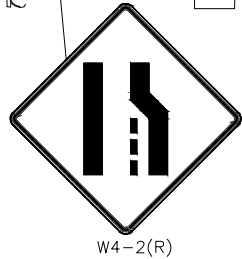
FOR ADVANCED WARNING SIGNS SECTION 2



END ROAD WORK

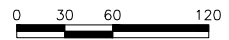
G20-2A
END ROAD WORK

FOR ADVANCED WARNING SIGNS SECTION 2

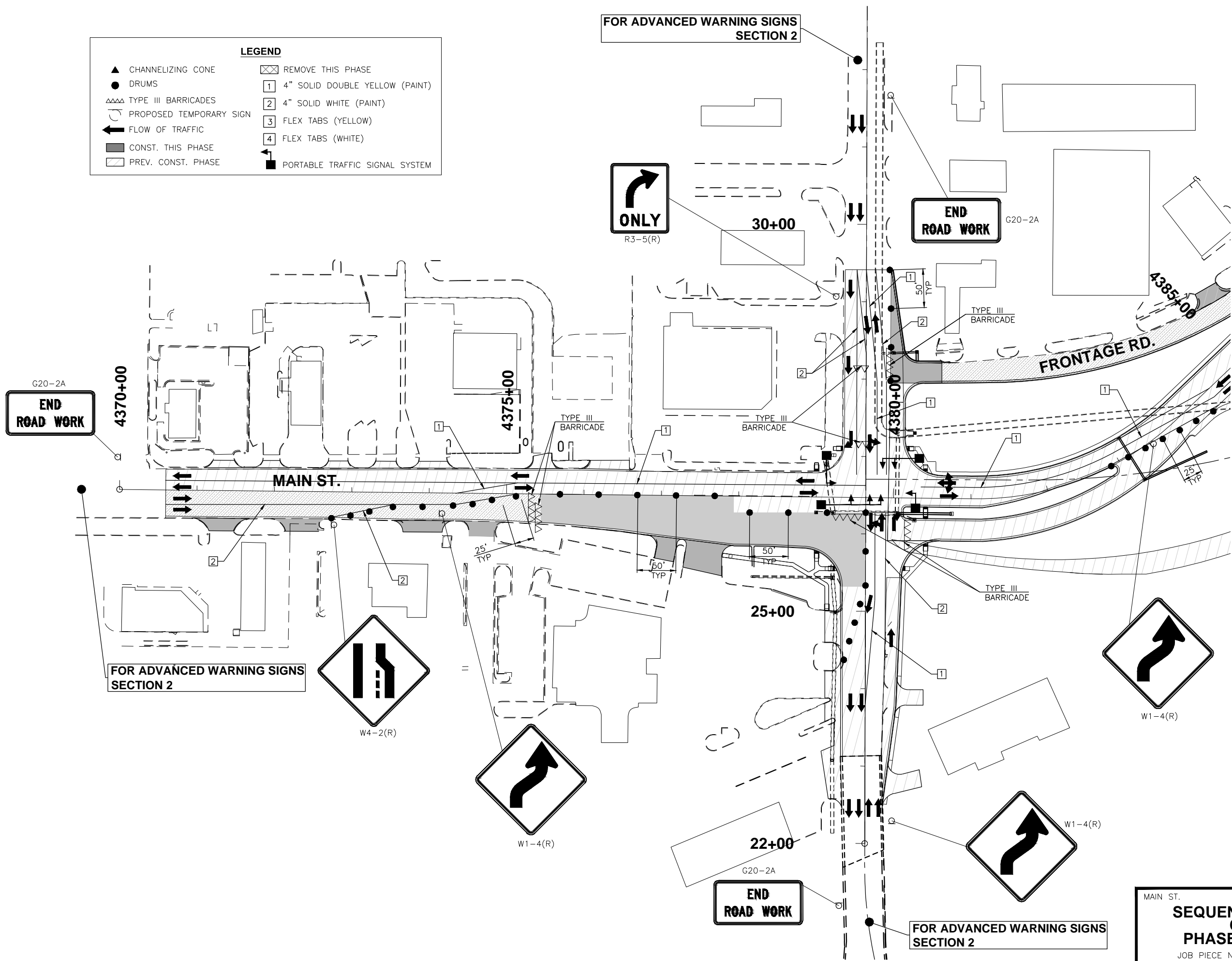


END ROAD WORK

FOR ADVANCED WARNING SIGNS SECTION 2



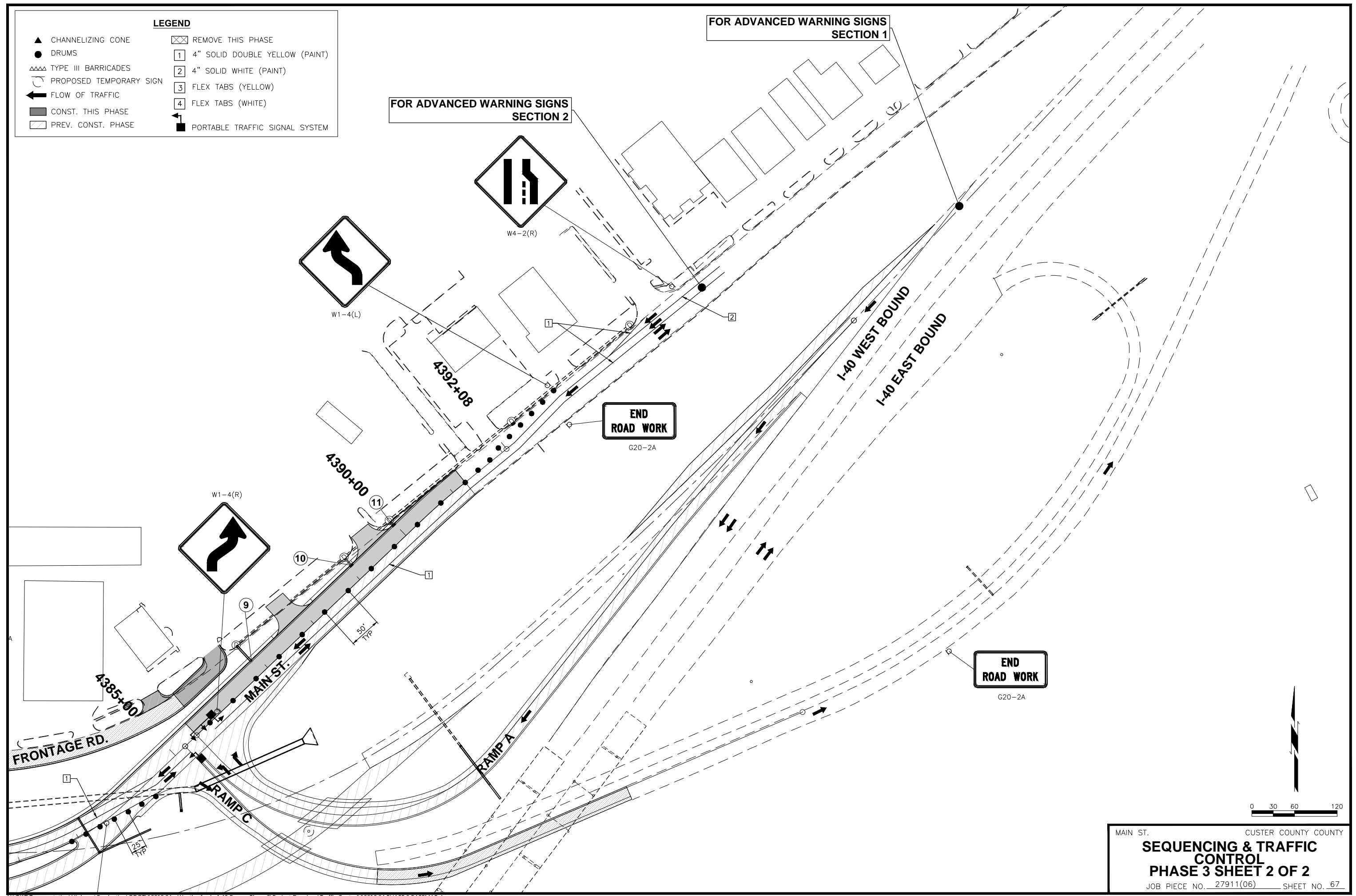
MAIN ST. CUSTER COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 3 SHEET 1 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 66



LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
← FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
■ CONST. THIS PHASE	⬇ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	

FOR ADVANCED WARNING SIGNS
SECTION 2

FOR ADVANCED WARNING SIGNS
SECTION 1

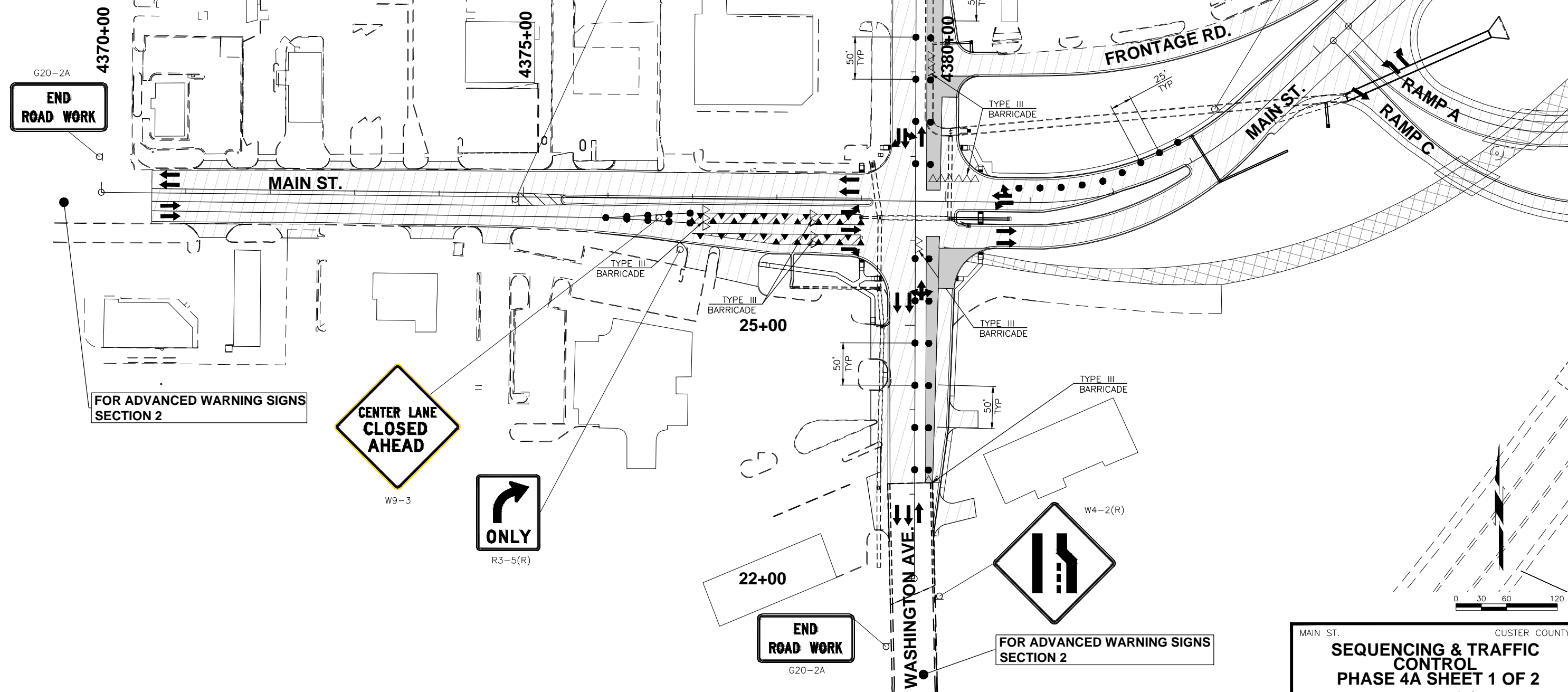


MAIN ST. CUSTER COUNTY COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 3 SHEET 2 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 67

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LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⤴ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
→ FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	🚦 PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	

FOR ADVANCED WARNING SIGNS SECTION 2



W4-2(R)

R3-5(L)

G20-2A

G20-2A

ONLY

END ROAD WORK

30+00

4385+00

4370+00

4375+00

4380+00

FRONTAGE RD.

MAIN ST.

MAIN ST.

RAMP A
RAMP C

TYPE III BARRICADE

TYPE III BARRICADE

25+00

TYPE III BARRICADE

TYPE III BARRICADE

FOR ADVANCED WARNING SIGNS SECTION 2

CENTER LANE CLOSED AHEAD

W9-3

ONLY

R3-5(R)

22+00

END ROAD WORK

G20-2A

WASHINGTON AVE.

W4-2(R)

FOR ADVANCED WARNING SIGNS SECTION 2

0 30 60 120

MAIN ST. CUSTER COUNTY

SEQUENCING & TRAFFIC CONTROL
PHASE 4A SHEET 1 OF 2

JOB PIECE NO. 27911(06) SHEET NO. 68

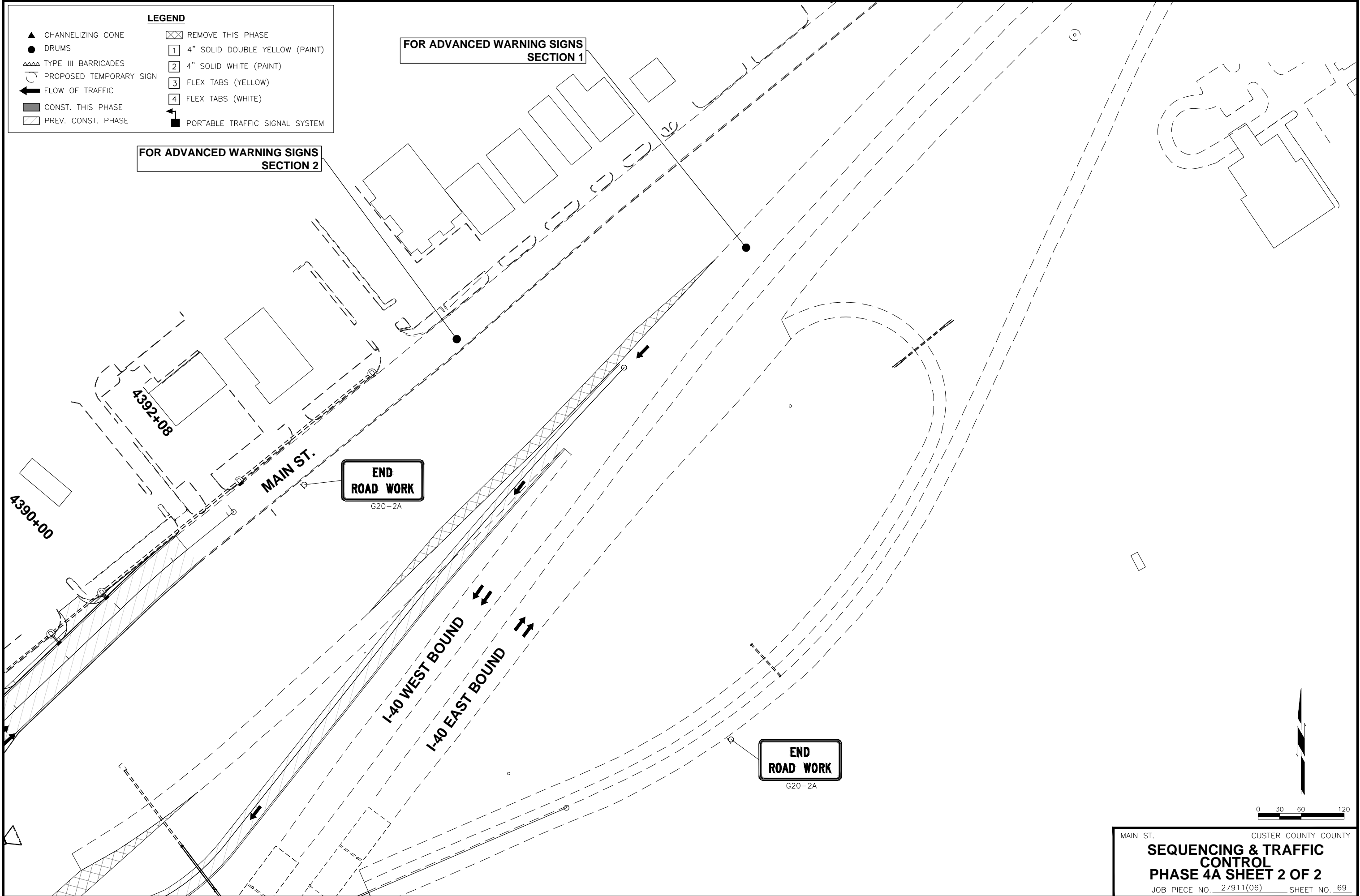
LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
⏏ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
← FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	⬛ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	

FOR ADVANCED WARNING SIGNS
SECTION 1

FOR ADVANCED WARNING SIGNS
SECTION 2

END
ROAD WORK
G20-2A

END
ROAD WORK
G20-2A



0 30 60 120

MAIN ST. CUSTER COUNTY COUNTY

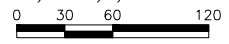
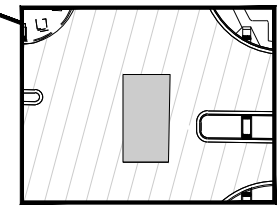
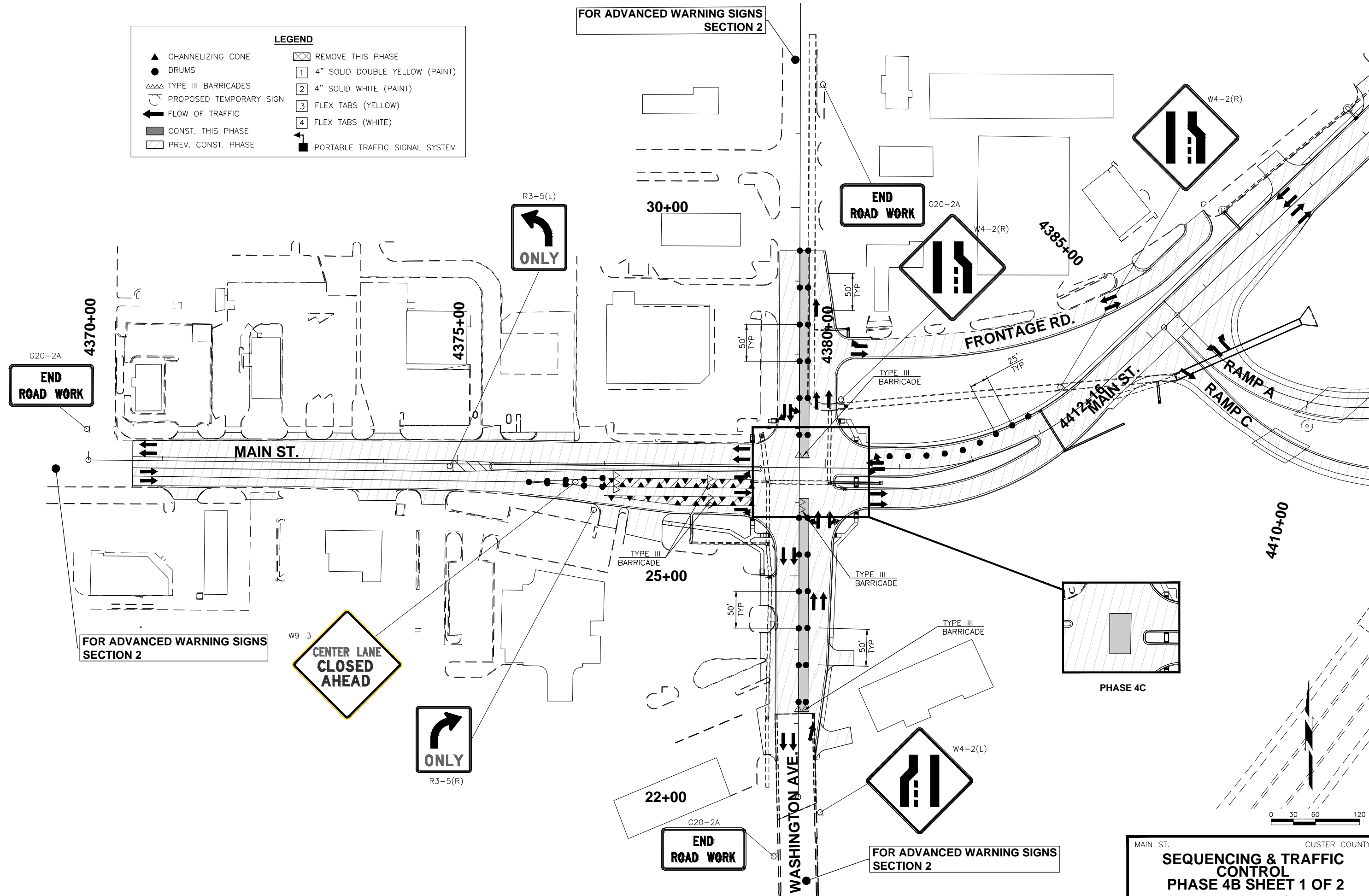
SEQUENCING & TRAFFIC CONTROL
PHASE 4A SHEET 2 OF 2

JOB PIECE NO. 27911(06) SHEET NO. 69

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LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
◁ PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
→ FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	🚦 PORTABLE TRAFFIC SIGNAL SYSTEM
▩ PREV. CONST. PHASE	

FOR ADVANCED WARNING SIGNS SECTION 2



MAIN ST. CUSTER COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 4B SHEET 1 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 70

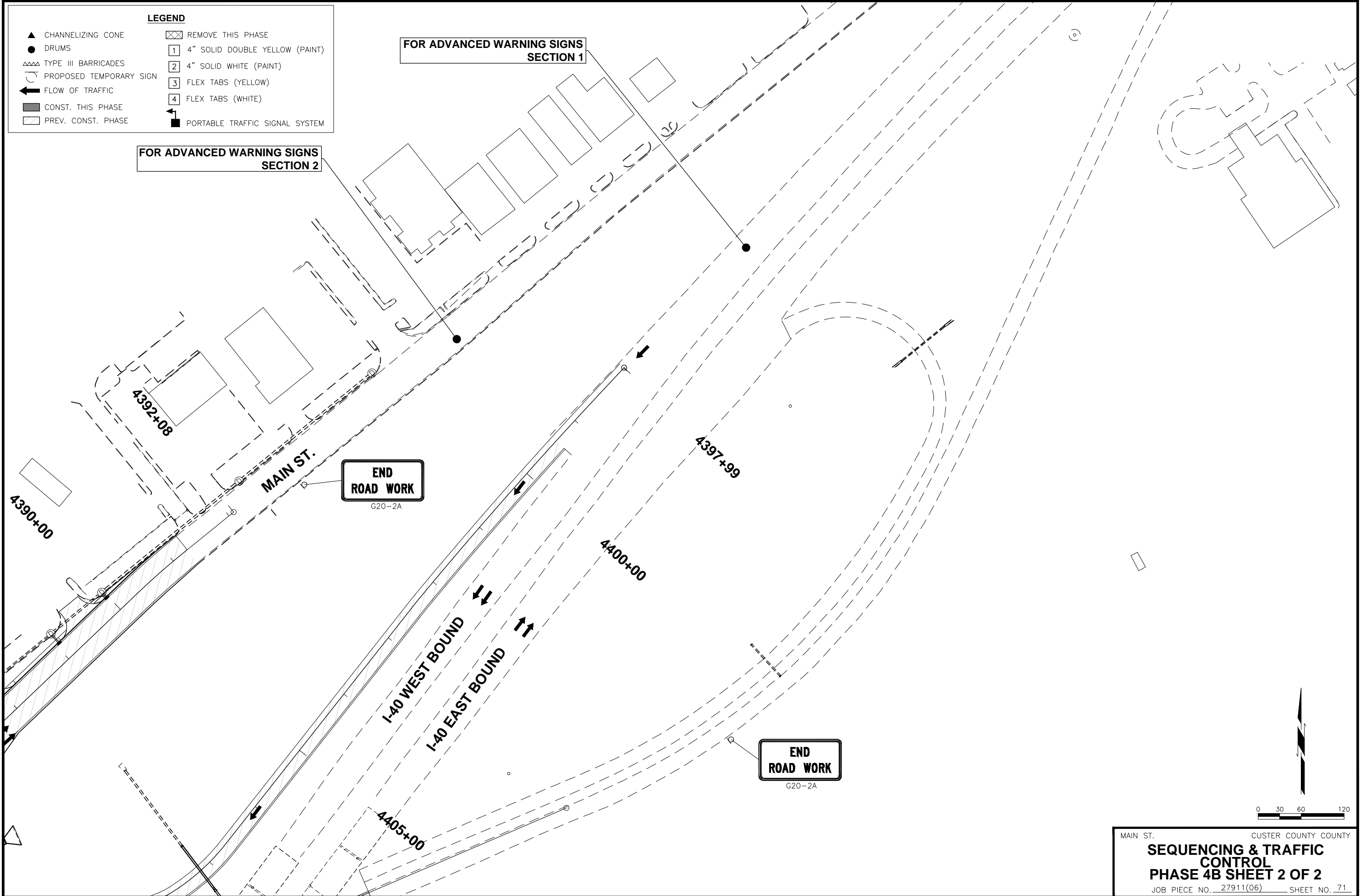
LEGEND	
▲ CHANNELIZING CONE	☒ REMOVE THIS PHASE
● DRUMS	1 4" SOLID DOUBLE YELLOW (PAINT)
△△△ TYPE III BARRICADES	2 4" SOLID WHITE (PAINT)
PROPOSED TEMPORARY SIGN	3 FLEX TABS (YELLOW)
← FLOW OF TRAFFIC	4 FLEX TABS (WHITE)
▨ CONST. THIS PHASE	■ PORTABLE TRAFFIC SIGNAL SYSTEM
▨ PREV. CONST. PHASE	

FOR ADVANCED WARNING SIGNS
SECTION 1

FOR ADVANCED WARNING SIGNS
SECTION 2

END
ROAD WORK
G20-2A

END
ROAD WORK
G20-2A



MAIN ST. CUSTER COUNTY COUNTY
SEQUENCING & TRAFFIC CONTROL
PHASE 4B SHEET 2 OF 2
 JOB PIECE NO. 27911(06) SHEET NO. 71



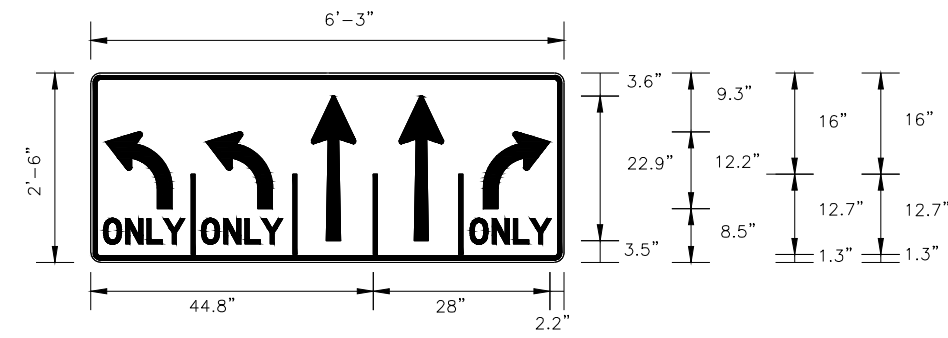
SPECIAL SIGN NO. 1

Dimensions are in inches.tenths Letter locations are panel edge to lower left corner

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE
S	T	A	Y	A	L	E	R	T			
3.0	7.3	11.1	16.0	23.4	28.3	32.1	36.2	41.1		42.0	
P	L	E	A	S	E						
13.1	17.0	20.2	23.6	27.7	31.2					21.3	
I	N	W	O	R	K	Z	O	N	E		
4.6	6.6	12.5	17.2	21.1	24.5	29.8	32.5	36.4	40.3	38.8	

SIGN NUMBER	STAY ALERT
WIDTH x HGHT.	4'-0" x 4'-0"
BORDER WIDTH	1.50"
CORNER RADIUS	2.25"
MOUNTING	Metal Post

SYMBOL	ROT	X	Y	WID	HT
	23.4	24.0	17.1	12.0	12.0



SPECIAL SIGN NO. 2

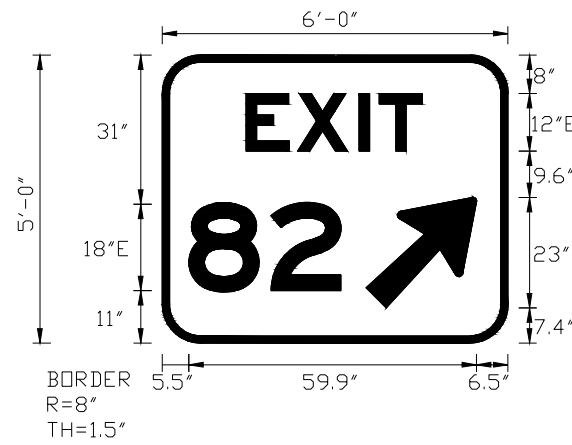
BORDER
R=1.13"
TH=0.63"
IN=0.38"

Dimensions are in inches.tenths Letter locations are panel edge to lower left corner

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE

SIGN NUMBER	ARROWS
WIDTH x HGHT.	6'-3" x 2'-6"
BORDER WIDTH	0.63"
CORNER RADIUS	0"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: White
LEGEND/BORDER	TYPE: Reflective COLOR: /Black

SYMBOL	ROT	X	Y	WID	HT
R3_5A	0	0	0	10.5	12.2
AR_Type A Extended	0	48.8	3.5	7.1	22.9



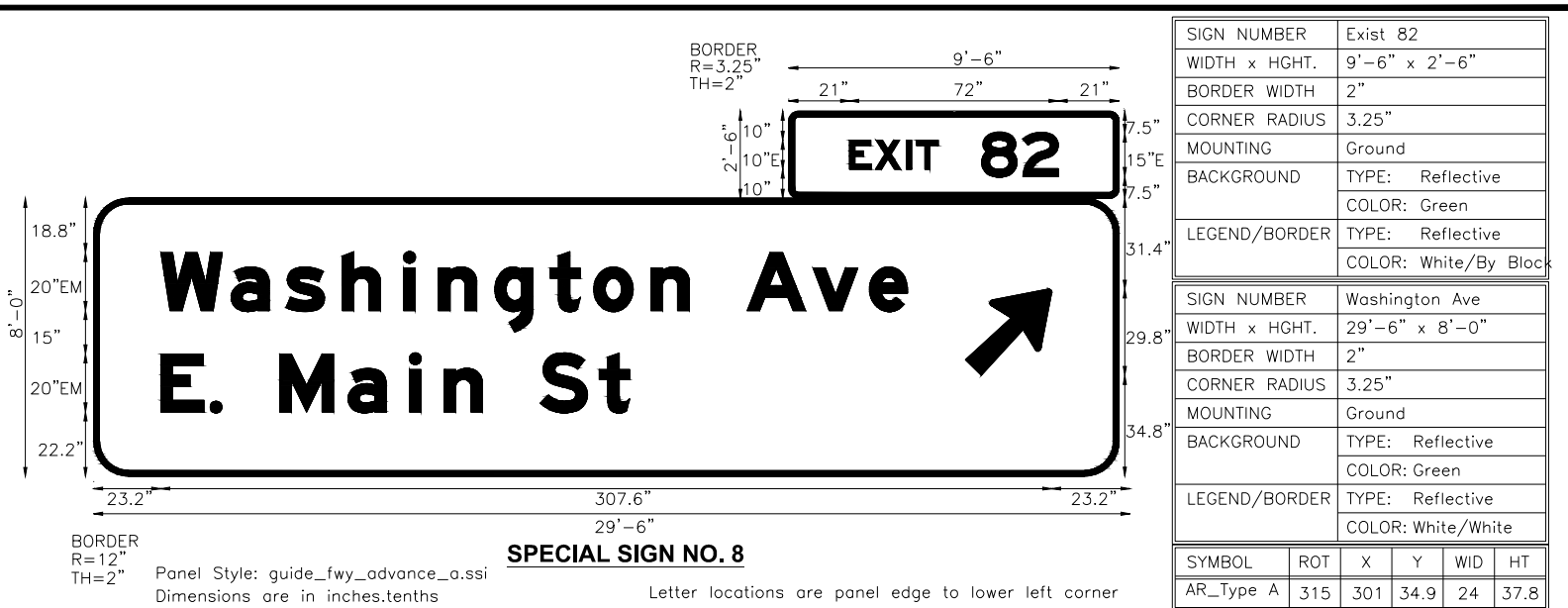
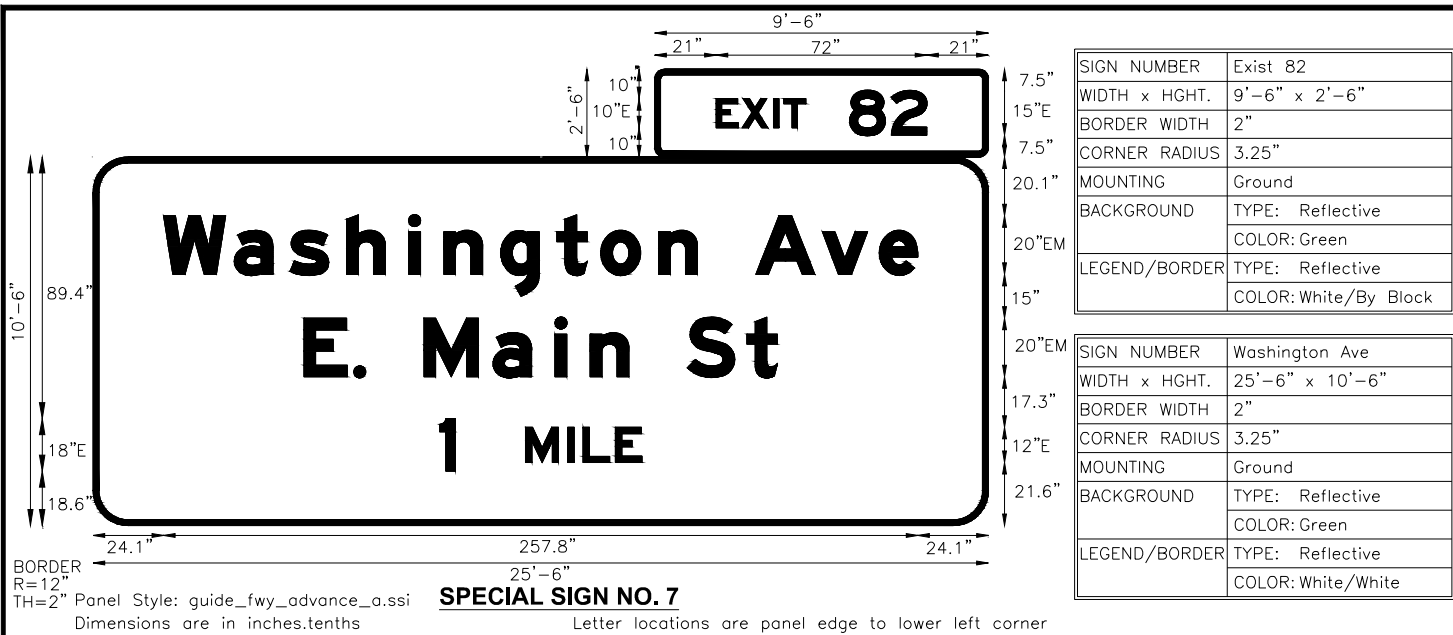
SPECIAL SIGN NO. 3

Dimensions are in inches.tenths Letter locations are panel edge to lower left corner

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE
E	X	I	T								E 2000
17.5	28.2	41	45.2							36.7	12
8	2										E 2000
5.5	22.6									31.7	18

SIGN NUMBER	EXIT
WIDTH x HGHT.	6'-0" x 5'-0"
BORDER WIDTH	1.5"
CORNER RADIUS	0"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Green
LEGEND/BORDER	TYPE: Reflective COLOR: White/White

SYMBOL	ROT	X	Y	WID	HT
AR_Type A	315	42.5	7.4	18.5	29.2



LETTER POSITIONS (X)													LENGTH	SERIES/SIZE		
E	X	I	T	8	2											E 2000
21	29.9	40.6	44.1	66.6	80.8											72 10,15
W	a	s	h	i	n	g	t	o	n	A	v	e				EM 2000
24.1	48.3	67.3	86.7	107.9	119.9	139.3	158.5	173.5	193.3	206.5	226.5	249.3	268.7			257.8 20/15
E	.	M	a	i	n	S	t									EM 2000
72.1	89.1	93.1	113.1	136.7	157.9	169.9	183.1	203.1	223.5							161.8 20/15
1	M	I	L	E												E 2000
118.4	147.8	162.2	167.5	178.6												69.1 18,12

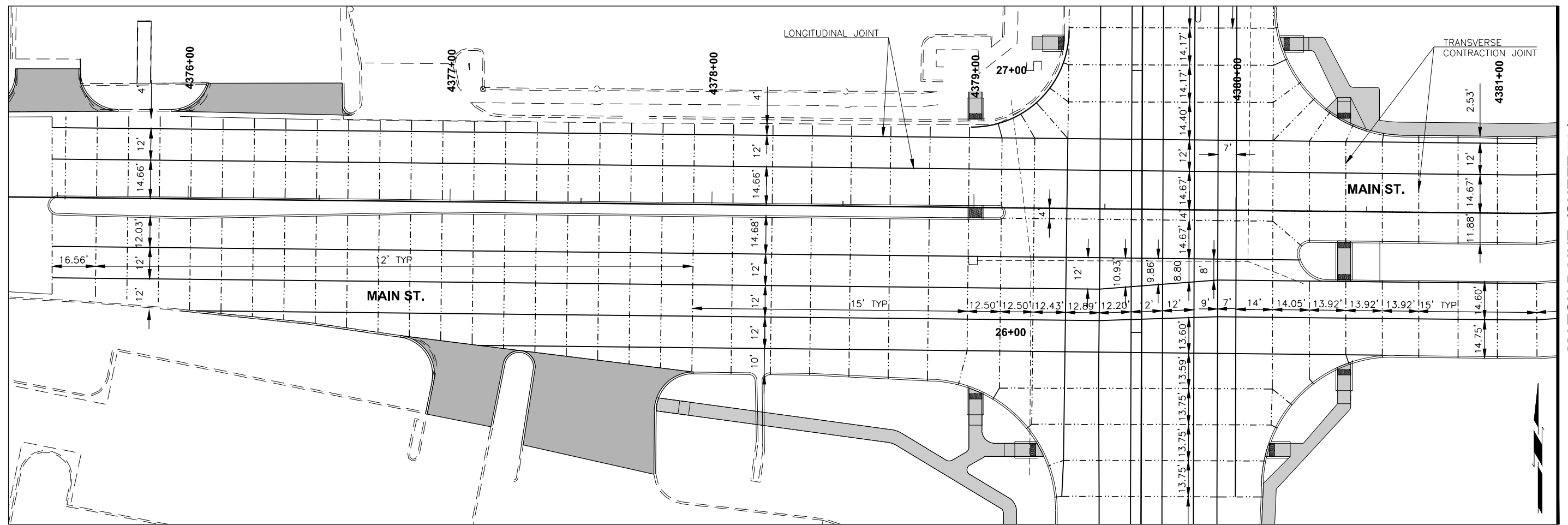
LETTER POSITIONS (X)													LENGTH	SERIES/SIZE		
E	X	I	T	8	2											E 2000
21	29.9	40.6	44.1	66.6	80.8											72 10,15
W	a	s	h	i	n	g	t	o	n	A	v	e				EM 2000
23.2	47.4	66.4	85.8	107	119	138.4	157.6	172.6	192.4	205.6	225.6	248.4	267.8			257.8 20/15
E	.	M	a	i	n	S	t									EM 2000
23.2	40.2	44.2	64.2	87.8	109	121	134.2	154.2	174.6							161.8 20/15



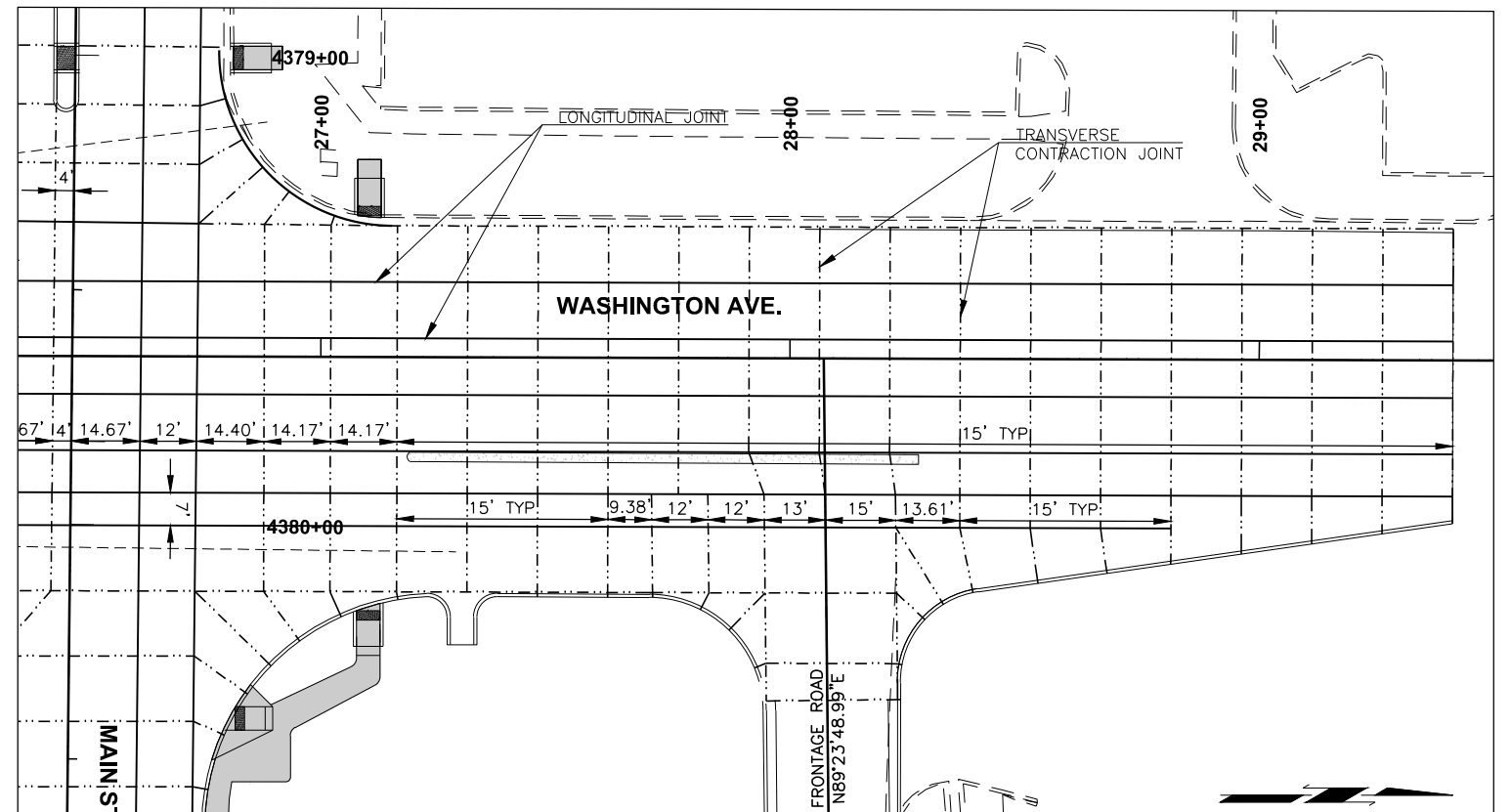
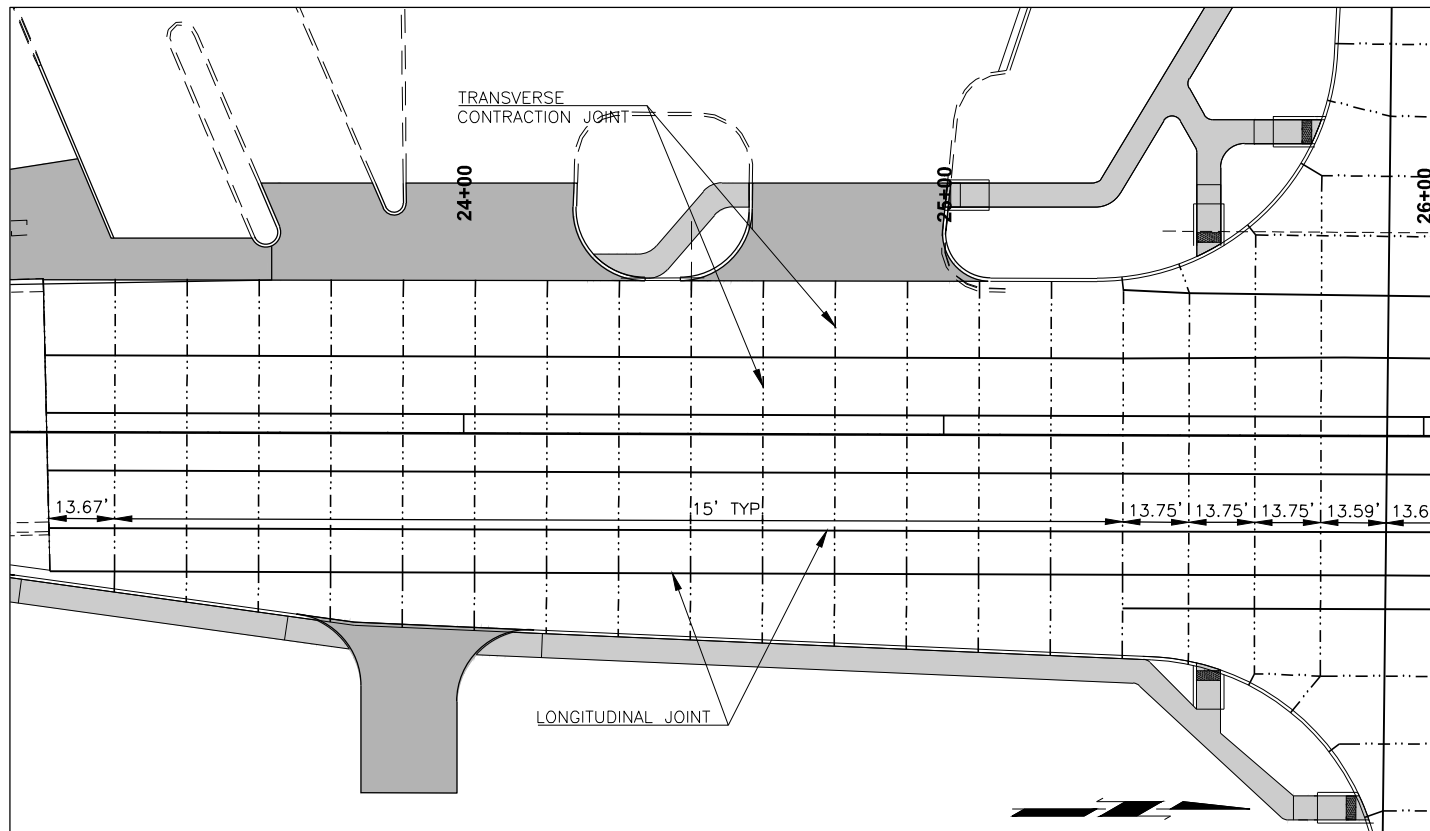
LETTER POSITIONS (X)													LENGTH	SERIES/SIZE		
E	.	M	a	i	n	S	t									D 2000
23.8	28	29.1	35.1	40.8	45.4	47.5	51.1	57.1	61.5							40.1 6/4.5

LETTER POSITIONS (X)													LENGTH	SERIES/SIZE		
W	a	s	h	i	n	g	t	o	n	A	v	e				D 2000
22.6	28.4	32.6	36.2	40.9	43	47.5	51.7	54.8	59.2	62.7	68.7	74.1	78.9			59.8 6/4.5
E	.	M	a	i	n	S	t									D 2000
22.6	26.9	27.9	33.9	39.6	44.2	46.4	49.9	55.9	60.3							40.1 6/4.5

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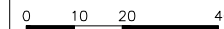


MATCH POINT (SHEET 76)



LEGEND

- LONGITUDINAL JOINT
- - - - TRANSVERSE CONTRACTION JOINT

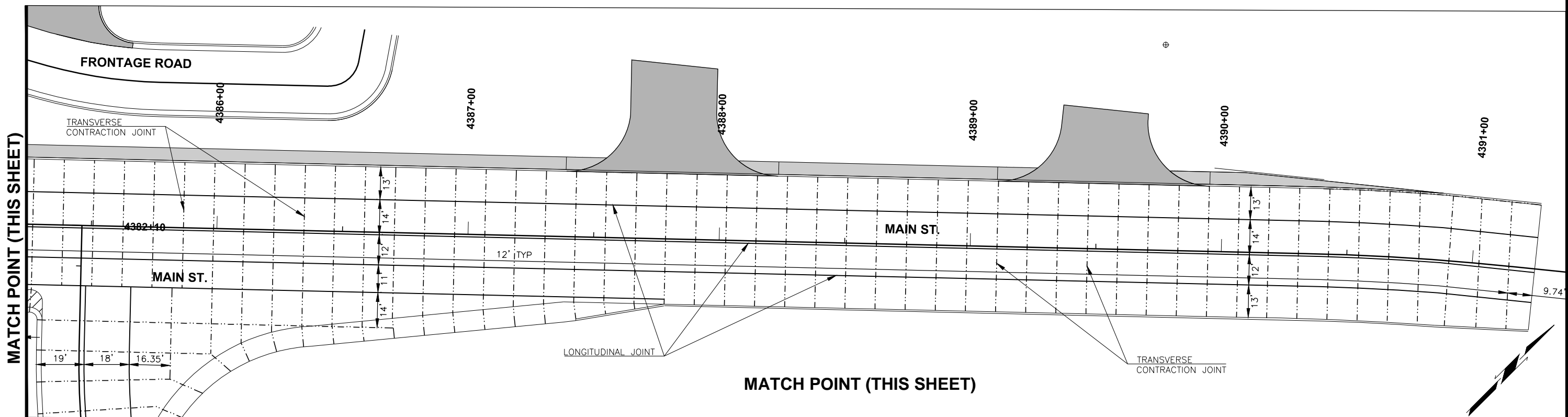
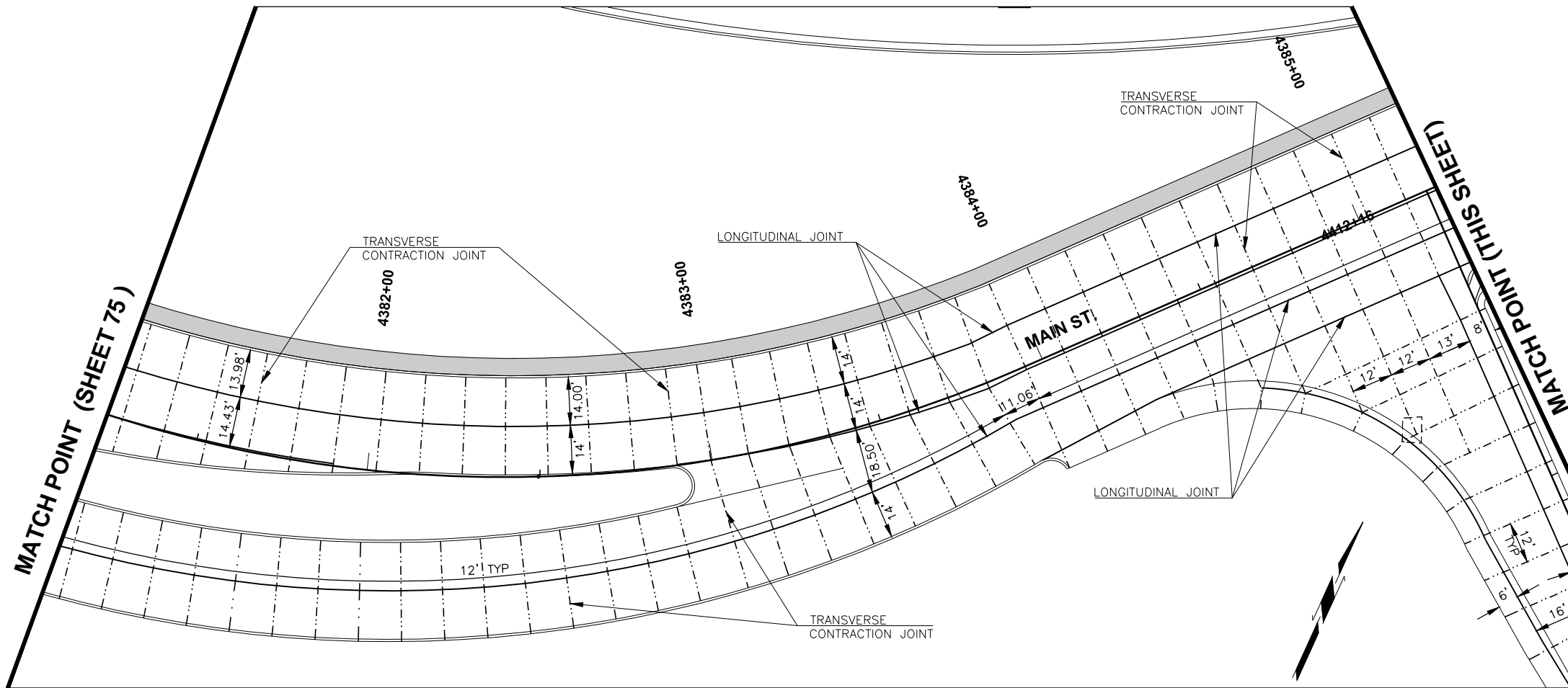


WEATHERFORD I40 RAMP PHASE 2 CUSTER COUNTY

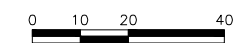
JOINT LAYOUT SHEET 1 OF 4

JOB PIECE NO. 27911(06) SHEET NO. 75

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LEGEND	
	LONGITUDINAL JOINT
	TRANSVERSE CONTRACTION JOINT



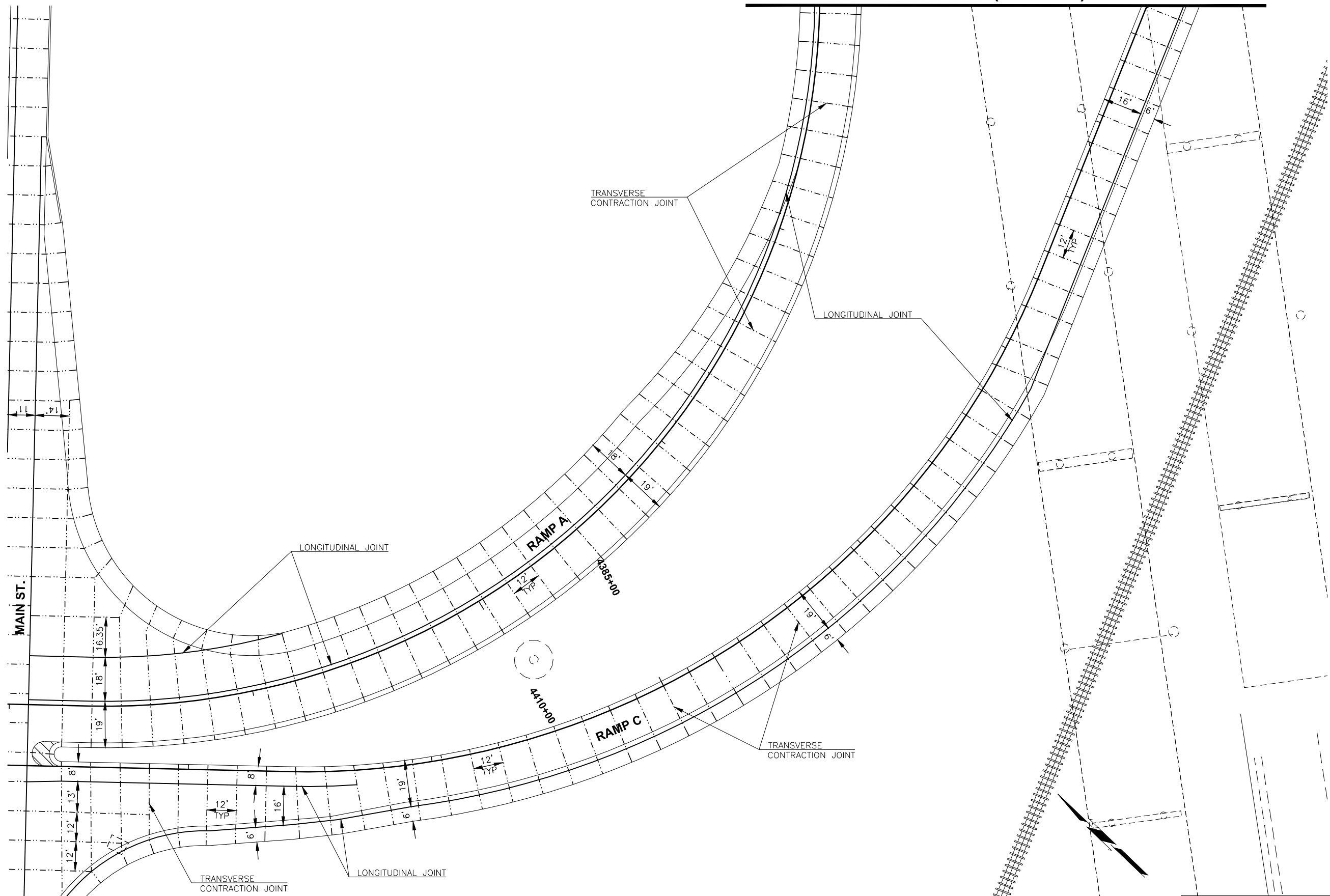
WEATHERFORD I40 RAMP PHASE 2 CUSTER COUNTY

JOINT LAYOUT SHEET 2 OF 4

JOB PIECE NO. 27911(06) SHEET NO. 76

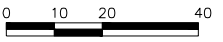
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MATCH POINT (SHEET 77)



LEGEND

- LONGITUDINAL JOINT
- - - TRANSVERSE CONTRACTION JOINT



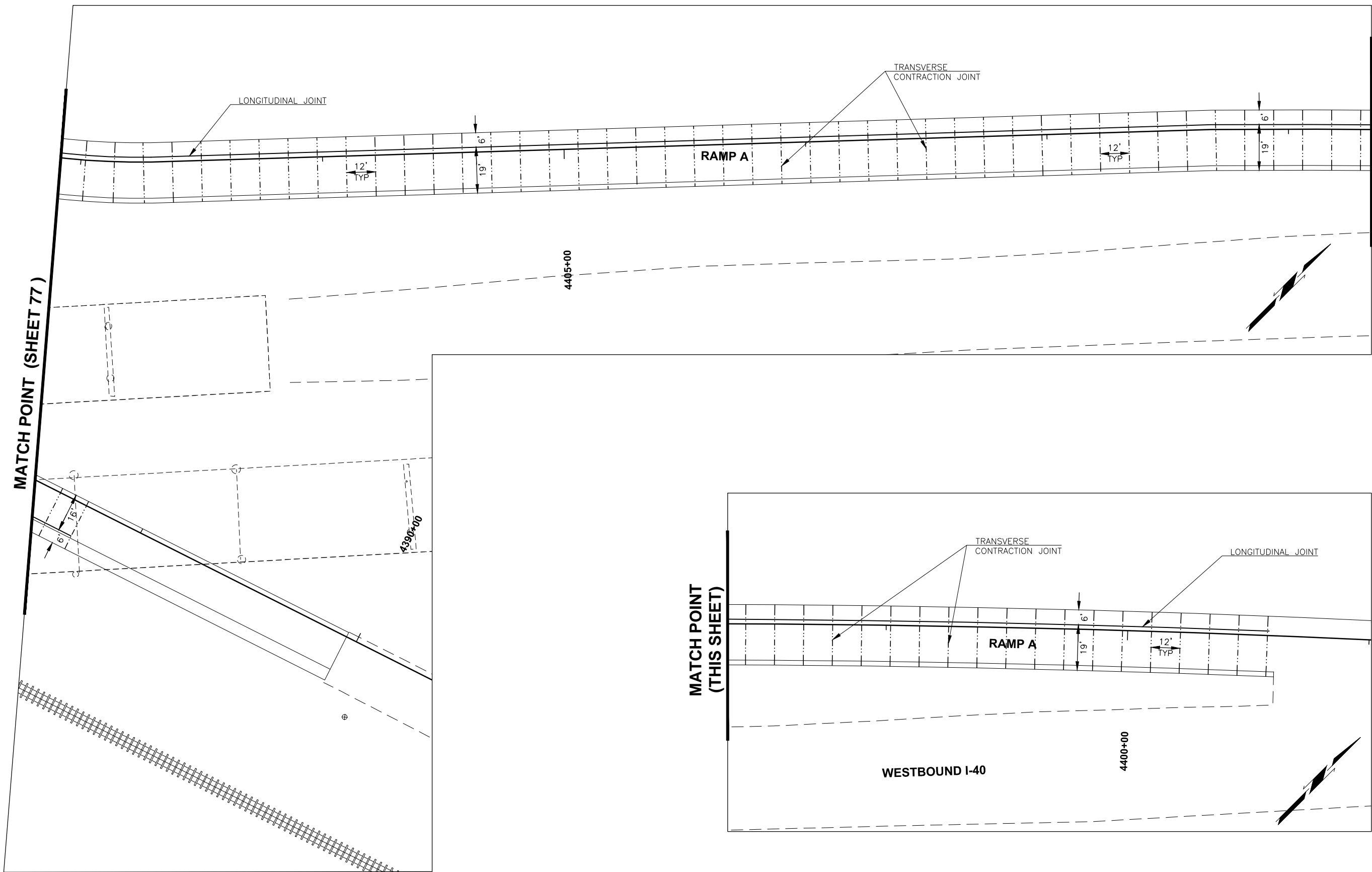
WEATHERFORD I40 RAMPS PHASE 2 CUSTER COUNTY

JOINT LAYOUT SHEET 3 OF 4

JOB PIECE NO. 27911(06) SHEET NO. 77

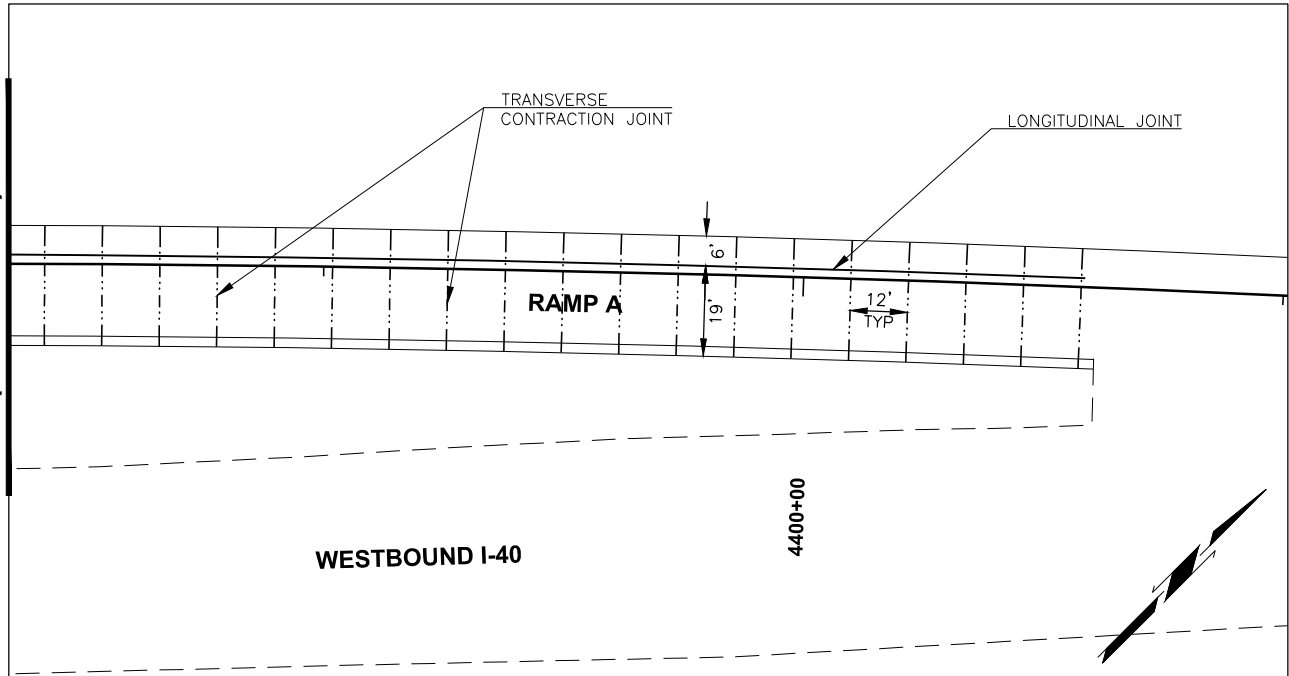
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MATCH POINT (SHEET 77)

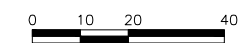


MATCH POINT (THIS SHEET)

MATCH POINT (THIS SHEET)

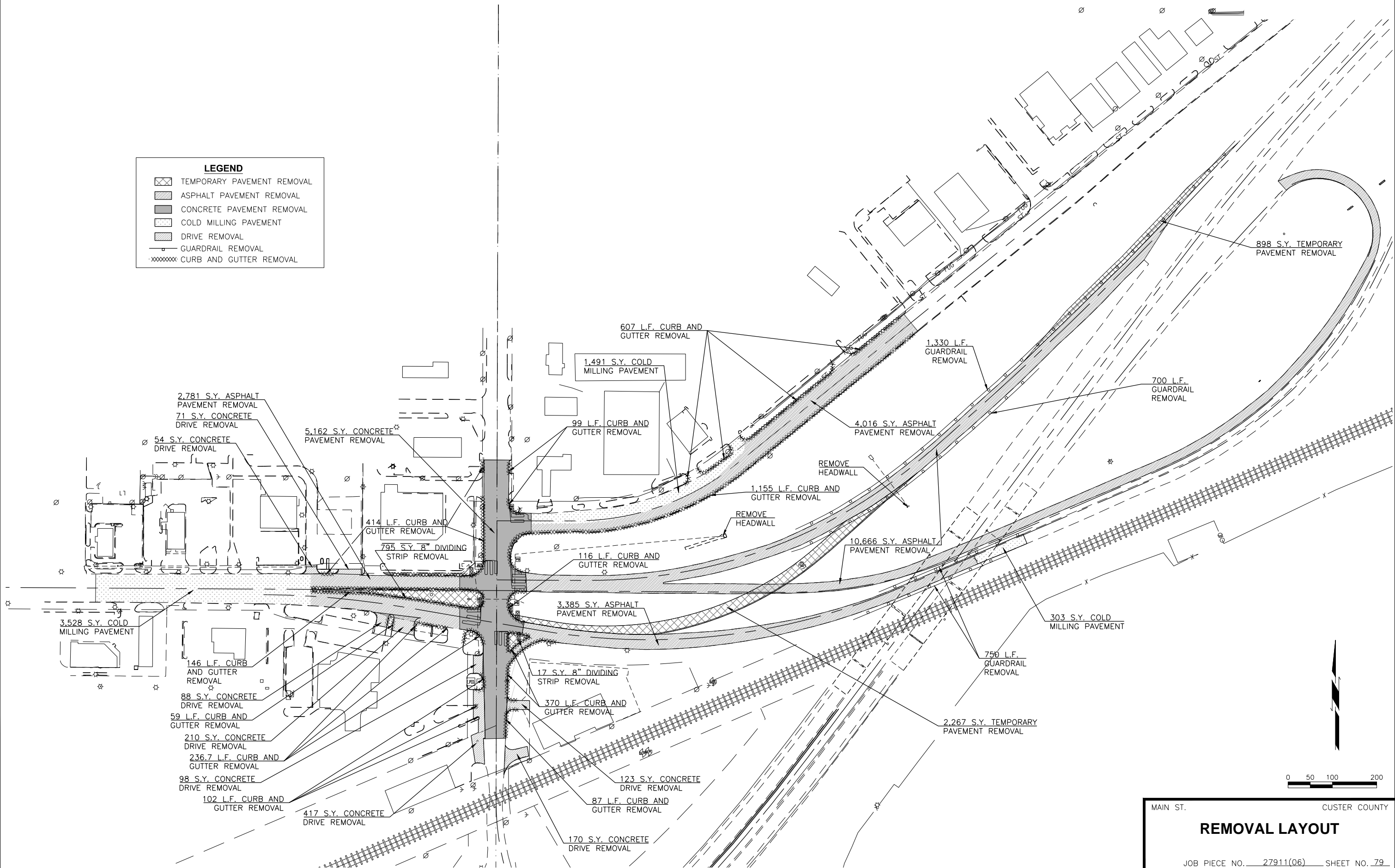


LEGEND	
———	LONGITUDINAL JOINT
- - - - -	TRANSVERSE CONTRACTION JOINT



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LEGEND	
	TEMPORARY PAVEMENT REMOVAL
	ASPHALT PAVEMENT REMOVAL
	CONCRETE PAVEMENT REMOVAL
	COLD MILLING PAVEMENT
	DRIVE REMOVAL
	GUARDRAIL REMOVAL
	CURB AND GUTTER REMOVAL

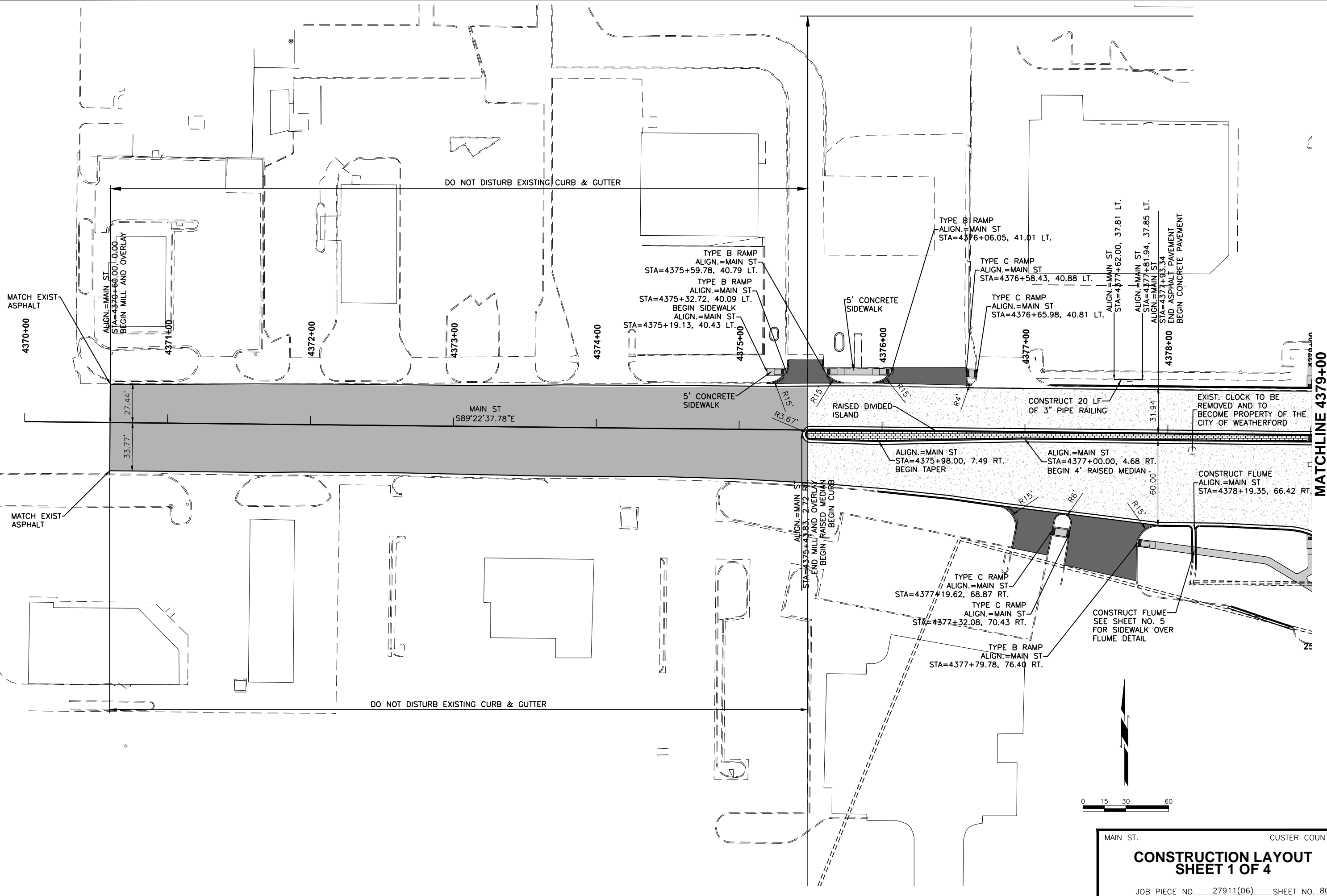


MAIN ST. CUSTER COUNTY

REMOVAL LAYOUT

JOB PIECE NO. 27911(06) SHEET NO. 79

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MATCH EXIST- ASPHALT

MATCH EXIST- ASPHALT

DO NOT DISTURB EXISTING CURB & GUTTER

DO NOT DISTURB EXISTING CURB & GUTTER

MAIN ST
S89°22'37.78"E

5' CONCRETE SIDEWALK

5' CONCRETE SIDEWALK

RAISED DIVIDED ISLAND

CONSTRUCT 20 LF OF 3" PIPE RAILING

EXIST. CLOCK TO BE REMOVED AND TO BECOME PROPERTY OF THE CITY OF WEATHERFORD

ALIGN.=MAIN ST
STA=4375+98.00, 7.49 RT.
BEGIN TAPER

ALIGN.=MAIN ST
STA=4377+00.00, 4.68 RT.
BEGIN 4' RAISED MEDIAN

CONSTRUCT FLUME
ALIGN.=MAIN ST
STA=4378+19.35, 66.42 RT

TYPE C RAMP
ALIGN.=MAIN ST
STA=4377+19.62, 68.87 RT.

TYPE C RAMP
ALIGN.=MAIN ST
STA=4377+32.08, 70.43 RT.

TYPE B RAMP
ALIGN.=MAIN ST
STA=4377+79.78, 76.40 RT.

TYPE B RAMP
ALIGN.=MAIN ST
STA=4376+06.05, 41.01 LT.

TYPE C RAMP
ALIGN.=MAIN ST
STA=4376+58.43, 40.88 LT.

TYPE C RAMP
ALIGN.=MAIN ST
STA=4376+65.98, 40.81 LT.

ALIGN.=MAIN ST
STA=4377+62.00, 37.81 LT.

ALIGN.=MAIN ST
STA=4377+81.94, 37.85 LT.

ALIGN.=MAIN ST
STA=4377+93.34
END ASPHALT PAVEMENT
BEGIN CONCRETE PAVEMENT

ALIGN.=MAIN ST
STA=4370+60.00-10.00
BEGIN MILL AND OVERLAY

4371+00

4372+00

4373+00

4374+00

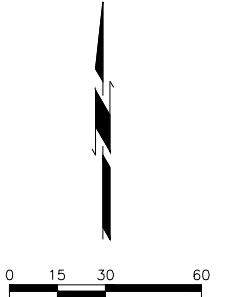
4375+00

4376+00

4377+00

4378+00

MATCHLINE 4379+00

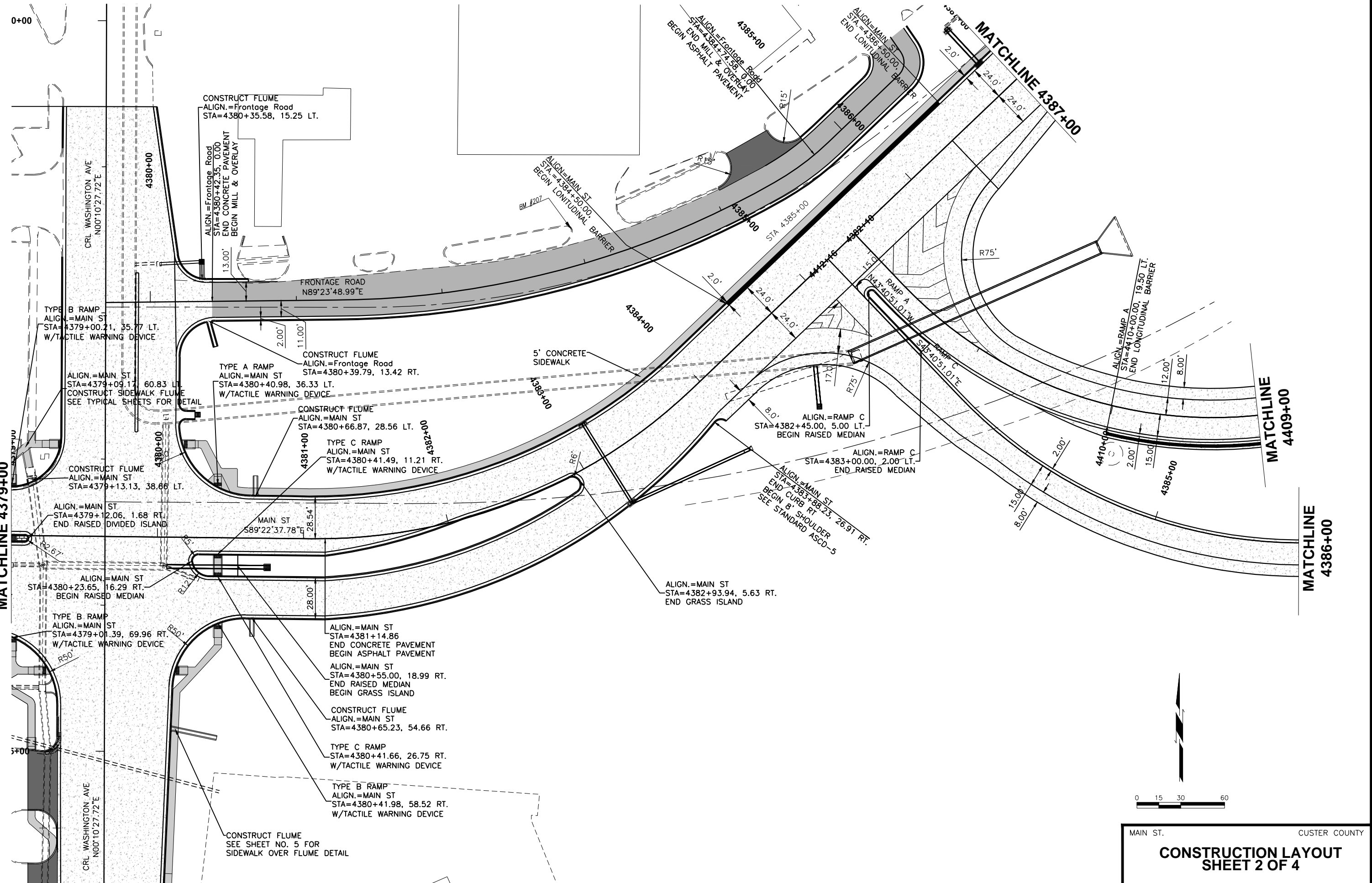


MAIN ST. CUSTER COUNTY

CONSTRUCTION LAYOUT SHEET 1 OF 4

JOB PIECE NO. 27911(06) SHEET NO. 80

7/8/2016 12:12:46 PM H:\KOC\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\0019.04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\GENERAL SIDEWALK LAYOUT.DWG



0+00

MATCHLINE 4379+00

5+00

CRL WASHINGTON AVE
N00°10'27.72"E

CONSTRUCT FLUME
ALIGN.=Frontage Road
STA=4380+35.58, 15.25 LT.

ALIGN.=Frontage Road
STA=4380+42.35, 0.00
END CONCRETE PAVEMENT
BEGIN MILL & OVERLAY

FRONTAGE ROAD
N89°23'48.99"E

TYPE B RAMP
ALIGN.=MAIN ST
STA=4379+00.21, 35.77 LT.
W/TACTILE WARNING DEVICE

ALIGN.=MAIN ST
STA=4379+09.17, 60.83 LT.
CONSTRUCT SIDEWALK FLUME
SEE TYPICAL SHEETS FOR DETAIL

CONSTRUCT FLUME
ALIGN.=MAIN ST
STA=4379+13.13, 38.66 LT.

ALIGN.=MAIN ST
STA=4379+12.06, 1.68 RT.
END RAISED DIVIDED ISLAND

ALIGN.=MAIN ST
STA=4380+23.65, 16.29 RT.
BEGIN RAISED MEDIAN

TYPE B RAMP
ALIGN.=MAIN ST
STA=4379+01.39, 69.96 RT.
W/TACTILE WARNING DEVICE

CRL WASHINGTON AVE
N00°10'27.72"E

CONSTRUCT FLUME
SEE SHEET NO. 5 FOR
SIDEWALK OVER FLUME DETAIL

CONSTRUCT FLUME
ALIGN.=Frontage Road
STA=4380+39.79, 13.42 RT.

TYPE A RAMP
ALIGN.=MAIN ST
STA=4380+40.98, 36.33 LT.
W/TACTILE WARNING DEVICE

CONSTRUCT FLUME
ALIGN.=MAIN ST
STA=4380+66.87, 28.56 LT.

TYPE C RAMP
ALIGN.=MAIN ST
STA=4380+41.49, 11.21 RT.
W/TACTILE WARNING DEVICE

ALIGN.=MAIN ST
STA=4381+14.86
END CONCRETE PAVEMENT
BEGIN ASPHALT PAVEMENT

ALIGN.=MAIN ST
STA=4380+55.00, 18.99 RT.
END RAISED MEDIAN
BEGIN GRASS ISLAND

CONSTRUCT FLUME
ALIGN.=MAIN ST
STA=4380+65.23, 54.66 RT.

TYPE C RAMP
STA=4380+41.66, 26.75 RT.
W/TACTILE WARNING DEVICE

TYPE B RAMP
ALIGN.=MAIN ST
STA=4380+41.98, 58.52 RT.
W/TACTILE WARNING DEVICE

ALIGN.=Frontage Road
STA=4384+74.55, 0.00
BEGIN ASPHALT PAVEMENT

ALIGN.=MAIN ST
STA=4384+50.00
BEGIN LONGITUDINAL BARRIER

ALIGN.=MAIN ST
STA=4384+50.00
END LONGITUDINAL BARRIER

4384+00

5' CONCRETE SIDEWALK

4385+00

ALIGN.=RAMP C
STA=4382+45.00, 5.00 LT.
BEGIN RAISED MEDIAN

ALIGN.=RAMP C
STA=4383+00.00, 2.00 LT.
END RAISED MEDIAN

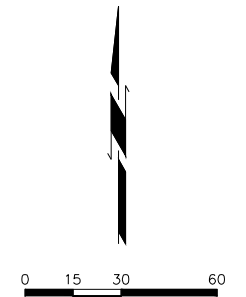
ALIGN.=MAIN ST
STA=4383+88.23, 26.97 RT.
BEGIN CURB RT
SEE STANDARD ASCO-5

ALIGN.=MAIN ST
STA=4382+93.94, 5.63 RT.
END GRASS ISLAND

MATCHLINE 4387+00

MATCHLINE 4409+00

MATCHLINE 4386+00

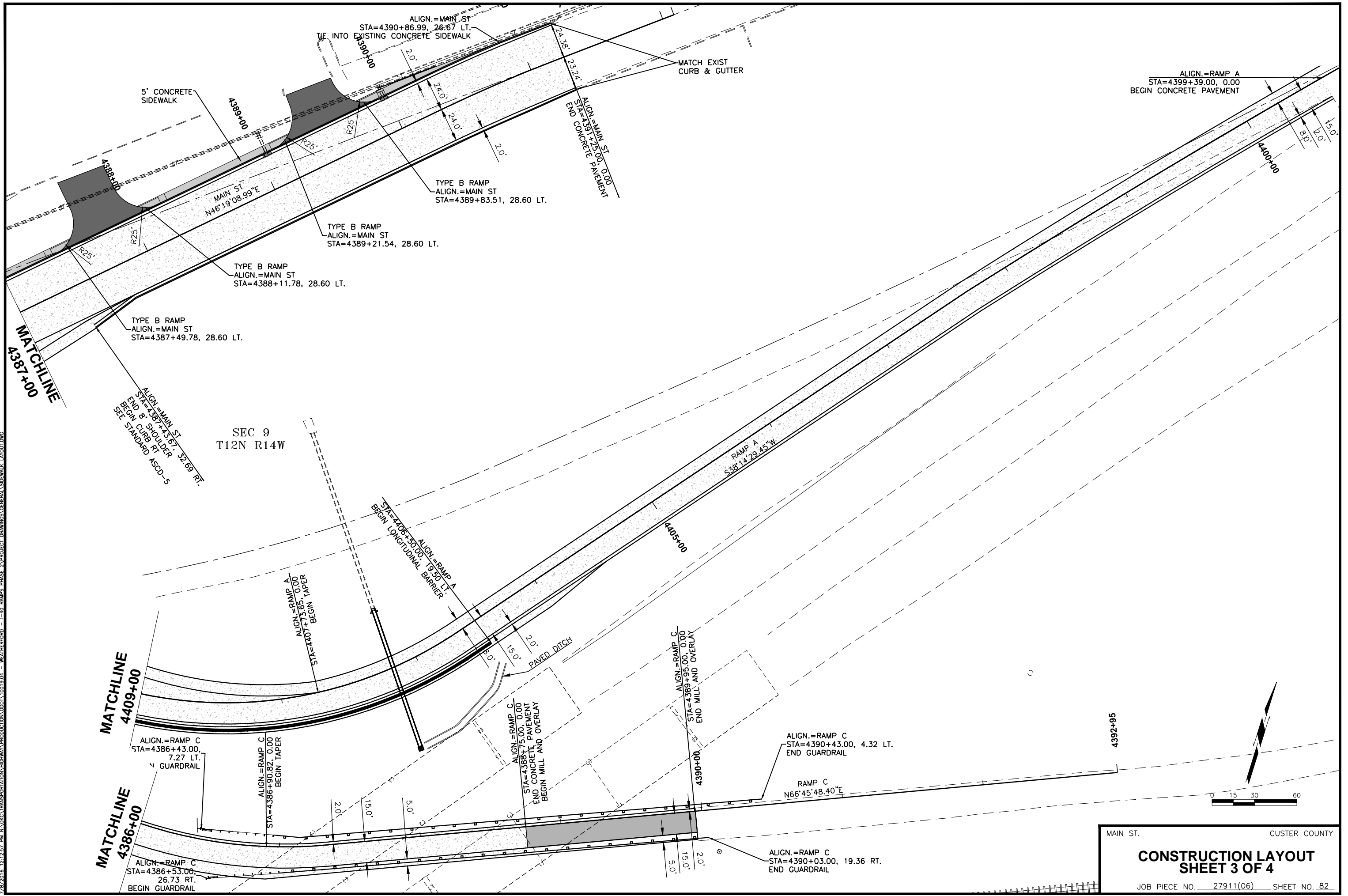


MAIN ST. CUSTER COUNTY

CONSTRUCTION LAYOUT SHEET 2 OF 4

JOB PIECE NO. 27911(06) SHEET NO. 81

7/8/2016 12:12:57 PM N:\KCC\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\10019.04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\GENERAL\SIDEWALK LAYOUT.DWG



5' CONCRETE SIDEWALK

ALIGN.=MAIN ST
STA=4390+86.99, 26.67 LT.
TIE INTO EXISTING CONCRETE SIDEWALK

ALIGN.=RAMP A
STA=4399+39.00, 0.00
BEGIN CONCRETE PAVEMENT

MAIN ST
N46°19'08.99"E

TYPE B RAMP
ALIGN.=MAIN ST
STA=4389+83.51, 28.60 LT.

TYPE B RAMP
ALIGN.=MAIN ST
STA=4389+21.54, 28.60 LT.

TYPE B RAMP
ALIGN.=MAIN ST
STA=4388+11.78, 28.60 LT.

TYPE B RAMP
ALIGN.=MAIN ST
STA=4387+49.78, 28.60 LT.

ALIGN.=MAIN ST
STA=4387+43.67, 32.69 RT.
END B SHOULDER
BEGIN CURB RT
SEE STANDARD ASCD-5

SEC 9
T12N R14W

ALIGN.=RAMP A
STA=4407+75.65, 0.00
BEGIN TAPER

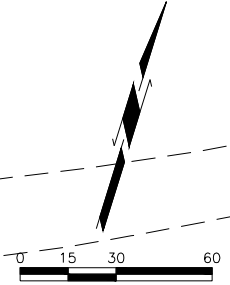
ALIGN.=RAMP C
STA=4388+15.00, 0.00
END CONCRETE PAVEMENT
BEGIN MILL AND OVERLAY

ALIGN.=RAMP C
STA=4389+95.00, 0.00
END MILL AND OVERLAY

ALIGN.=RAMP C
STA=4390+43.00, 4.32 LT.
END GUARDRAIL

RAMP C
N66°45'48.40"E

ALIGN.=RAMP C
STA=4390+03.00, 19.36 RT.
END GUARDRAIL

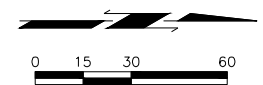
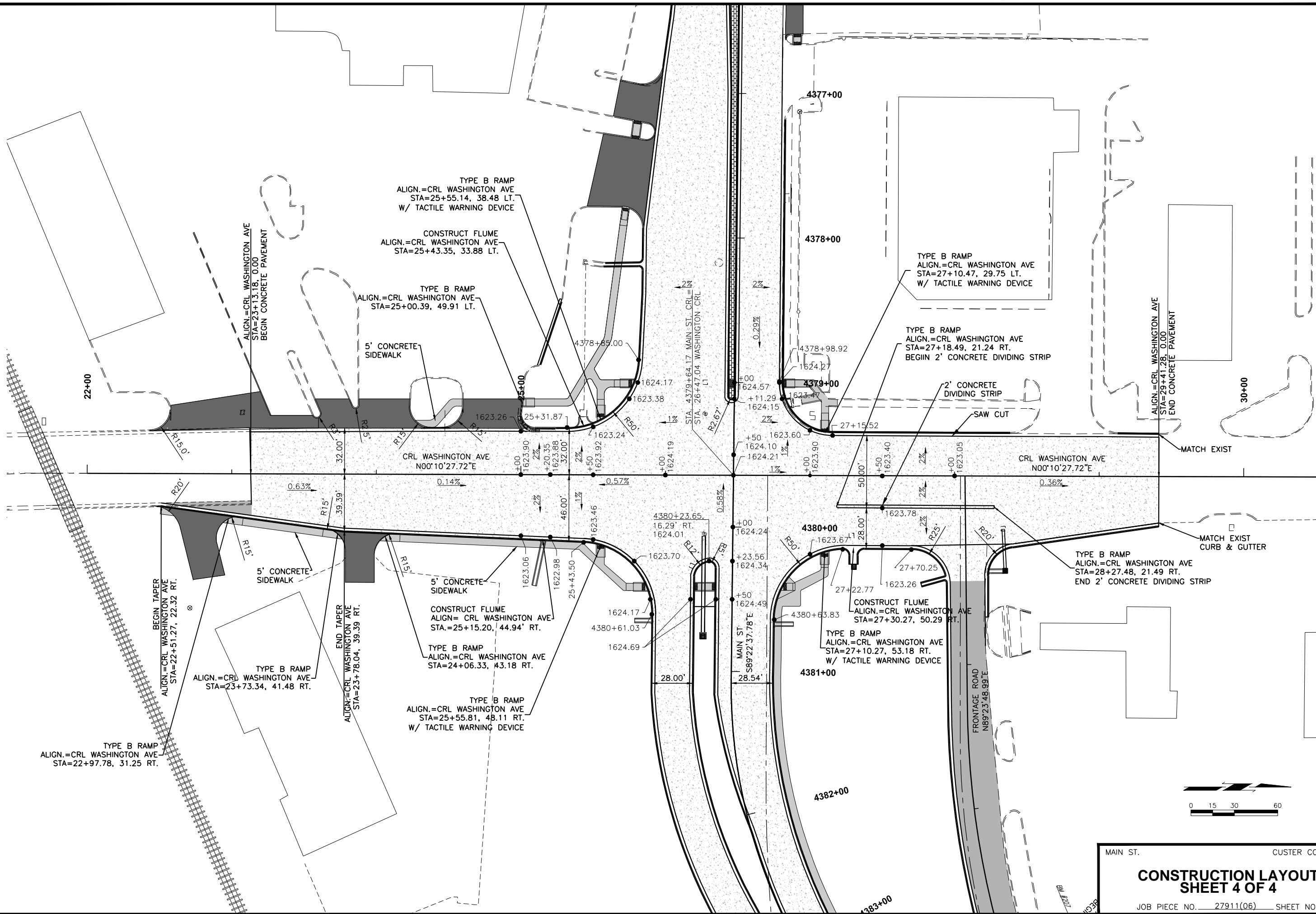


MAIN ST. CUSTER COUNTY

CONSTRUCTION LAYOUT SHEET 3 OF 4

JOB PIECE NO. 27911(06) SHEET NO. 82

7/8/2016 12:13:06 PM N:\KOC\TRANSPORTATION\HIGHWAY\PRODUCTION\0001\10019.04 - WEATHERFORD - I-40 RAMP/PHASE 2\PROJECT DRAWINGS\GENERAL\SIDEWALK LAYOUT.DWG



MAIN ST. CUSTER COUNTY
CONSTRUCTION LAYOUT
SHEET 4 OF 4
JOB PIECE NO. 27911(06) SHEET NO. 83

GEOLOGICAL STATEMENT

Division Five of the "Engineering Classification of Geological Materials", published by the Oklahoma Department of Transportation (ODOT) indicates the project site is located over Terrace Deposits (Qts) underlain by the Rush Springs Unit (Prs). Terrace deposits consist of sand, silt, clay, gravel and/or mixtures of these. Terrace materials occur adjacent to or near streams at higher elevations than the flood plain (bottom land). Most Terrace deposits will have seepage where the underlying geologic material is less pervious.

The Rush Springs unit consists dominantly of soft, reddish-brown, massive, crossbedded to regular-bedded silty sandstone which weathers rapidly, producing a sandy soil which is often blown about by the wind and in some localities is piled into sand. A few dolomite and gypsum beds occur in the upper portions. These beds are generally less than 2 feet thick. One marker bed (Weatherford Dolomite) occurs some 24 to 52 feet below the top of the unit. It varies from 1 foot to 8 feet in thickness and grade from dolomite in eastern Custer and northern Washita Counties to gypsum elsewhere. The Weatherford Dolomite was previously correlated as the base of the Cloud Chief Unit as is the case in the Division 7 publication. The Rush Springs Unit is one of the best water aquifers in western Oklahoma.

The total thickness of the unit varies from 186 to 430 feet with the unit generally thinning northwestward from the Custer-Washita Counties area.

In Division 5 the Rush Springs Unit outcrops in the east-west band two to six miles wide on the south flank of the Anadarko Basin in Southern Washita and northeaster Kiowa Counties. On the north flank of the basin, it outcrops over broad band which extend across northeastern Washita, western Blaine, Custer Dewey, and eastern Roger Mills Counties. In Beckham, Greet, and Harmon Counties, the strata of the Rush Springs Unit and the underlying Marlow Unit are similar and are mapped together as the Whitehorse Unit.

The Rush Springs Unit forms broad gently rolling topography dissected locally by rugged canyons throughout most of its outcrop. Rolling hills are prominent near its base with massive bluffs to rounded hills overlooking the underlying Marlow Unit.

According to the Geologic Map of the "Hydrologic Atlas 5 of Oklahoma," Reconnaissance of the Water Resources of the Clinton quadrangle, west-central Oklahoma," by Jerry E.Carr and DeRoy L. Bergman, U.S. Geological Survey, 1976, indicates that the project site is located over Terrace Deposits (Qt) underlain by Rush Springs Formation (Pr) of the Whitehorse Group.

Terrace deposits consists of stream-laid deposits of sand, silt, clay, and gravel which ranges in thickness from 0 to about 170 feet.

Rush Springs Formation consists of orange-brown, cross-bedded, fine-grained sandstone with some dolomite and gypsum beds. Thickness, about 300 feet, thinning northward to about 186 feet.



7042 Highwater Circle, Suite B
Edmond, Ok 73034
405-562-3328

BORING NUMBER B-1

PAGE 1 OF 1

CLIENT CEC PROJECT NAME I-40 Mast Light
 PROJECT NUMBER 15153 PROJECT LOCATION Weatherford, Custer County, Oklahoma
 DATE STARTED 10/20/15 COMPLETED 10/20/15 GROUND ELEVATION _____ STATION _____ OFFSET _____
 DRILLING CONTRACTOR DSO - Drilling Services of Oklahoma GROUND WATER LEVELS:
 DRILLING METHOD 4.5" augers - CME 55 DURING DRILLING none
 LOGGED BY RAG CHECKED BY KKB 0 hrs AFTER DRILLING none
 NOTES 27911(06) Cave In Depth open

ELEVATION (ft)	DEPTH (ft)	GRAPHIC LOG	MATERIAL DESCRIPTION	SAMPLE TYPE	BLOW COUNTS	MOISTURE CONTENT (%)	ATTERBERG LIMITS			PASSING #200 SIEVE (%)
							LIQUID LIMIT	PLASTIC LIMIT	PLASTICITY INDEX	
0			SILTY SAND, red, medium dense	SPT	20	2	0	0	NP	13.8
			POORLY GRADED SAND with SILT, red, loose	SPT	8	9	0	0	NP	5.1
5			SILTY SAND, red, loose to medium dense	SPT	6	13	0	0	NP	23.0
				TC	50/0.5" 50/0.3"					
				TC	50/0.5" 50/0.3"					
15			SILTY SANDSTONE, red, very well cemented	SPT	50/4"	21	0	0	NP	16.1
				TC	50/0.5" 50/0.3"					
				TC	50/0.5" 50/0.3"					
				TC	50/0.4" 50/0.3"					
			Boring Termination Depth = 24.5 feet Boring Completed and Grouted on 10/20/15	TC	50/0.4" 50/0.3"					

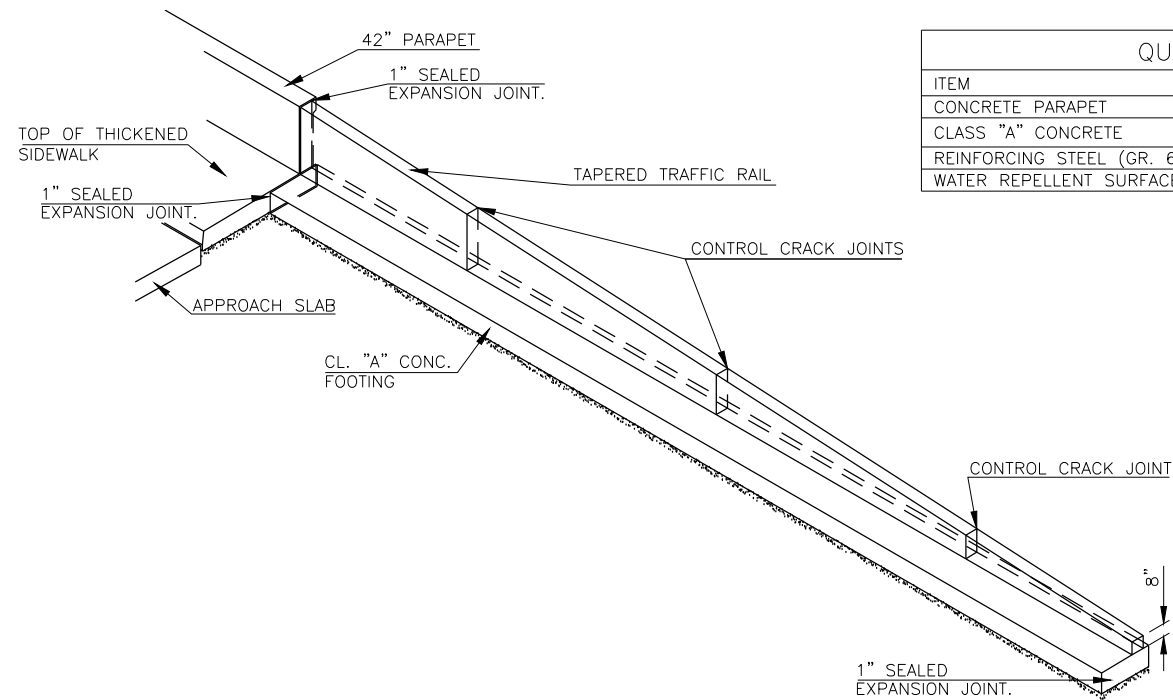
1 DURING AFTER CAVE IN NO ELEV 15153 LOGS.GPJ DATA TEMPLATE.GDT 2/9/16

MAIN ST. CUSTER COUNTY

**HIGH MAST LIGHT
FOUNDATION REPORT**

JOB PIECE NO. 27911(06) SHEET NO. 84

7/8/2016 12:13:12 PM N:\GCS\TRANSPORTATION\HIGHWAY\PRODUCTION\LOGS\15153\04 - WEATHERFORD - I-40 RAMP PHASE 2\PROJECT DRAWINGS\GENERAL\GEOTECH SHEET.DWG



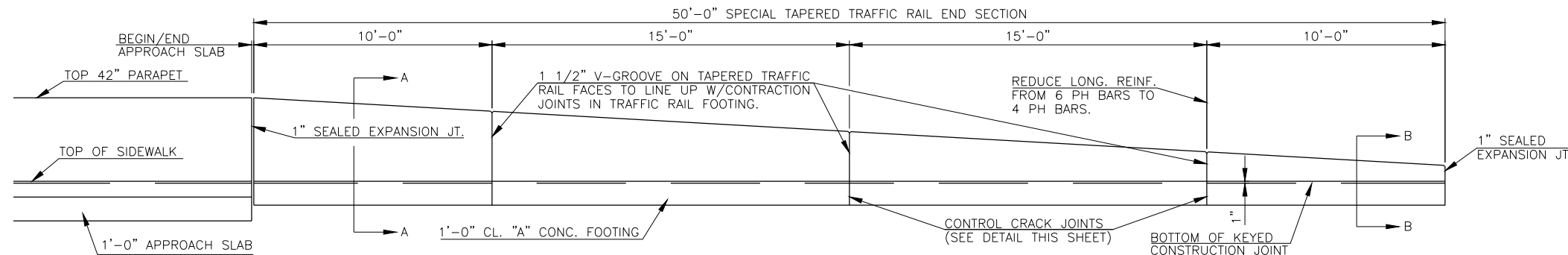
QUANTITIES		
ITEM	UNIT	TOTAL
CONCRETE PARAPET	L.F.	450.0
CLASS "A" CONCRETE	C.Y.	20.0
REINFORCING STEEL (GR. 60)	LB.	3072.0
WATER REPELLENT SURFACE TREAT.	SY	150.0

CLASS "A" CONCRETE FOOTING IS PAID FOR BY CUBIC YARD OF CLASS "A" CONCRETE AND POUNDS OF REINFORCING STEEL. ALL REINFORCING STEEL SHALL BE GRADE 60. ALL COST OF SAWING AND OTHER INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SPECIFIED TO BE INCLUDED IN OTHER ITEMS. THE TOP OF TRAFFIC RAIL FOOTING SHALL CONFORM TO SLOPE OF ROADWAY PAVEMENT. PAYMENT FOR THIS ITEM IS INCLUDED WITH BRIDGE PAY

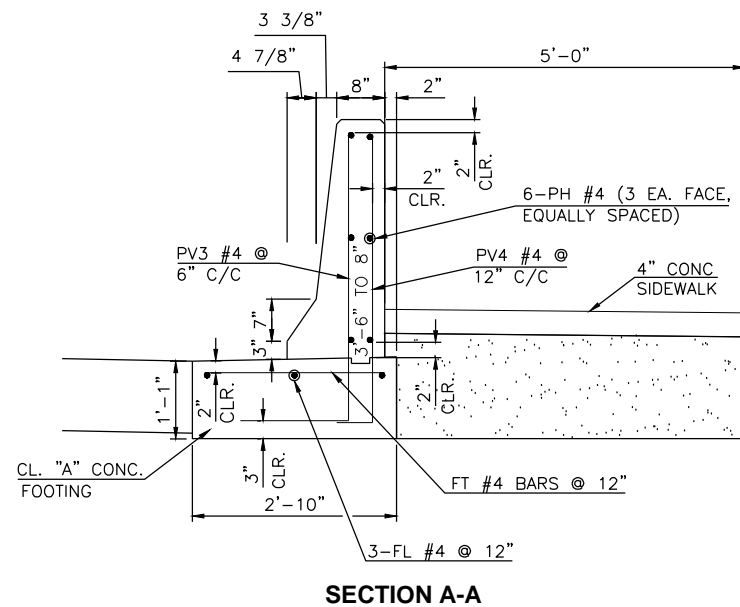
SPECIAL TAPERED TRAFFIC RAIL END SECTION IS PAID FOR AS LINEAR FOOT OF 42" PARAPET WALL. THE CONSTRUCTION OF THE 1 1/2" "V" GROOVE AND ALL LABOR, SAWING MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN PRICE BID FOR 42" PARAPET WALL. ALL REINFORCING STEEL TO BE GRADE 60. PAYMENT FOR THIS ITEM IS INCLUDED WITH BRIDGE PAY QUANTITIES.

GENERAL NOTES: PENETRATING WATER REPELLENT SURFACE TREATMENT SHALL BE APPLIED TO FRONT FACE AND TOP OF CONCRETE TRAFFIC RAILS. PAYMENT FOR THIS ITEM INCLUDED WITH BRIDGE PAY QUANTITIES. COST OF SAND CUSHION INCLUDED IN OTHER ITEMS.

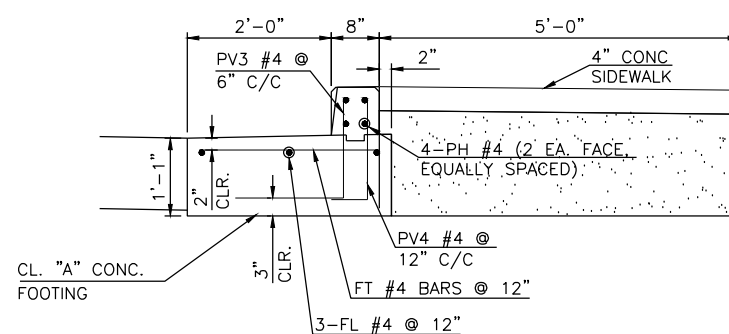
A CONTROL CRACK JOINT AS SHOWN BY THE DETAIL SHALL BE PLACED IN THE PARAPET @ 15' CTRS. (BEGINNING 5' MIN. AND 15' MAX. FROM ABUTMENT). PARAPETS WITH DRAIN OPENINGS SHALL HAVE A CONTROL CRACK JOINT SPACED IN THE CENTER OF THE SOLID PARAPET BETWEEN THE DRAIN OPENINGS.



ELEVATION
(TAPERED TRAFFIC RAIL ON FOOTING AND APPROACH SLAB)



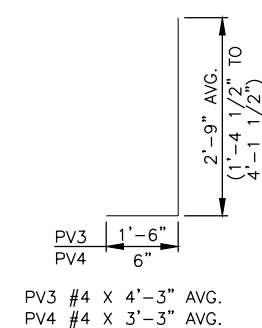
SECTION A-A



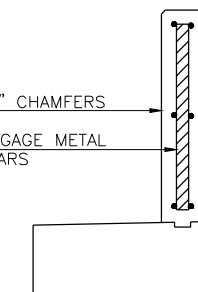
SECTION B-B

NOTE:
PH BARS SHALL BE CONTINUOUS THROUGH CONTROL CRACK JOINTS.

NOTE:
PV BARS ARE INCLUDED IN THE QUANTITY FOR REINFORCING STEEL GR. 60.



DOUBLE 3/4" CHAMFERS
GALV. LIGHT GAGE METAL INSIDE PH BARS



CONTROL CRACK JT.

BAR LIST				
PLAIN REINFORCING				
MARK	SIZE	NO.	SHAPE	LENGTH
FT	#4	204	STR	2'-4"
FL	#4	12	STR	49'-8"
PH1	#4	24	STR	42'-0"
PH2	#4	16	STR	12'-0"
PV3	#4	400	BNT	4'-3 1/2" AVG.
PV4	#4	204	BNT	3'-3 1/2" AVG.

MAIN ST. CUSTER COUNTY

SPECIAL TAPERED CONC. PARAPET END SECTION

JOB PIECE NO. 27911(06) SHEET NO. 85

GENERAL NOTES

SPECIFICATIONS –
 COMPLY WITH THE REQUIREMENTS OF THE 2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

VERIFICATION OF EXISTING CONDITIONS –
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR FULLY UNDERSTANDING THE NATURE OF THE WORK AND CONDITIONS UNDER WHICH THE WORK WILL BE PERFORMED.
 ALL DIMENSIONS OF THE EXISTING BRIDGE COMPONENTS SHOWN ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS NECESSARY TO CONNECT THE NEW MATERIAL AND SHALL BE SOLELY RESPONSIBLE FOR THE ACCURACY THEREOF.
 THE CONTRACTOR SHALL ADOPT METHODS CONSISTENT WITH GOOD CONSTRUCTION PRACTICE AND SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT DAMAGE TO THE EXISTING BRIDGE AND ATTACHMENTS. ANY DAMAGE TO THE EXISTING BRIDGE STRUCTURE OR ROADWAY DUE TO THE CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED, AT THE CONTRACTOR'S EXPENSE, TO THE SATISFACTION OF THE ENGINEER.

EXISTING PLANS –
 THE EXISTING STRUCTURE WAS ORIGINALLY CONSTRUCTED AS PART OF FAP NO. MG-2861(27) AND M-2861(15). PLANS OF THIS PROJECT ARE AVAILABLE FROM THE OKLAHOMA DEPARTMENT OF TRANSPORTATION TECHNOLOGY SERVICES PLANS SECTION, 200 N.E. 21ST STREET, OKLAHOMA CITY, OKLAHOMA, 73105.

SILT REMOVAL –
 THE CELLS OF THE EXISTING R.C.B. SHALL BE CLEANED OF ALL DEBRIS AND SILT DEPOSITS IN A MANNER APPROVED BY THE ENGINEER. ALL COSTS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

CHANNEL EXCAVATION –
 THE CONTRACTOR SHALL CLEAR THE CHANNELS DOWNSTREAM TO THE NEW RIGHT-OF-WAY AS DIRECTED BY THE ENGINEER. ALL COSTS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

CONCRETE –
 ALL EXPOSED CONCRETE EDGES SHALL HAVE A 1½" CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER. ALL CONCRETE SHALL BE PLACED IN THE DRY.

REINFORCING –
 UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, ALL REINFORCING STEEL SHALL CONFORM TO AASHTO M31 (ASTM A615), GRADE 60.
 ALL REINFORCING STEEL SHALL HAVE A 2" MINIMUM CLEAR COVER UNLESS OTHERWISE NOTED.

PAY ITEM NOTES

B1 PAYMENT TO THE CONTRACTOR WILL BE BASED ON PLAN QUANTITIES.

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
REVISIONS					
DESCRIPTION					DATE

J.P. NO. 27911(06) PAY QUANTITIES				
1 – 8' X 4' X 195'-7" RCB EXTENSION, VARYING SKEW				
ITEM NO.	ITEM	UNIT	TOTAL	
201(A) 0102	UNCLASSIFIED EXCAVATION	B1 C.Y.	1,175	
501(A) 0313	STRUCTURAL EXCAVATION UNCLASSIFIED	B1 C.Y.	85	
509(A) 0319	CLASS AA CONCRETE	B1 C.Y.	213.6	
511(A) 0332	REINFORCING STEEL	B1 LB.	31,990	

THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING

DESIGN	B.J.K.	MAIN ST.	CUSTER COUNTY
DRAWN	B.J.K.		
CHECKED	B.J.K.		
APPROV.	T.A.C.		
SQUAD	CEC		
STR. NO. 7 GENERAL NOTES AND PAY ITEMS			JOB PIECE NO. <u>27911(06)</u> SHEET NO. <u>86</u>

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS

REVISIONS		DATE
DESCRIPTION		

DESIGN DATA

CLASS AA CONCRETE $f'c = 4$ K.S.I.
 REINFORCING STEEL (GRADE 60) $F_y = 60$ K.S.I.

LOADING:
 HL-93 OR OKLAHOMA OVERLOAD TRUCK

DESIGN:
 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 7TH EDITION.

INDEX OF STR. NO. 7 SHEETS

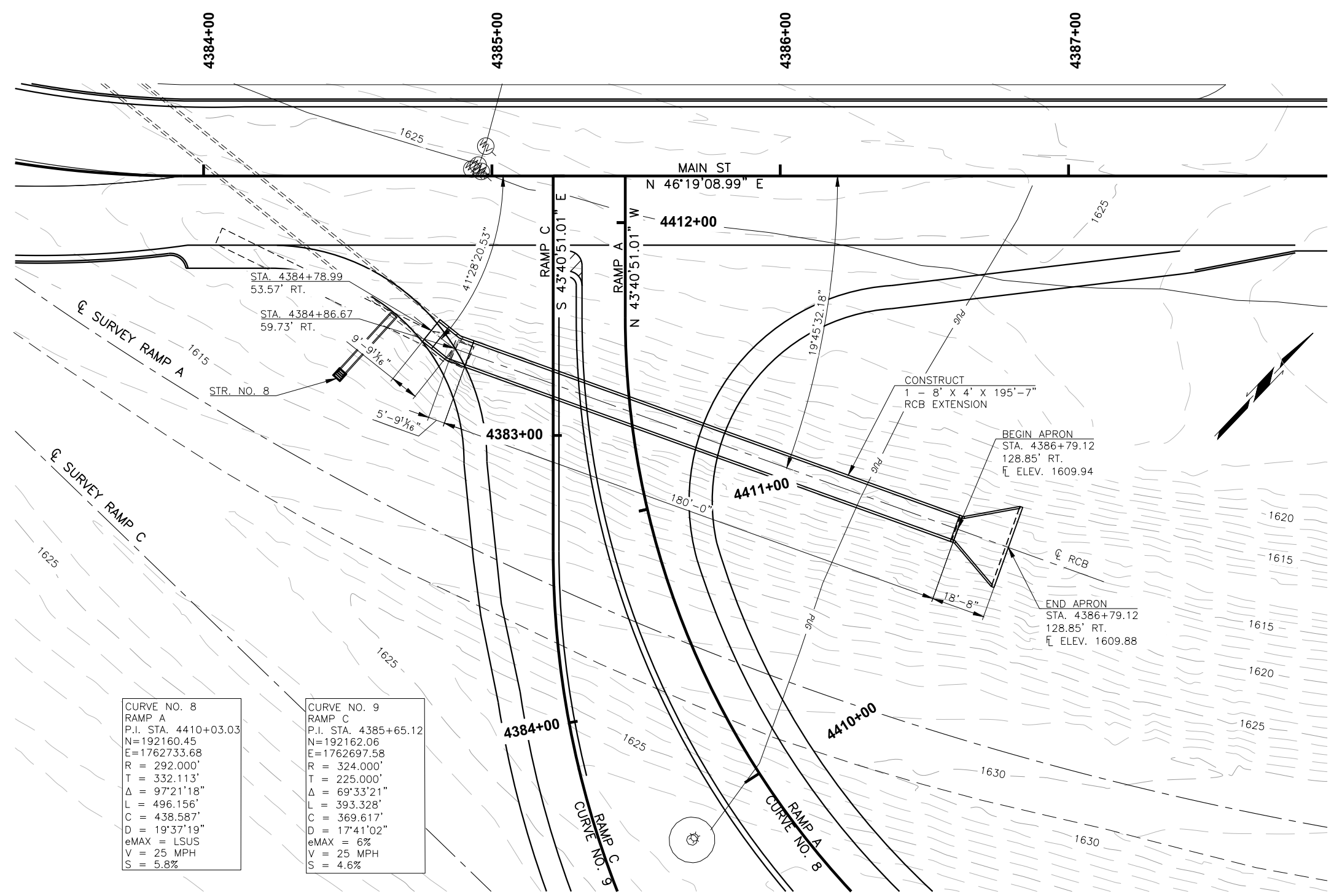
- 84. STR. NO. 7 GENERAL NOTES AND PAY ITEMS
- 87. GENERAL PLAN AND ELEVATION
- 88. RCB DETAILS

BRIDGE STANDARDS

- RCB-C1-8(14-20)-01E
- RCB-E1-H4-0-1-01E
- RCB-E1-H4-0-2-01E
- RCB-CW1-D4-0-01E

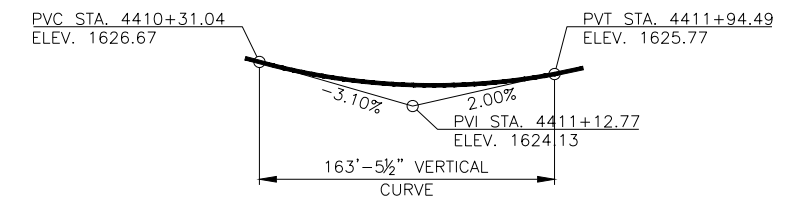
ROADWAY STANDARDS

- SBI-4-2

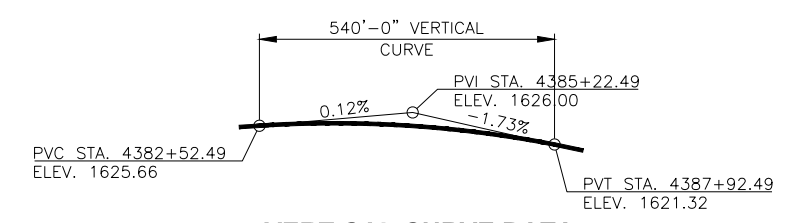


CURVE NO. 8
 RAMP A
 P.I. STA. 4410+03.03
 N=192160.45
 E=1762733.68
 R = 292.000'
 T = 332.113'
 $\Delta = 97^{\circ}21'18''$
 L = 496.156'
 C = 438.587'
 D = 19'37'19"
 eMAX = LSUS
 V = 25 MPH
 S = 5.8%

CURVE NO. 9
 RAMP C
 P.I. STA. 4385+65.12
 N=192162.06
 E=1762697.58
 R = 324.000'
 T = 225.000'
 $\Delta = 69^{\circ}33'21''$
 L = 393.328'
 C = 369.617'
 D = 17'41'02"
 eMAX = 6%
 V = 25 MPH
 S = 4.6%



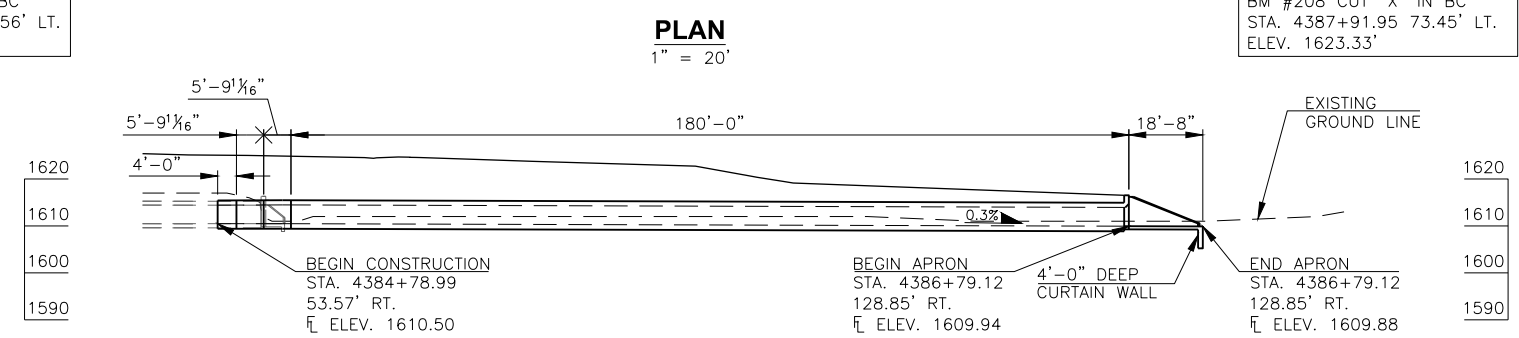
VERTICAL CURVE DATA
 P.G.L. RAMP A



VERTICAL CURVE DATA
 P.G.L. RAMP C

BM #207 CUT "X" IN BC
 STA. 4382+84.13 321.56' LT.
 ELEV. 1624.45'

BM #208 CUT "X" IN BC
 STA. 4387+91.95 73.45' LT.
 ELEV. 1623.33'



PLAN
 1" = 20'

ELEVATION
 1" = 20'

SUMMARY OF PAY QUANTITIES

ITEM	UNIT	RCB BARREL BEND	STANDARD RCB BARREL	END SECTION	CURTAIN WALL	TOTAL
UNCLASSIFIED EXCAVATION	C.Y.	115	1,060			1,175
STRUCTURAL EXCAVATION UNCLASSIFIED	C.Y.	5	80			85
CLASS AA CONCRETE	C.Y.	11.5	182.2	15.5	4.4	213.6
REINFORCING STEEL	L.B.	2,280	26,830	2,330	550	31,990

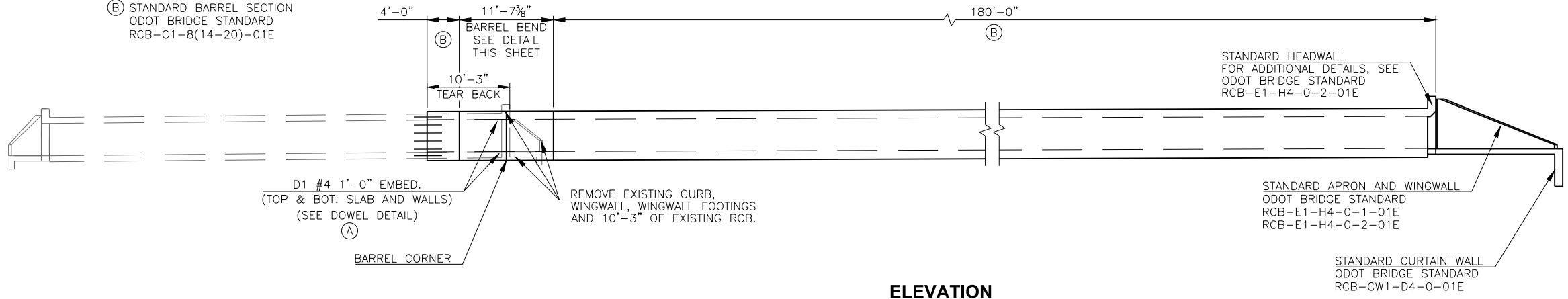
THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING

DESIGN	B.J.K.	MAIN ST. BRIDGE A	CUSTER COUNTY
DRAWN	R.A.P.	GENERAL PLAN AND ELEVATION 1 - 8' X 4' X 195'-7" RCB EXTENSION, VARYING SKEW	
CHECKED	B.J.K.		
APPROV.	T.A.C.		
SQUAD	CEC		
		JOB PIECE NO. 27911(06)	SHEET NO. 87

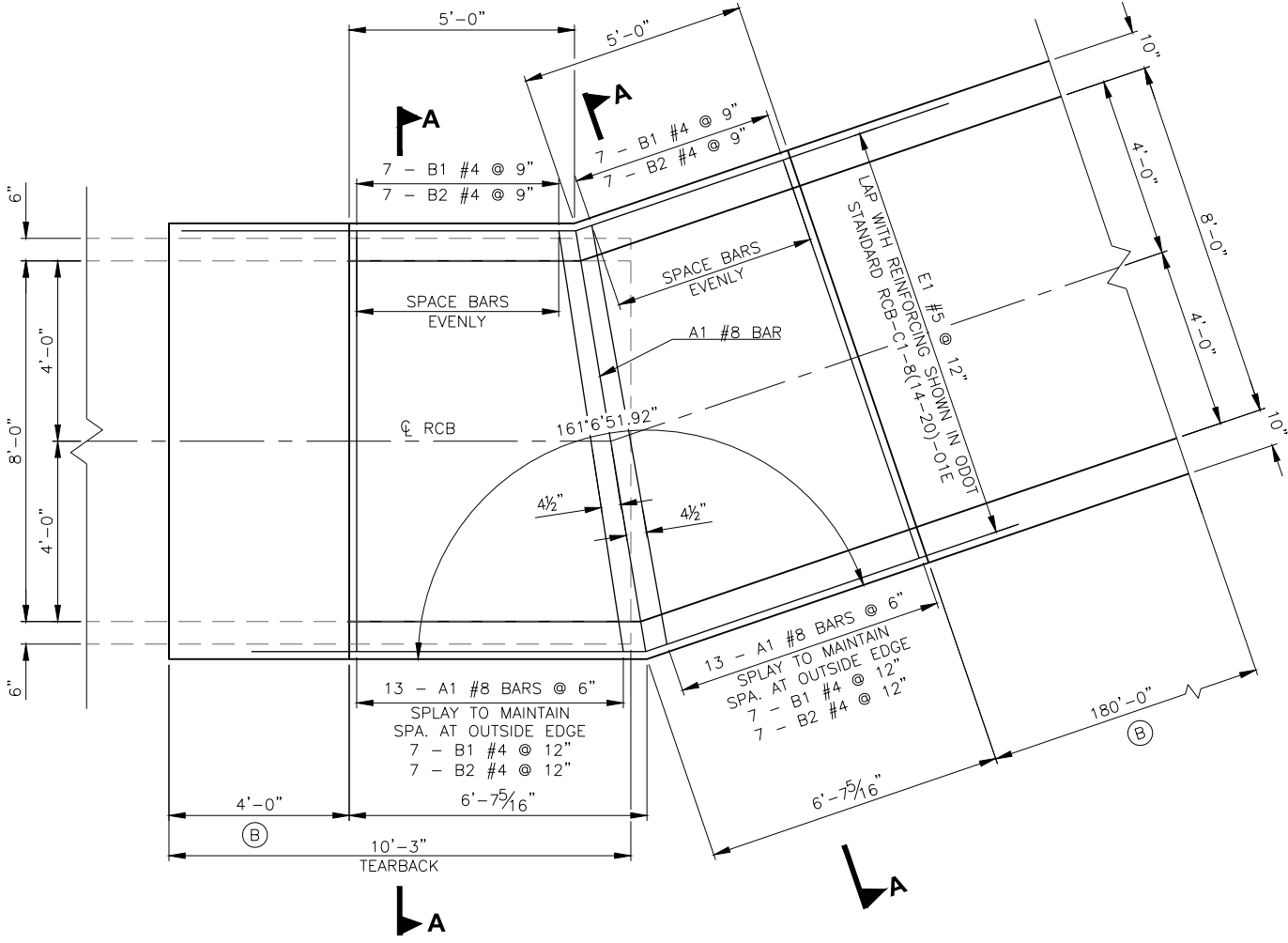
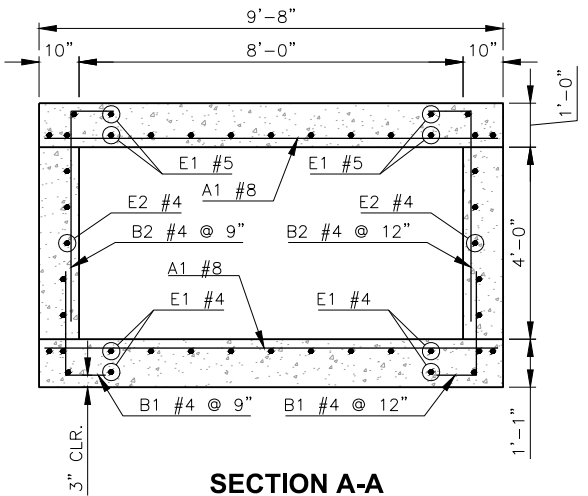
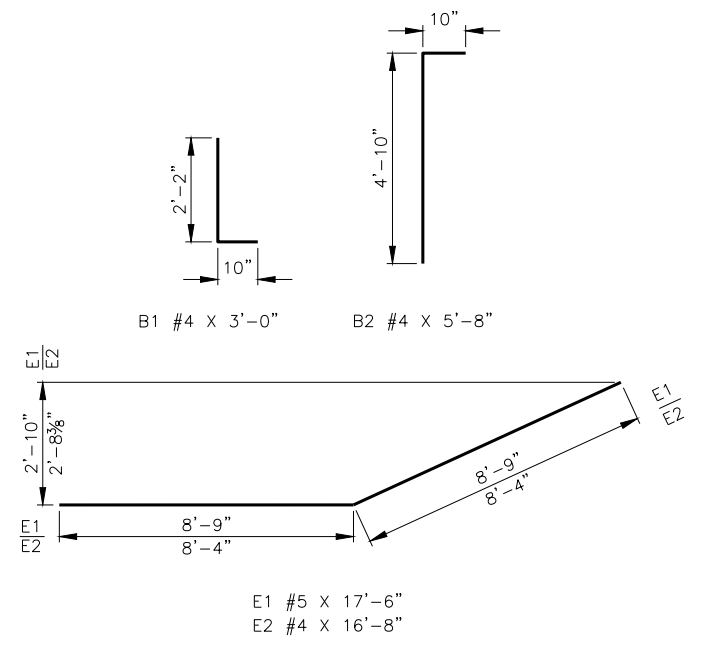
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
REVISIONS					DATE
DESCRIPTION					

- (A) DRILL AND EPOXY REBAR DOWELS, D1 INTO EXISTING BARREL OF RCB ACCORDING TO SECTION 509.04(D) OF THE STANDARD SPECIFICATIONS. ALL COST OF INSTALLING DOWELS INCLUDING LABOR, MATERIALS AND INCIDENTALS NECESSARY TO DO THE WORK, SHALL BE INCLUDED IN OTHER ITEMS OF WORK.
- (B) STANDARD BARREL SECTION ODOT BRIDGE STANDARD RCB-C1-8(14-20)-01E

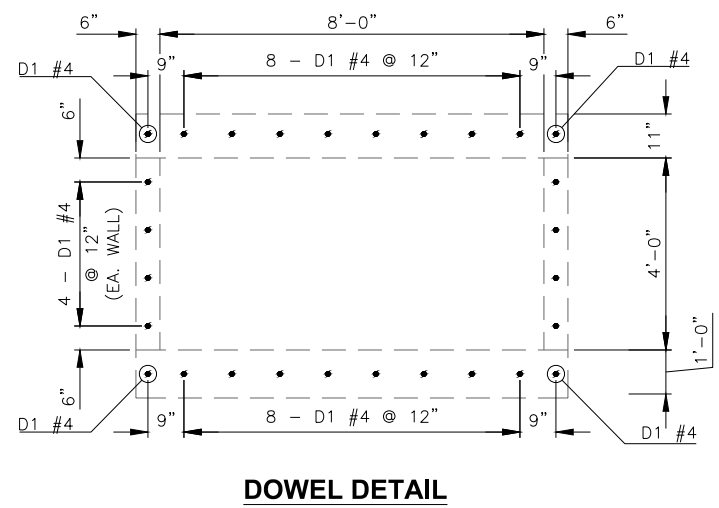
NOTE:
ALL REMOVED MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR. INCLUDE ALL COSTS FOR LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK IN THE CONTRACT UNIT PRICE OF "REMOVAL OF STRUCTURES AND OBSTRUCTIONS."



BARREL BEND BAR LIST				
REINFORCING STEEL				
MARK	SIZE	NO.	FORM	LENGTH
A1	#8	54	STR.	9'-4"
B1	#4	28	BNT.	3'-0"
B2	#4	28	BNT.	5'-8"
D1	#4	28	STR.	2'-0"
E1	#5	34	BNT.	17'-6"
E2	#4	10	BNT.	16'-8"



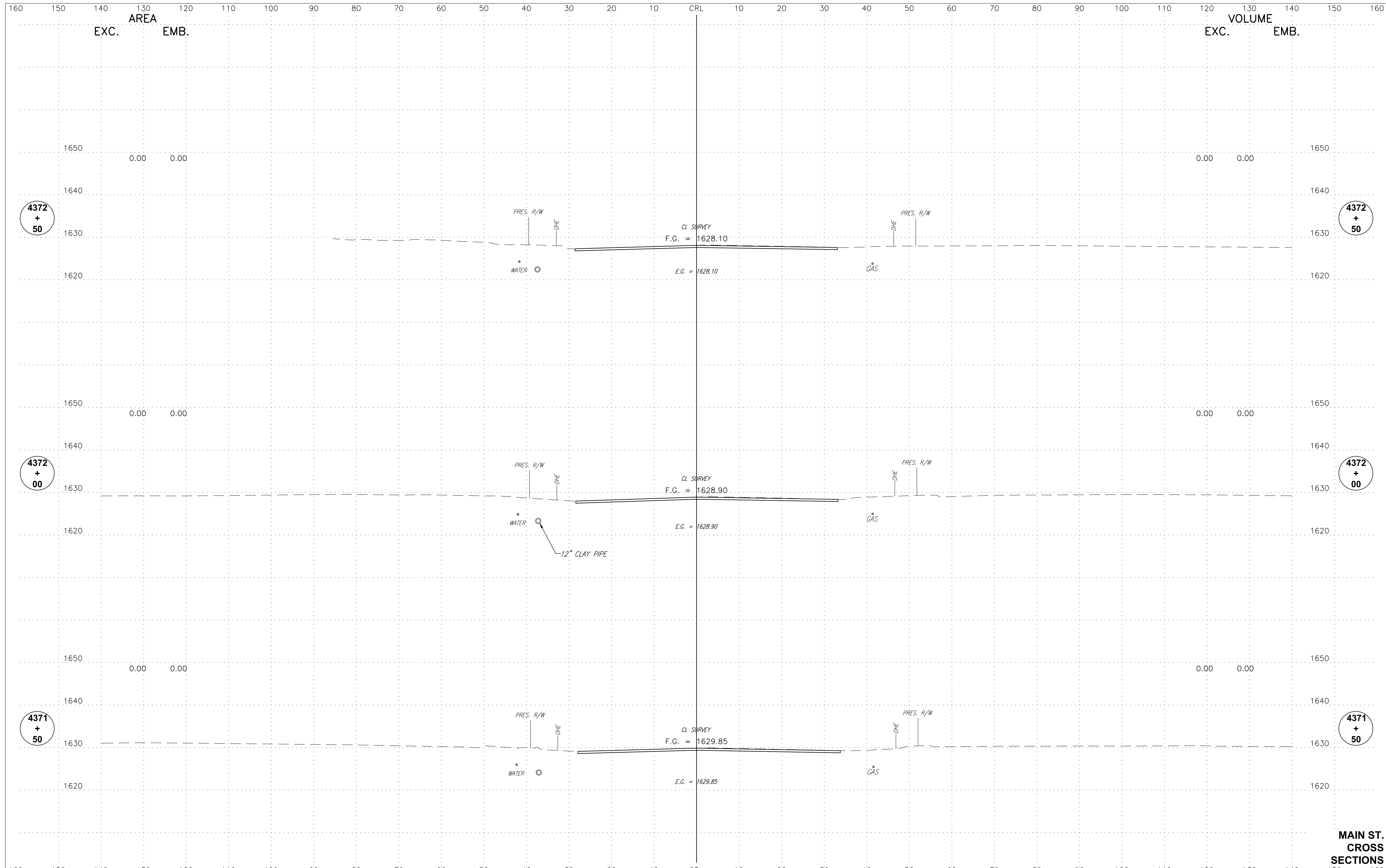
BARREL BEND QUANTITIES		
ITEM	UNIT	TOTAL
UNCLASSIFIED EXCAVATION	C.Y.	115
STRUCTURAL EXCAVATION UNCLASSIFIED	C.Y.	10
CLASS AA CONCRETE	C.Y.	11.5
REINFORCING STEEL	LB.	2,280



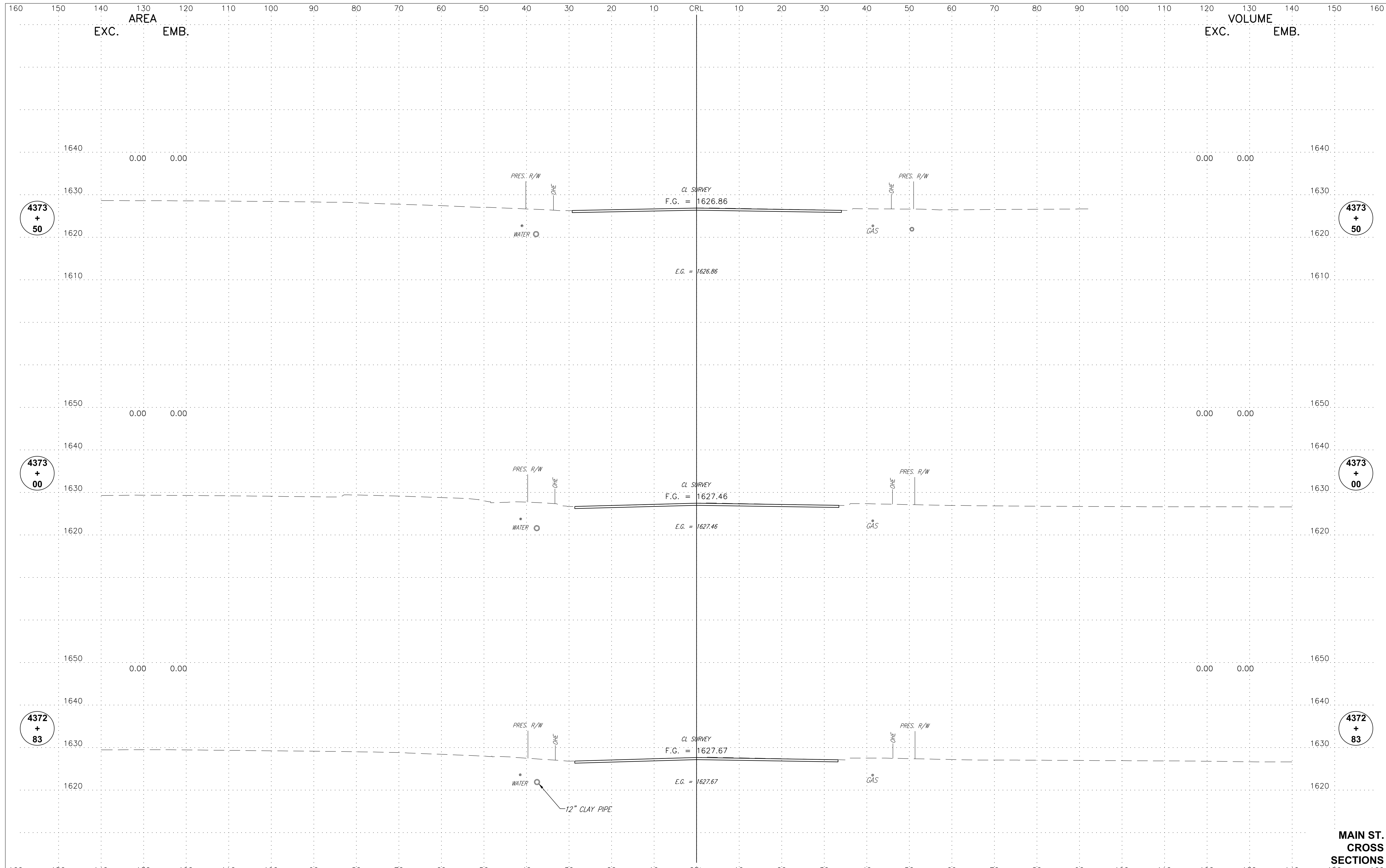
THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING

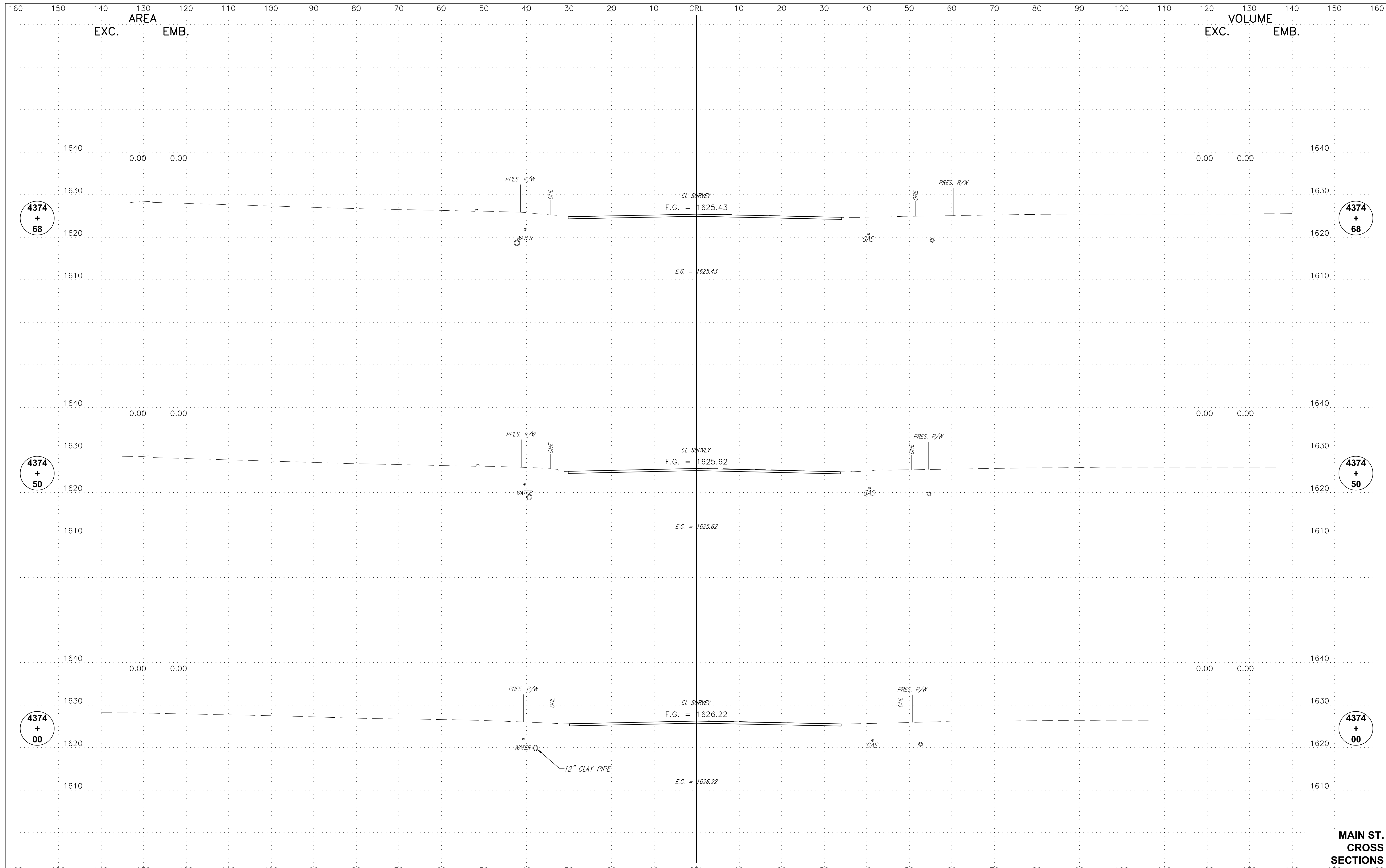
DESIGN	B.J.K.	MAIN ST.	CUSTER COUNTY
DRAWN	J.F.R. K.E.B.		
CHECKED	B.J.K.		
APPROV.	T.A.C.		
SQUAD	CEC		

RCB DETAILS



**MAIN ST.
CROSS
SECTIONS**

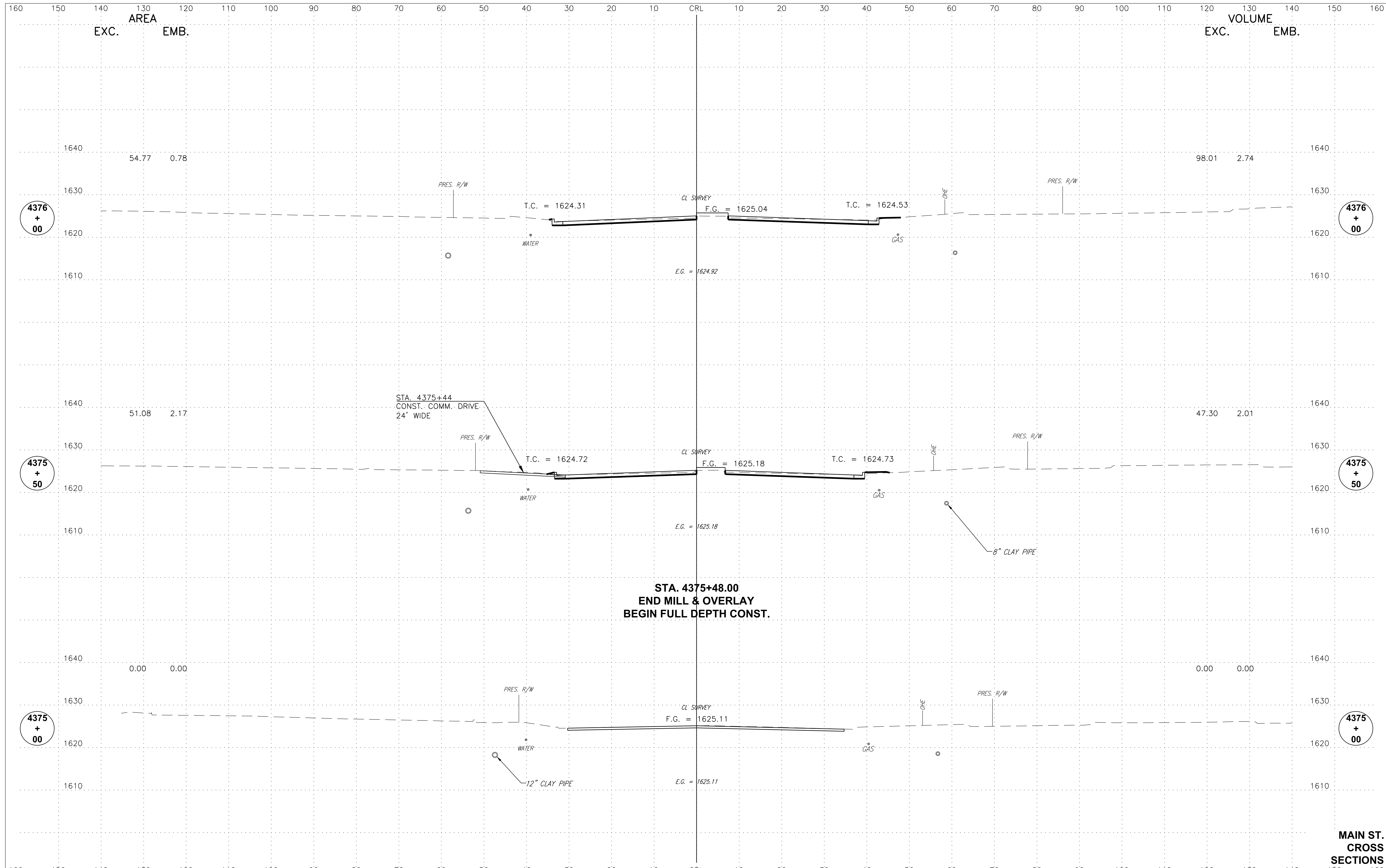


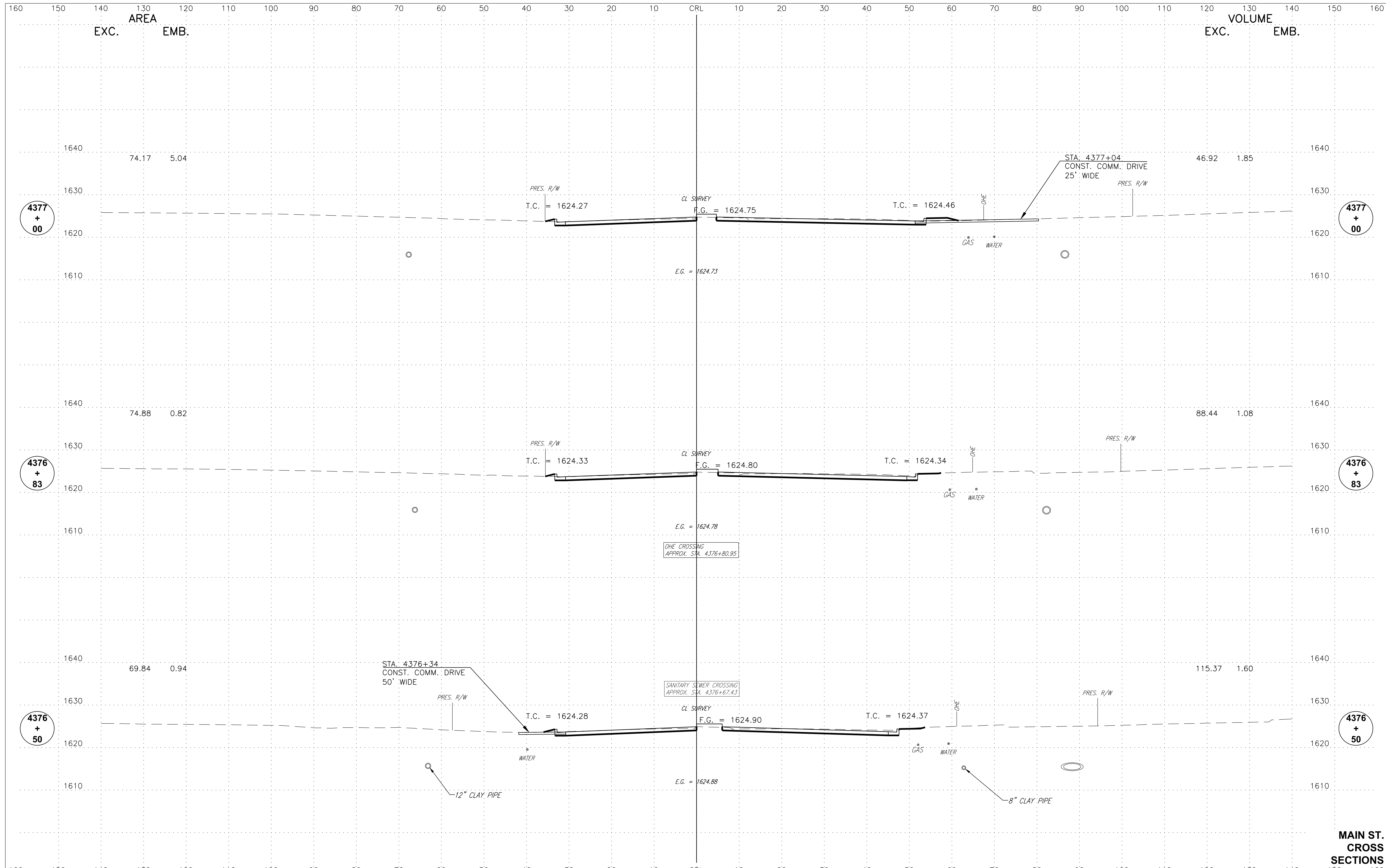


AREA
EXC. EMB.

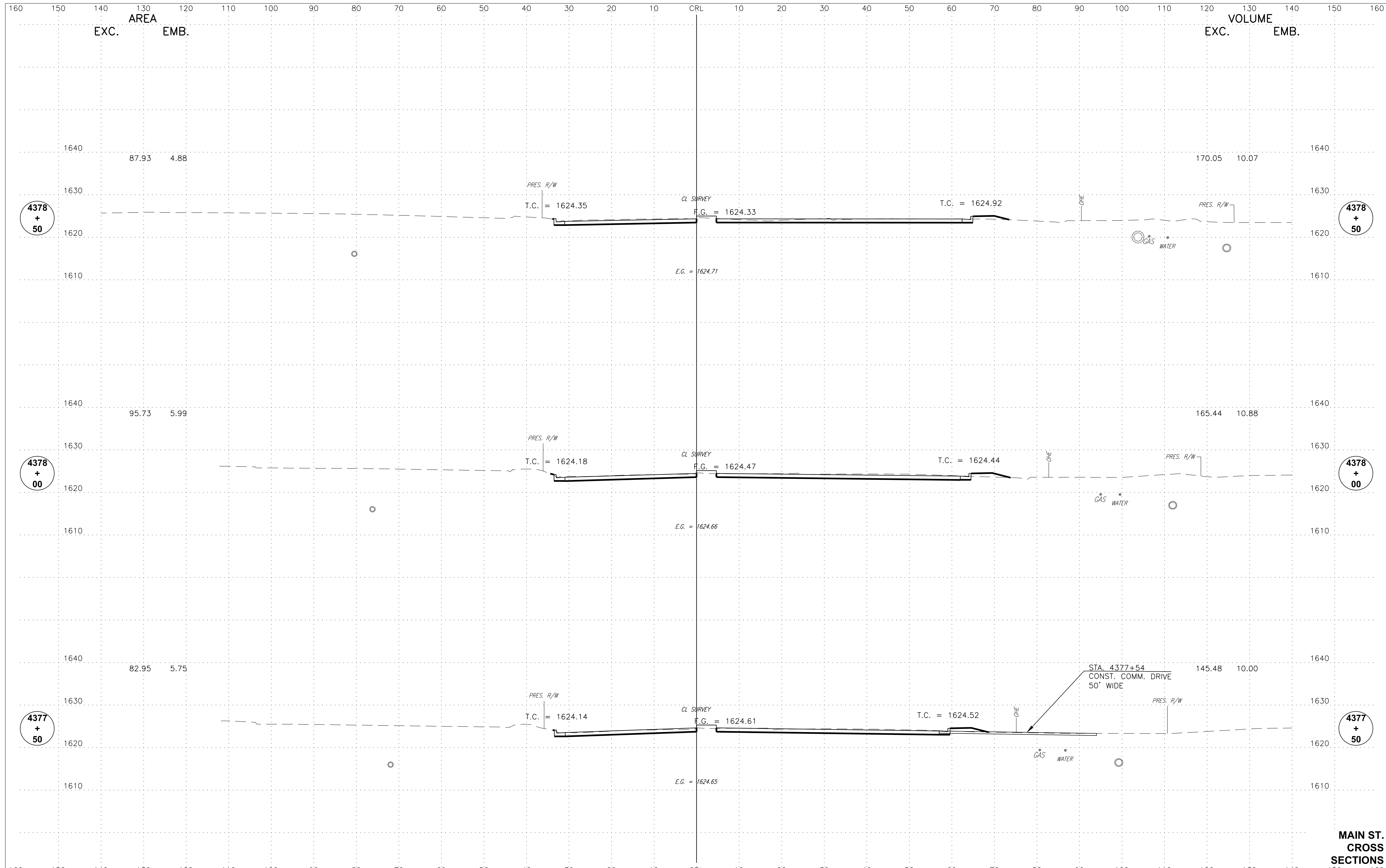
VOLUME
EXC. EMB.

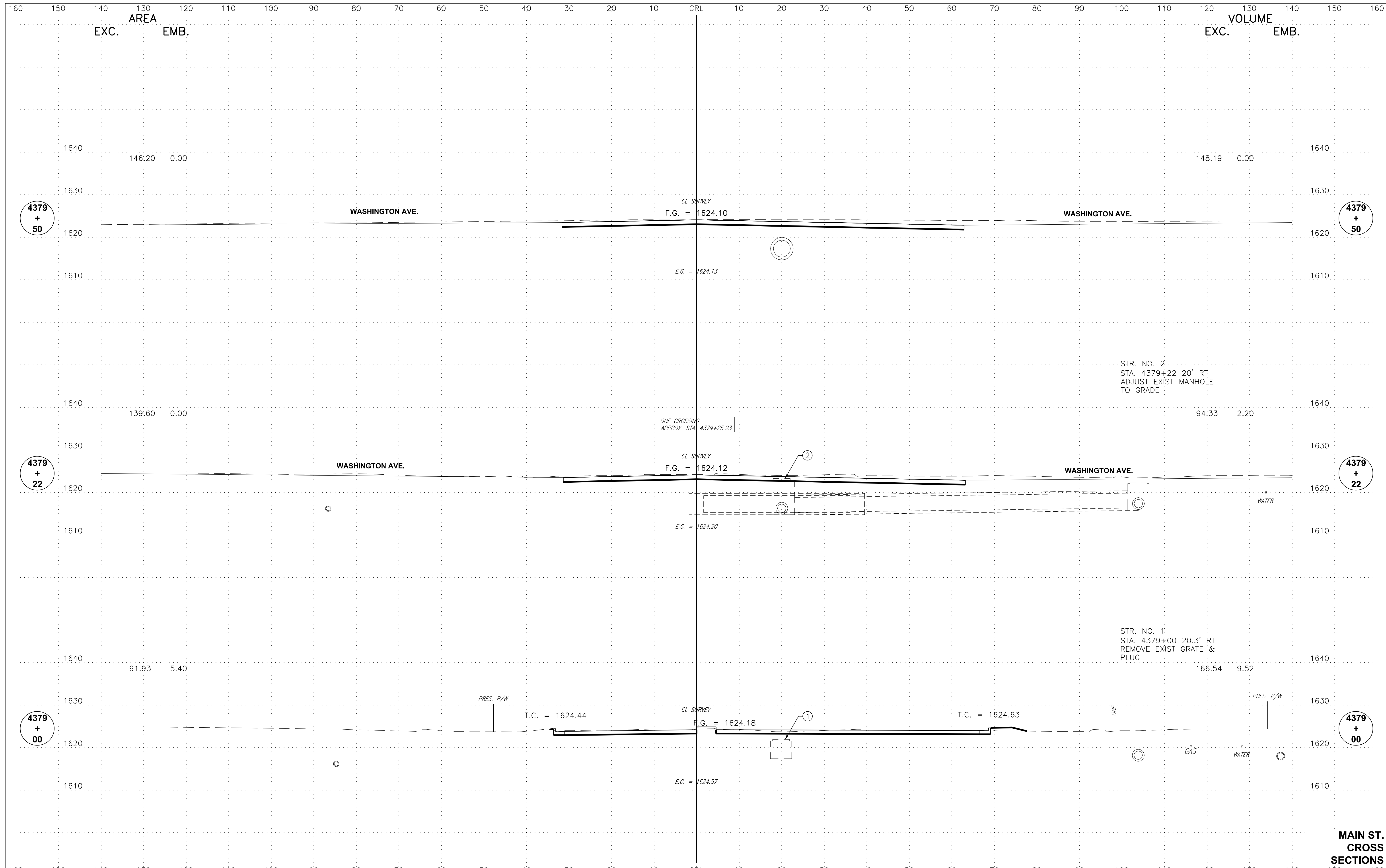
MAIN ST.
CROSS
SECTIONS

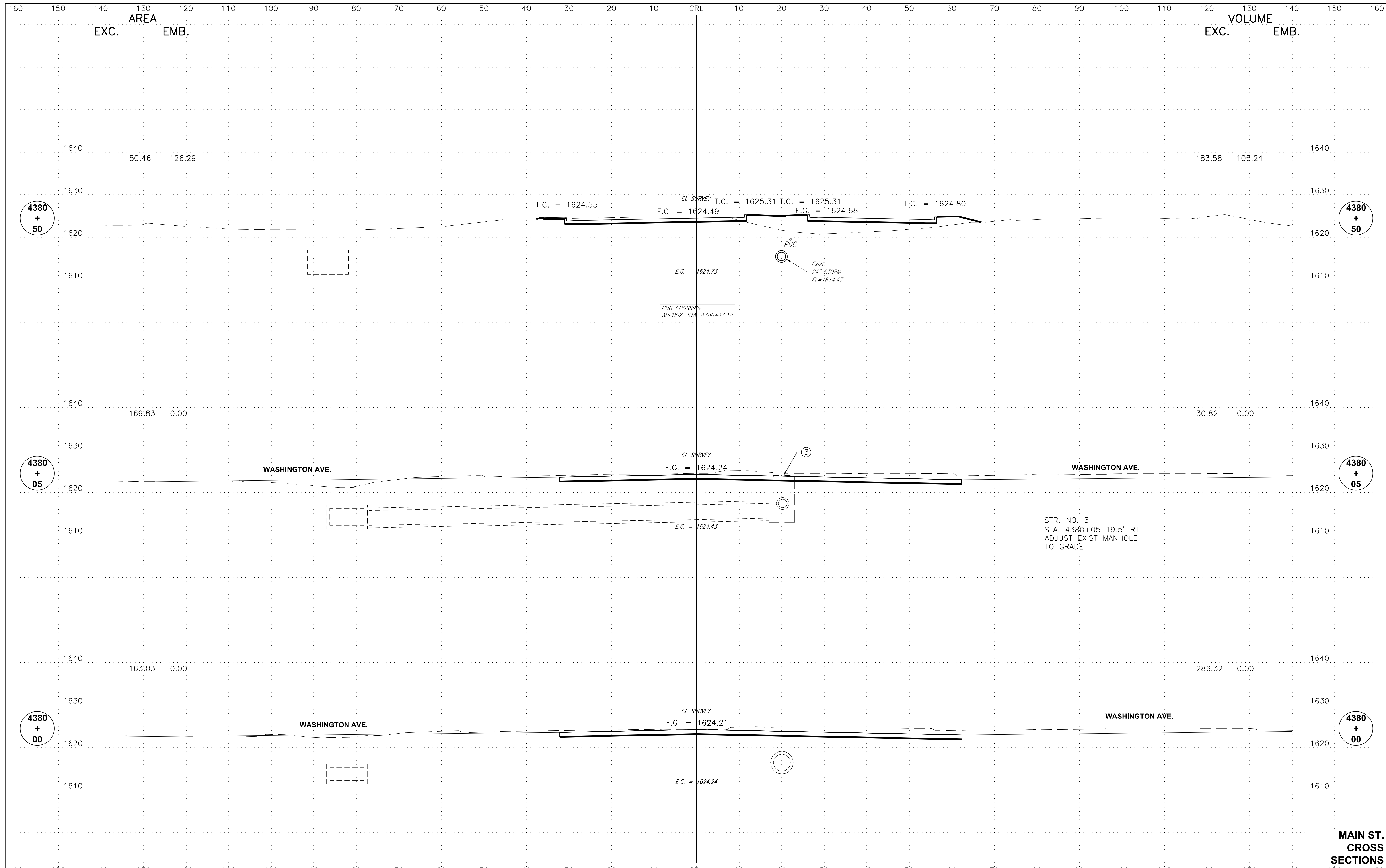




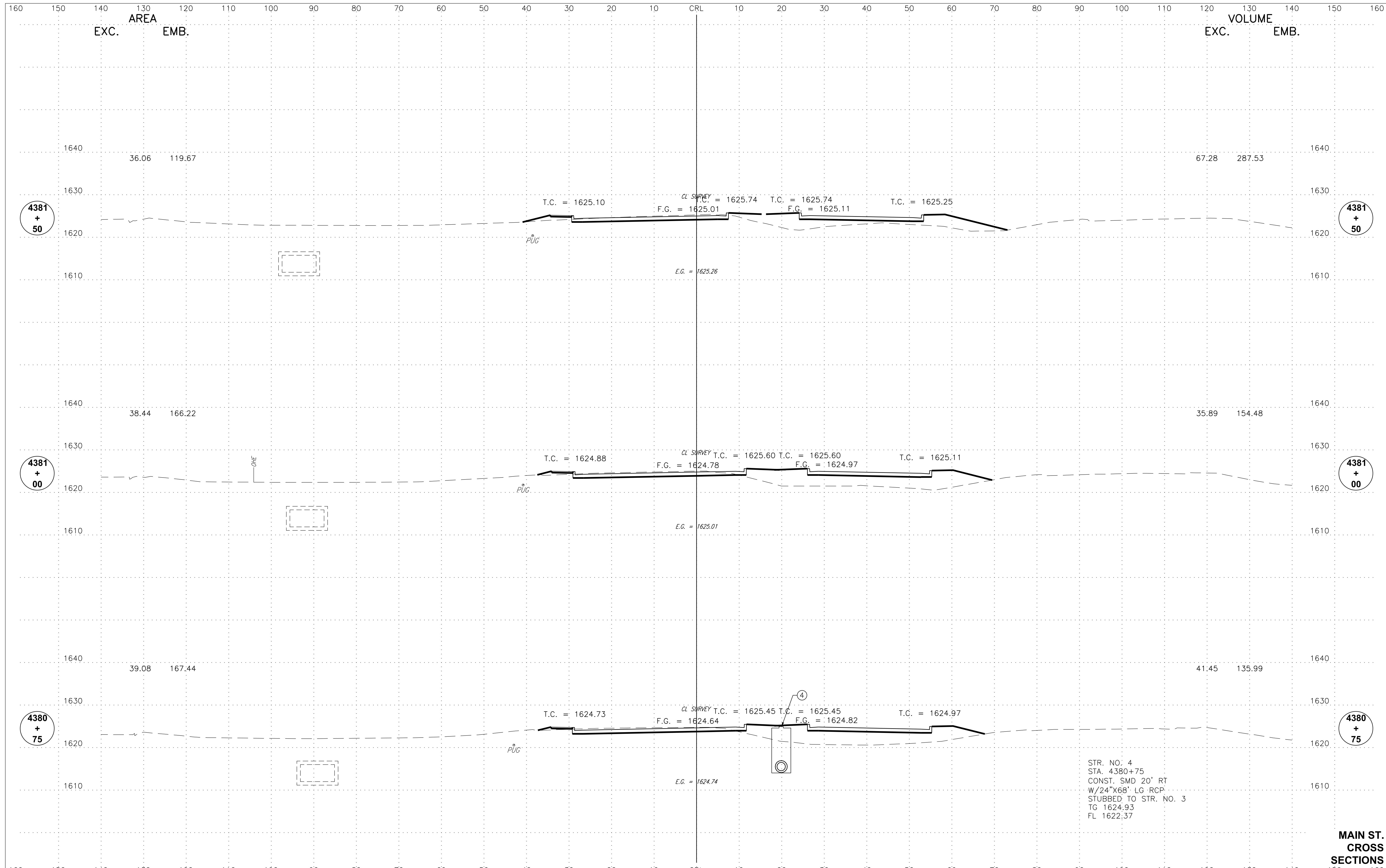
**MAIN ST.
CROSS
SECTIONS**





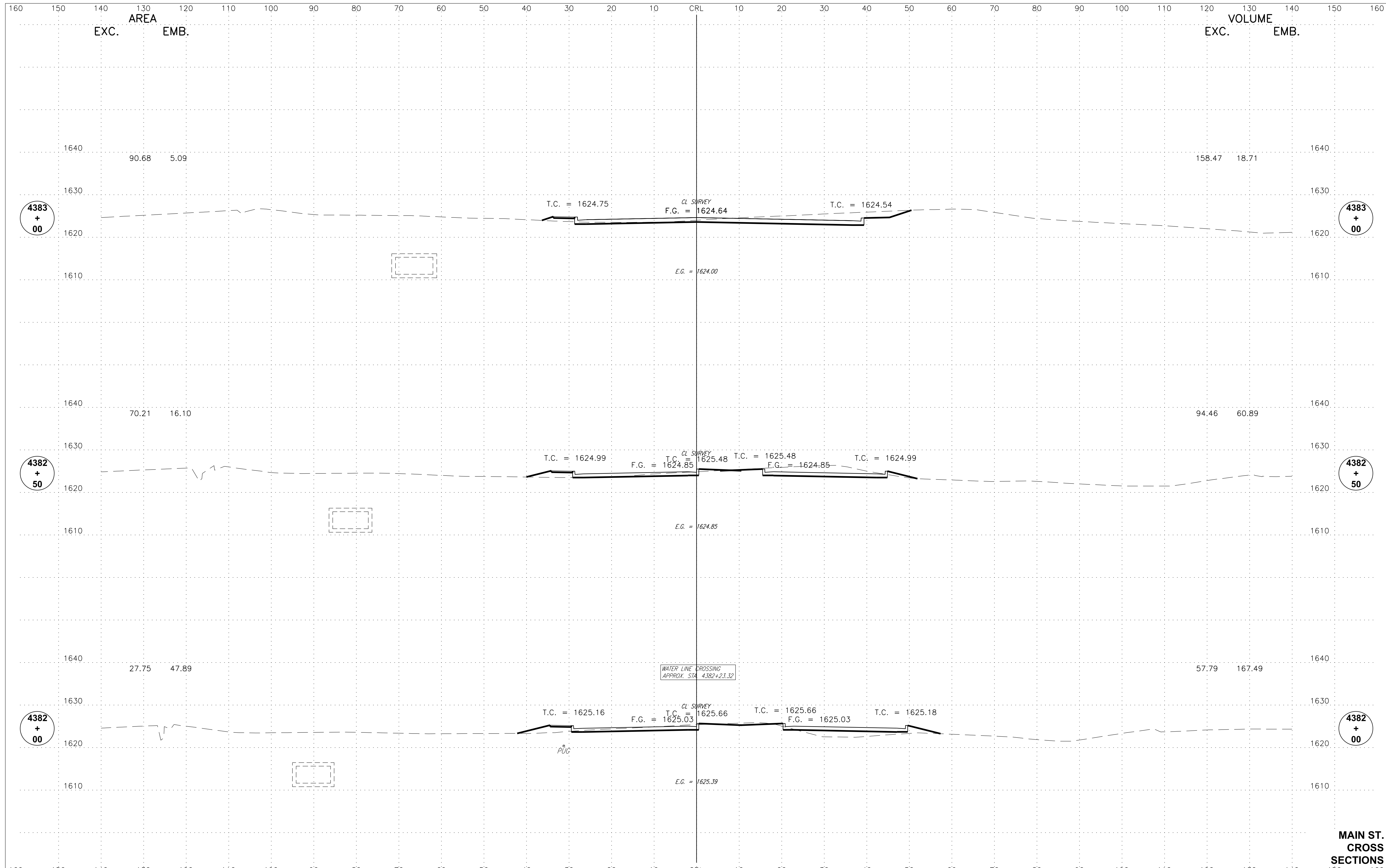


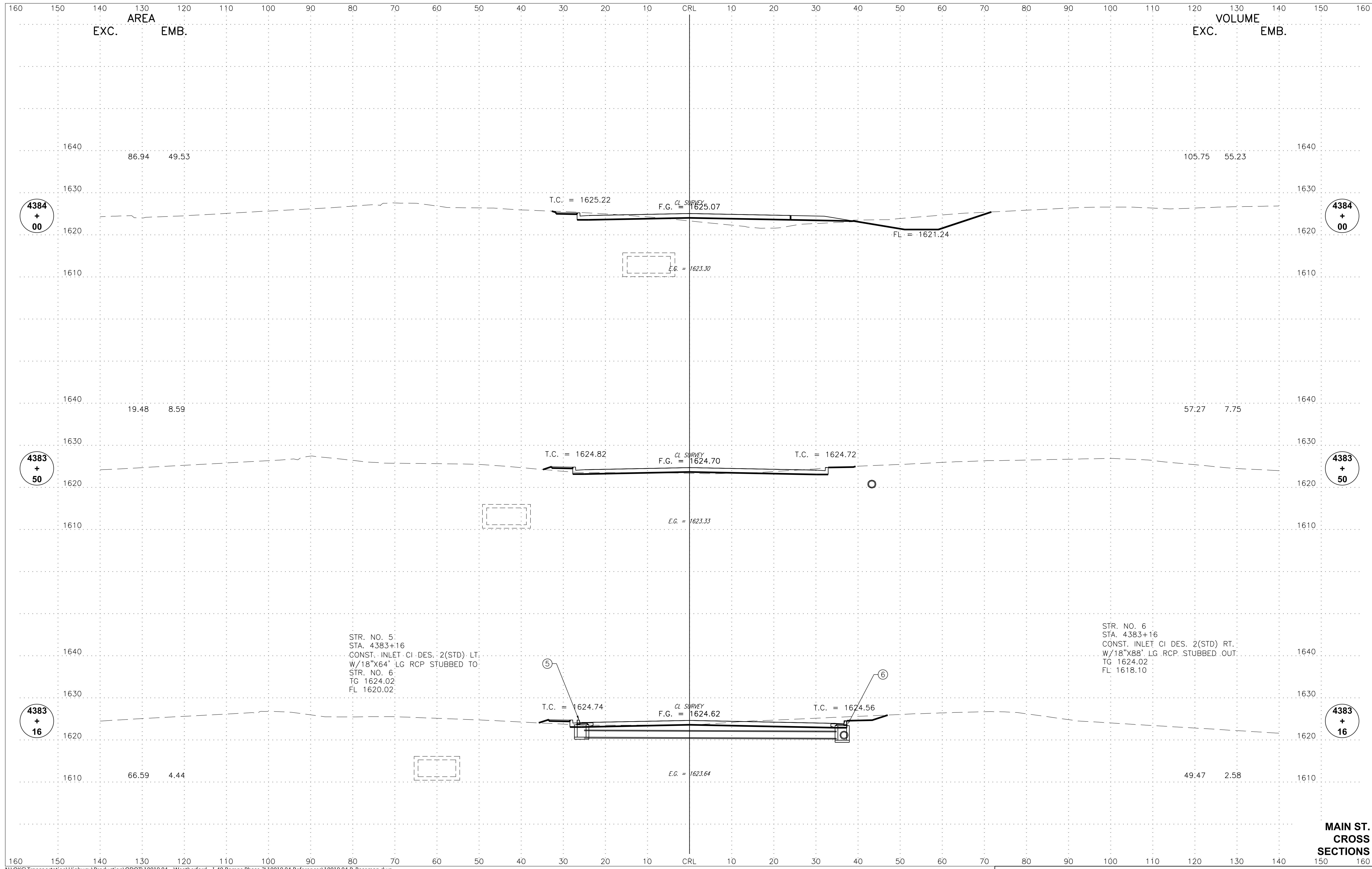
**MAIN ST.
CROSS
SECTIONS**



STR. NO. 4
 STA. 4380+75
 CONST. SMD 20' RT
 W/24"x68" LG RCP
 STUBBED TO STR. NO. 3
 TG 1624.93
 FL 1622.37

**MAIN ST.
 CROSS
 SECTIONS**

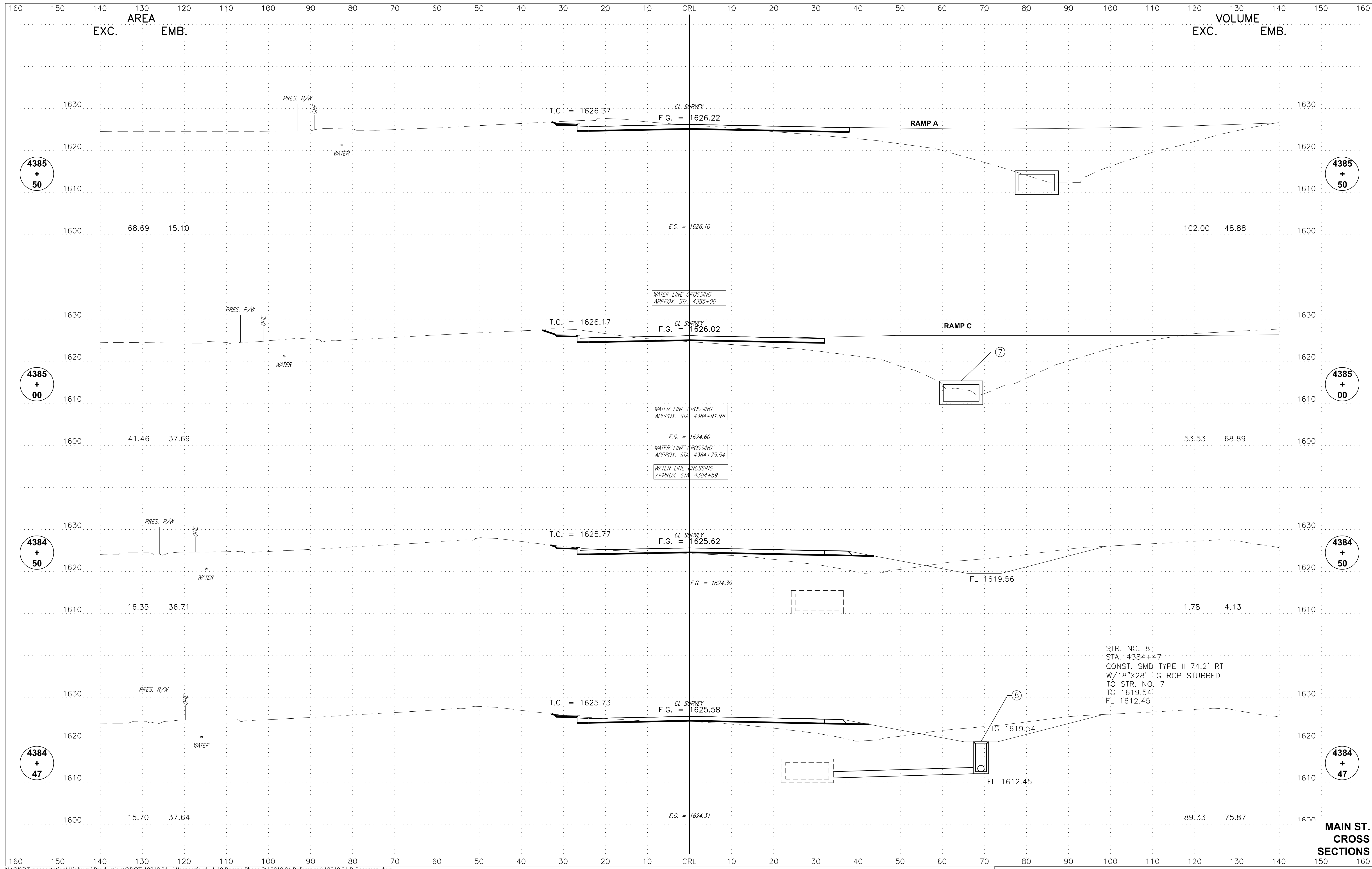




STR. NO. 5
 STA. 4383+16
 CONST. INLET CI DES. 2(STD) LT.
 W/18"x64" LG RCP STUBBED TO
 STR. NO. 6
 TG 1624.02
 FL 1620.02

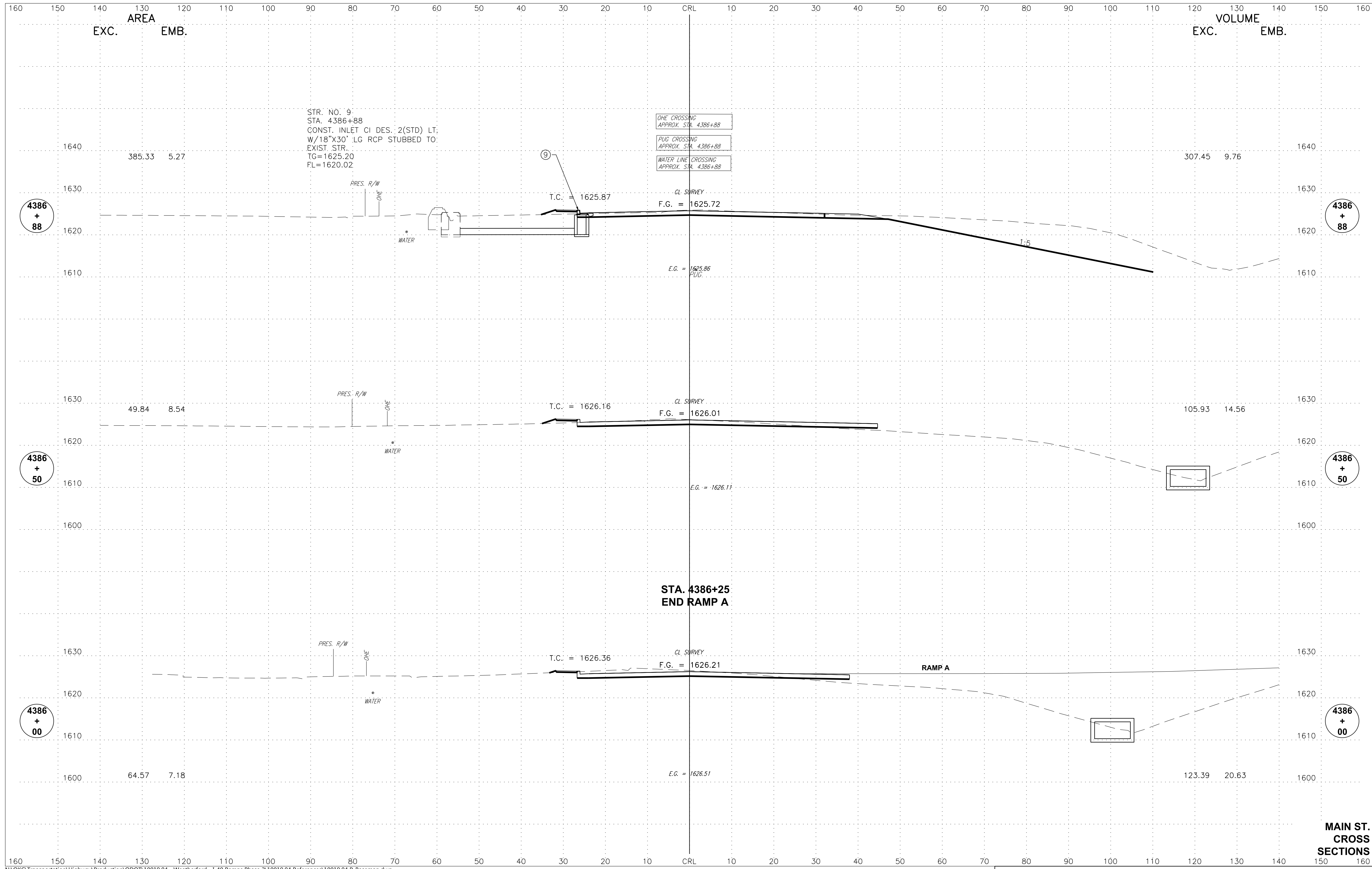
STR. NO. 6
 STA. 4383+16
 CONST. INLET CI DES. 2(STD) RT.
 W/18"x88" LG RCP STUBBED OUT
 TG 1624.02
 FL 1618.10

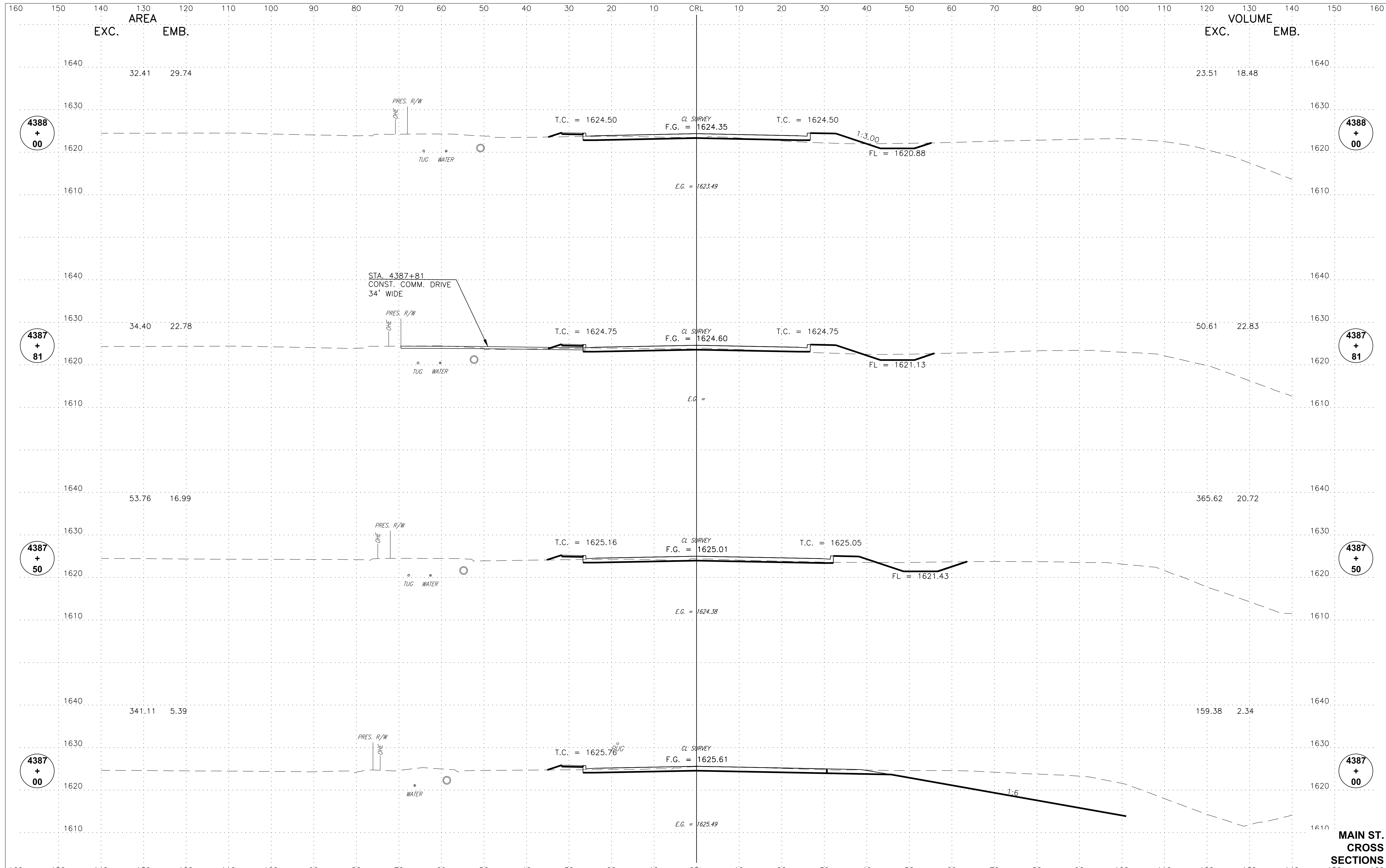
**MAIN ST.
 CROSS
 SECTIONS**



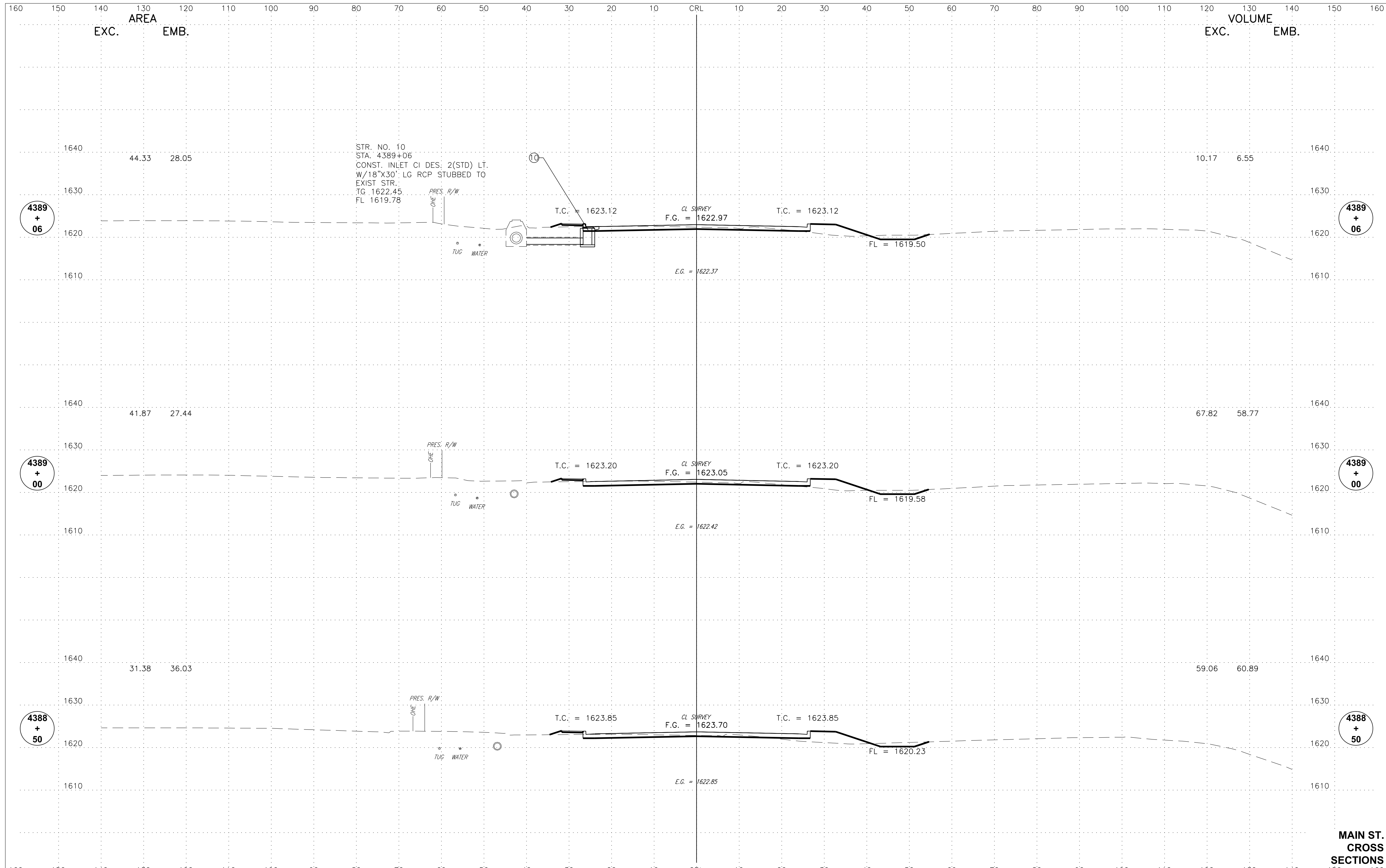
STR. NO. 8
 STA: 4384+47
 CONST. SMD TYPE II 74.2' RT
 W/18"X28' LG RCP STUBBED
 TO STR. NO. 7
 TG 1619.54
 FL 1612.45

**MAIN ST.
 CROSS
 SECTIONS**

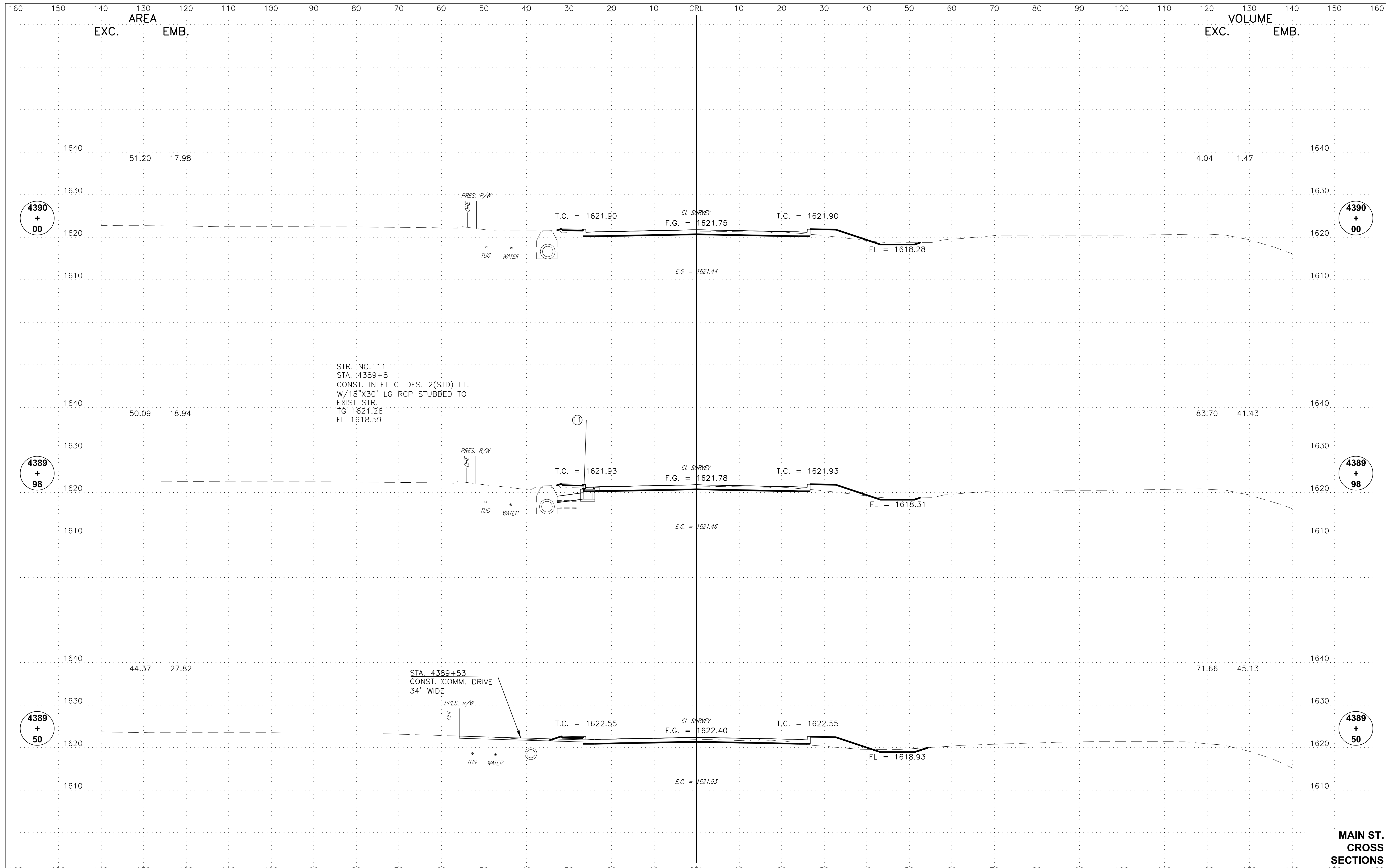




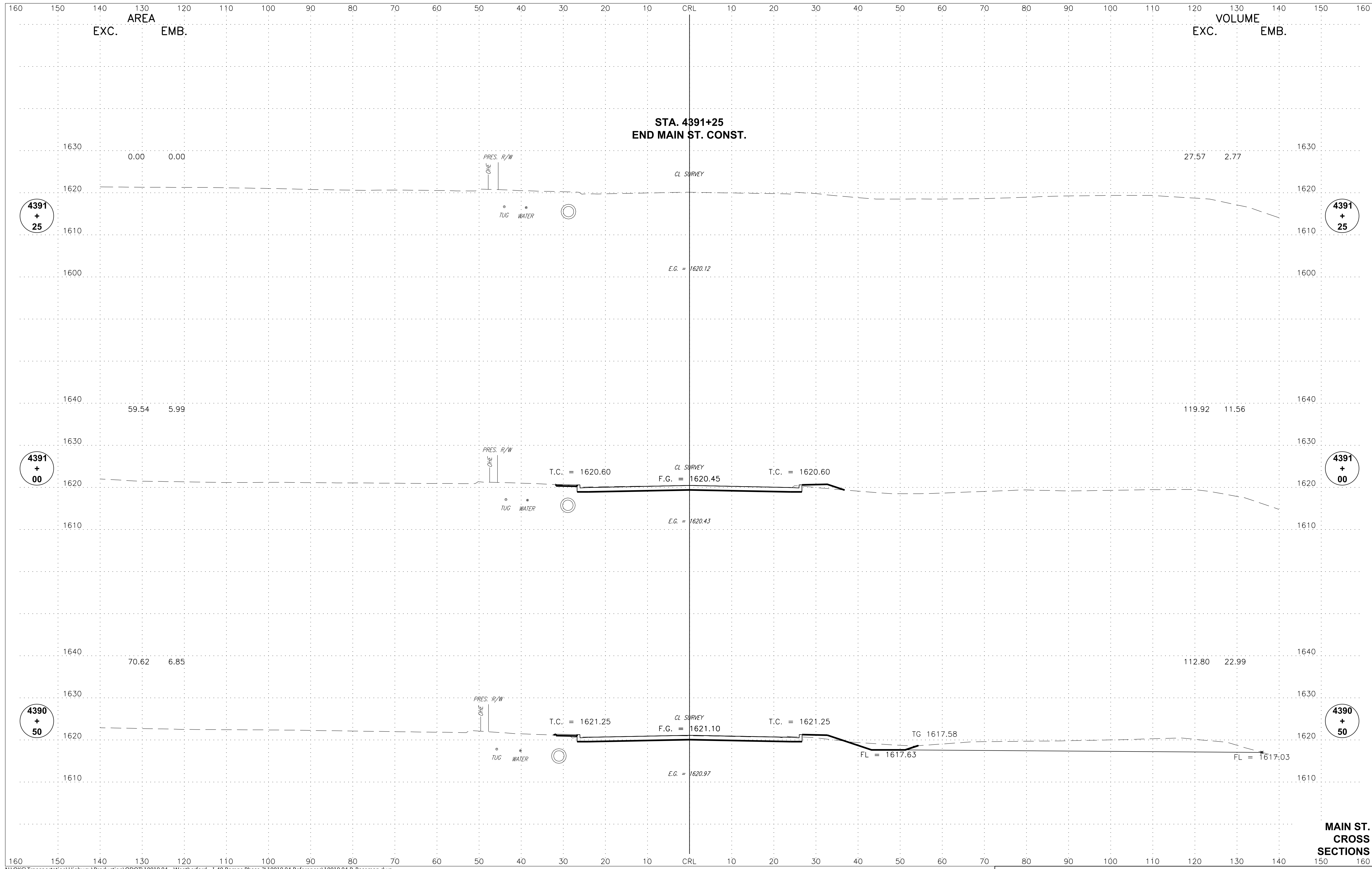
**MAIN ST.
CROSS
SECTIONS**



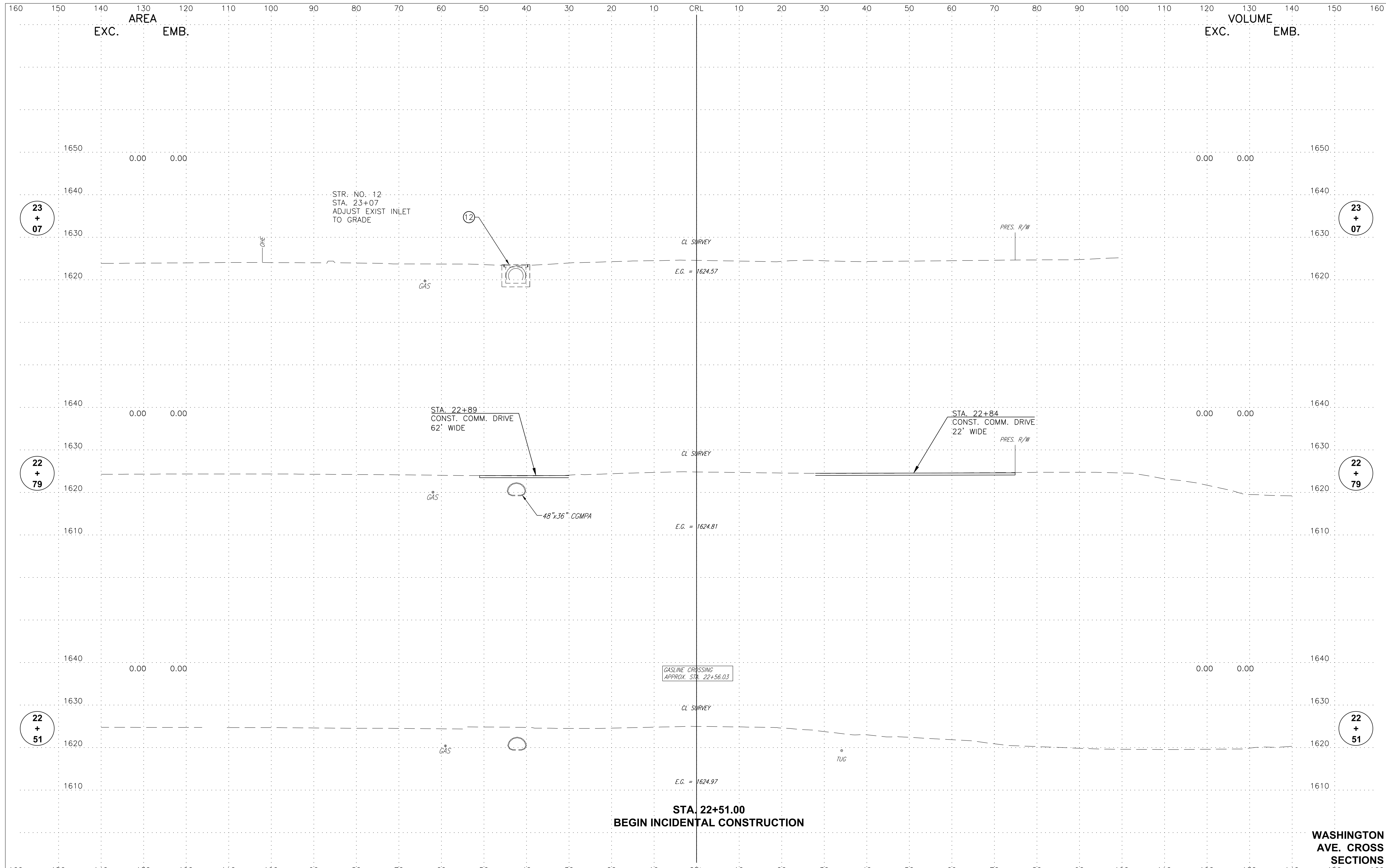
**MAIN ST.
CROSS
SECTIONS**



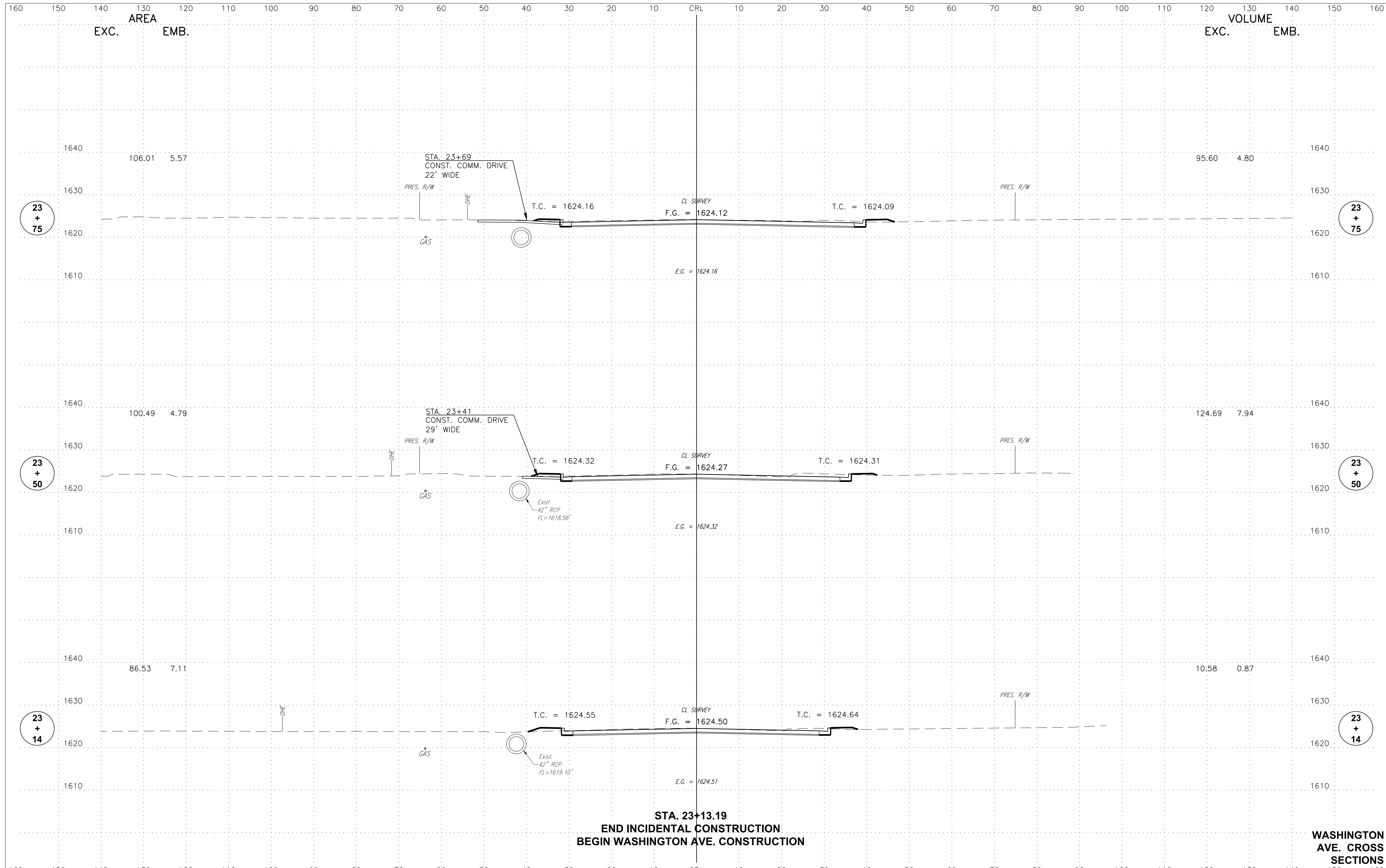
**MAIN ST.
CROSS
SECTIONS**



**MAIN ST.
CROSS
SECTIONS**

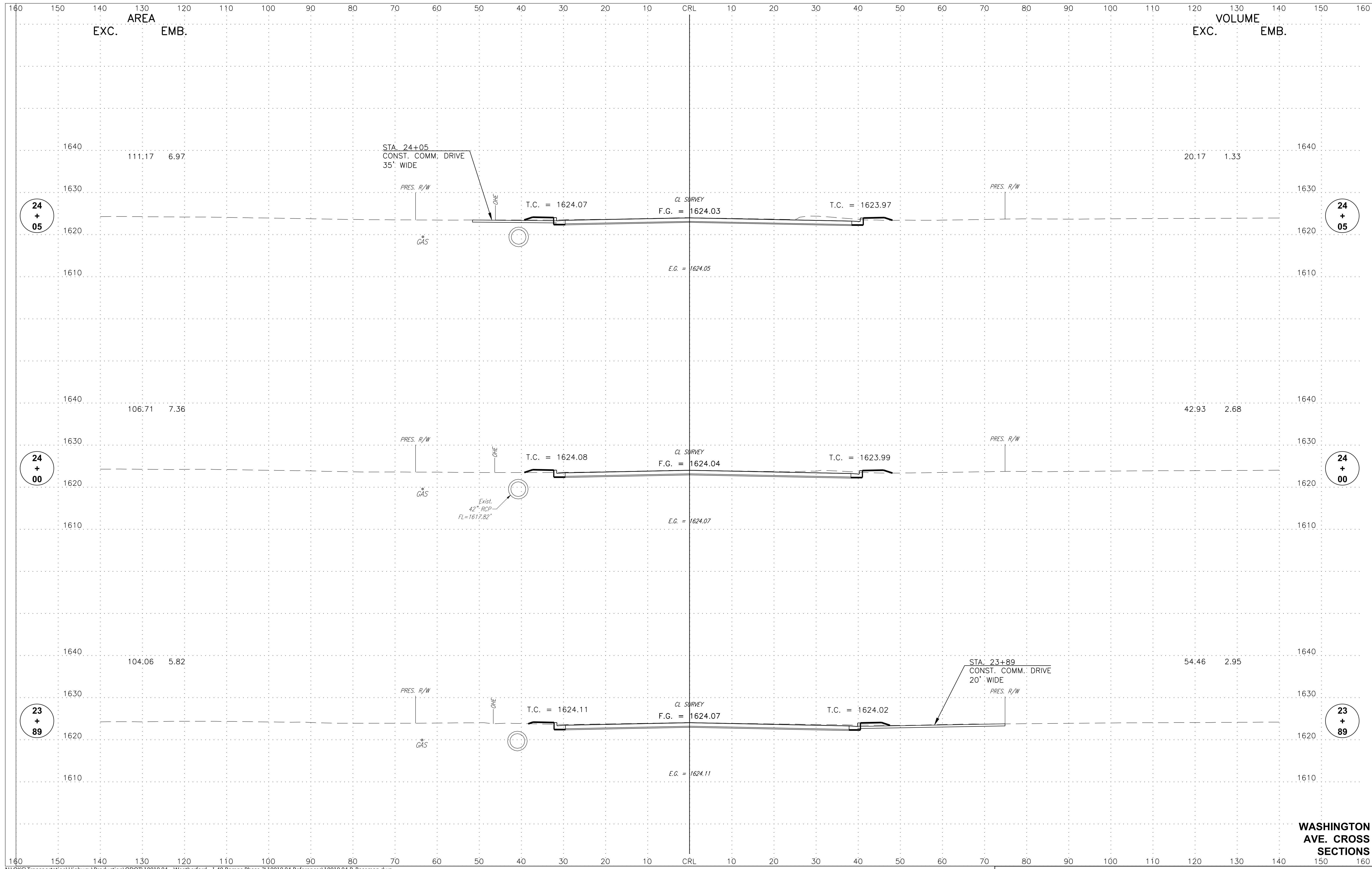


STA. 22+51.00
 BEGIN INCIDENTAL CONSTRUCTION

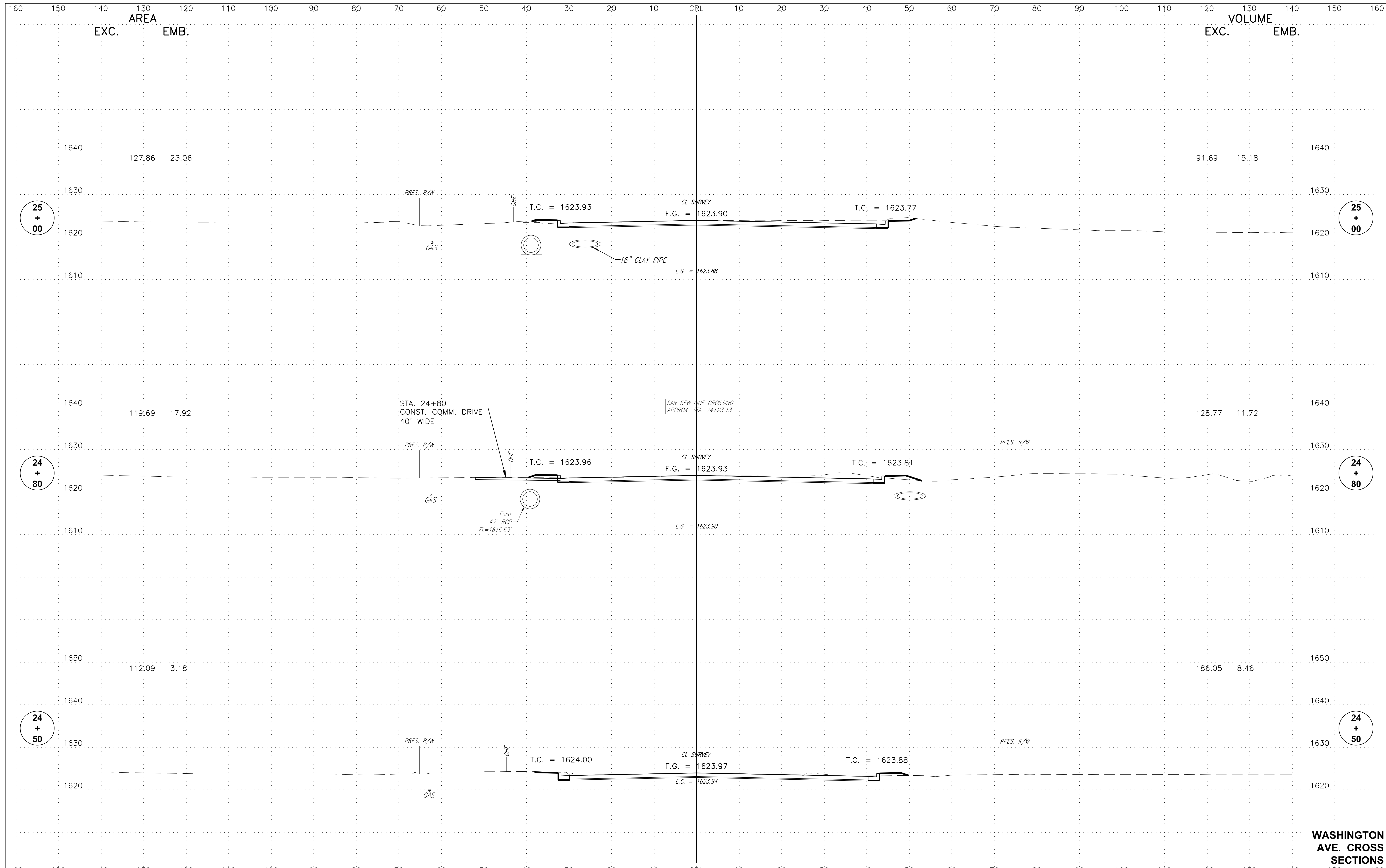


STA. 23+13.19
 END INCIDENTAL CONSTRUCTION
 BEGIN WASHINGTON AVE. CONSTRUCTION

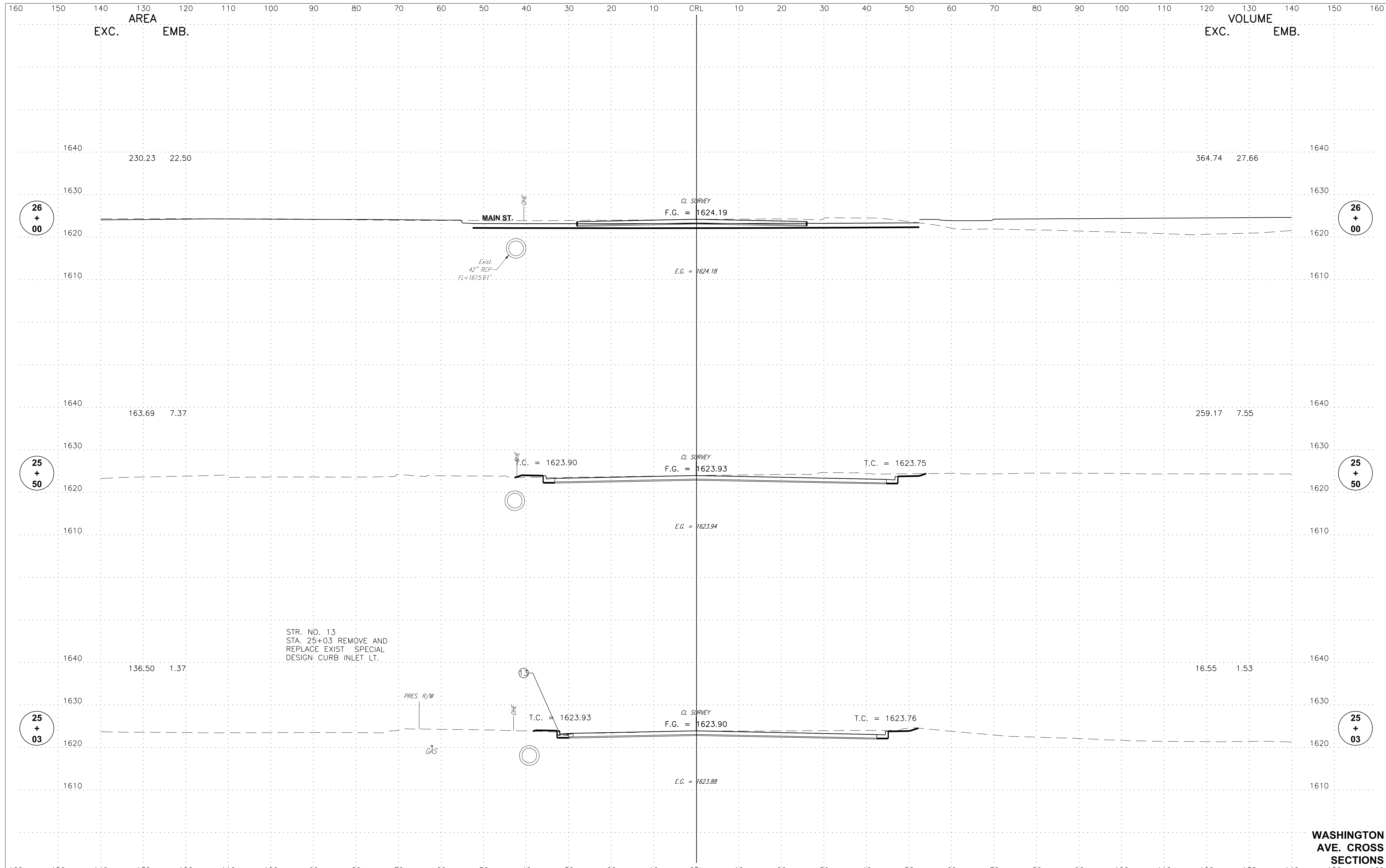
WASHINGTON
 AVE. CROSS
 SECTIONS



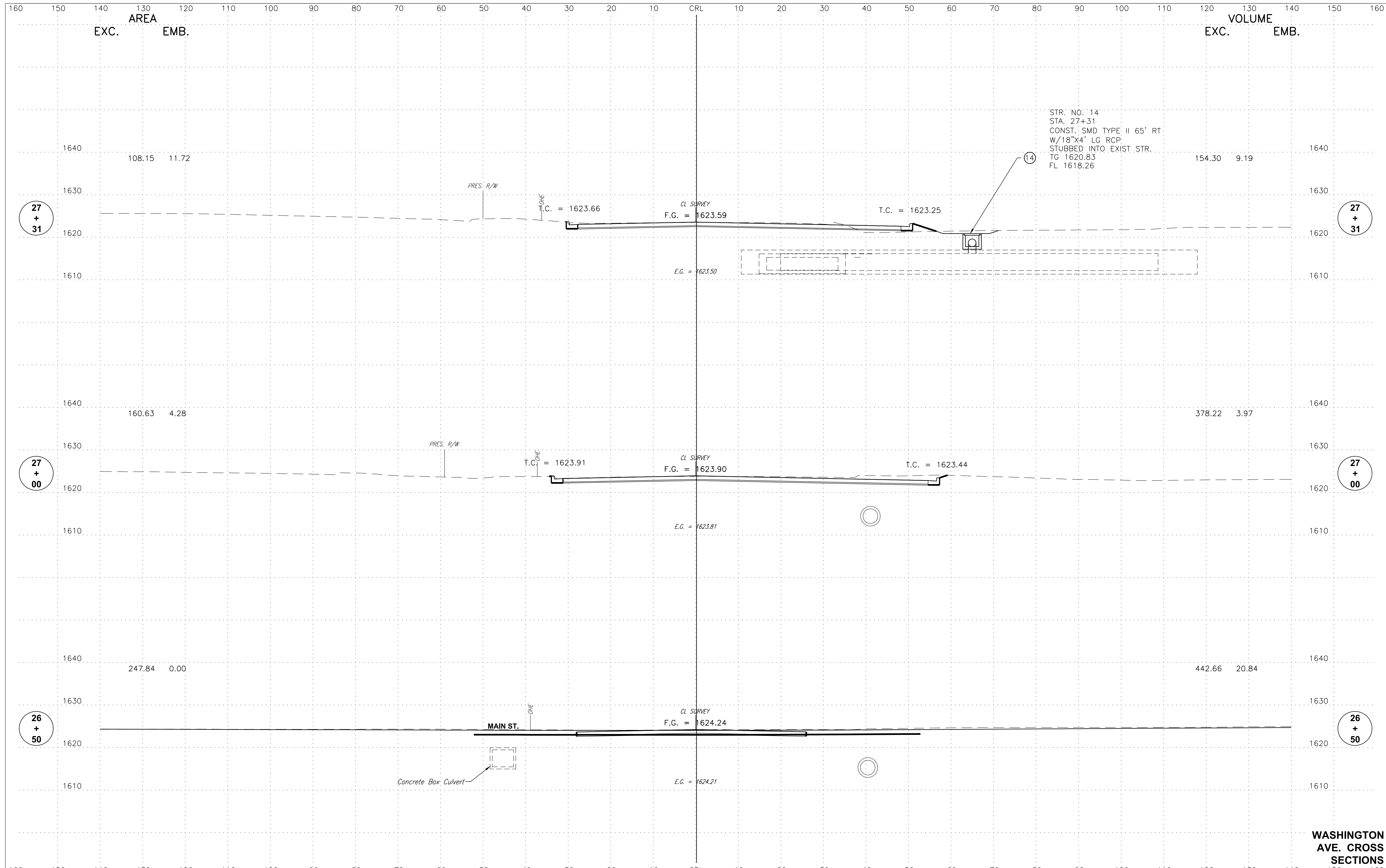
**WASHINGTON
 AVE. CROSS
 SECTIONS**



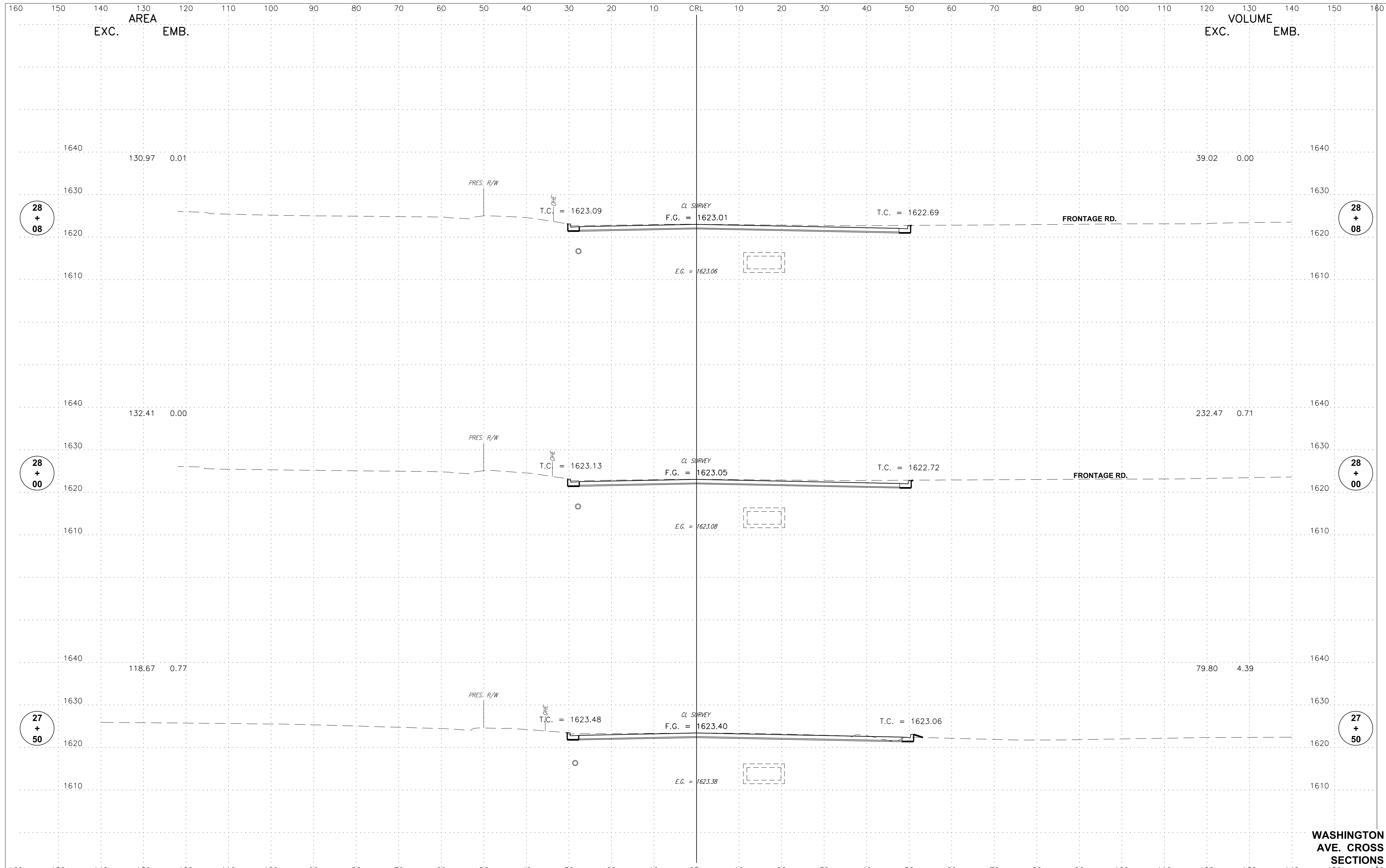
**WASHINGTON
AVE. CROSS
SECTIONS**



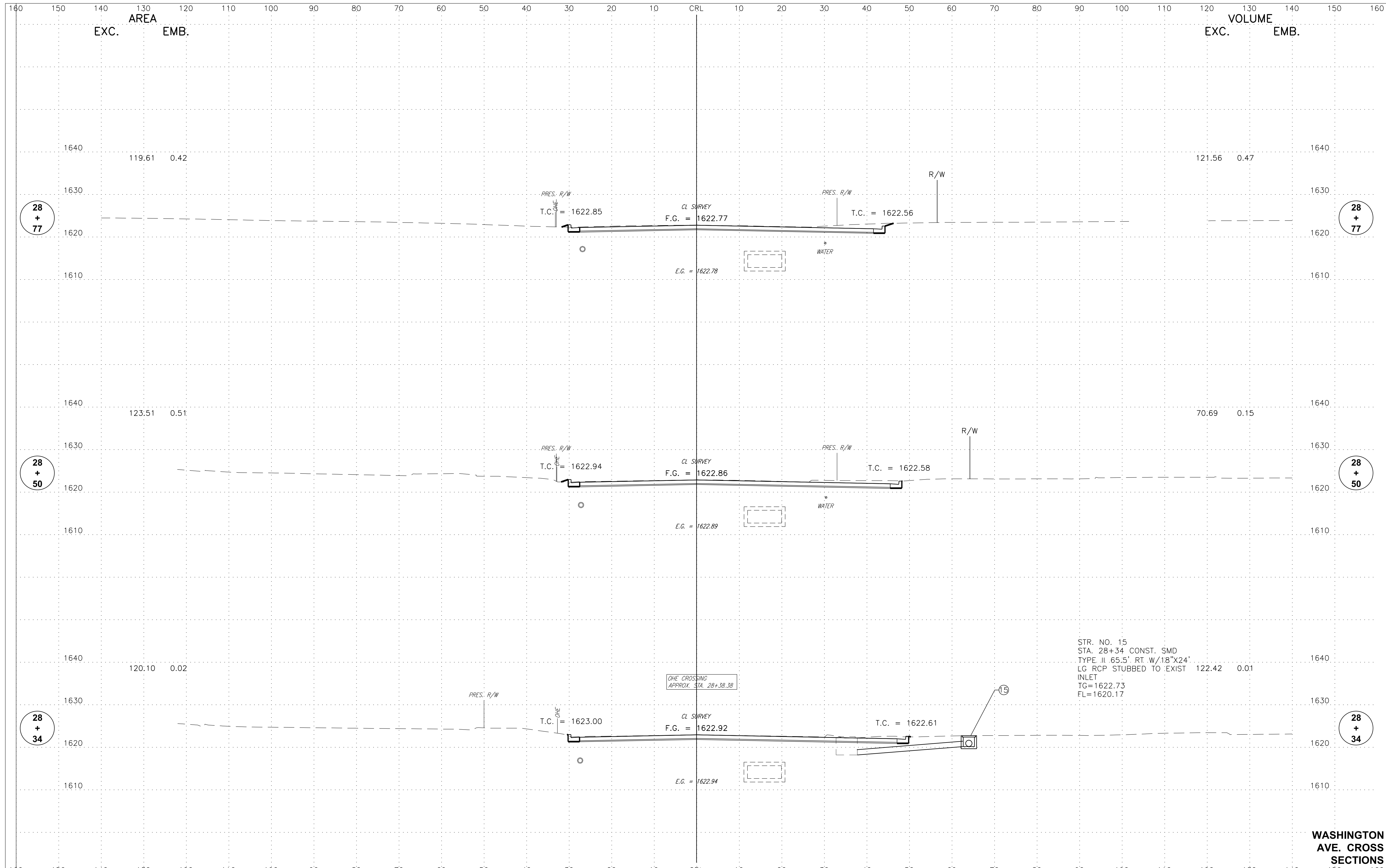
**WASHINGTON
AVE. CROSS
SECTIONS**



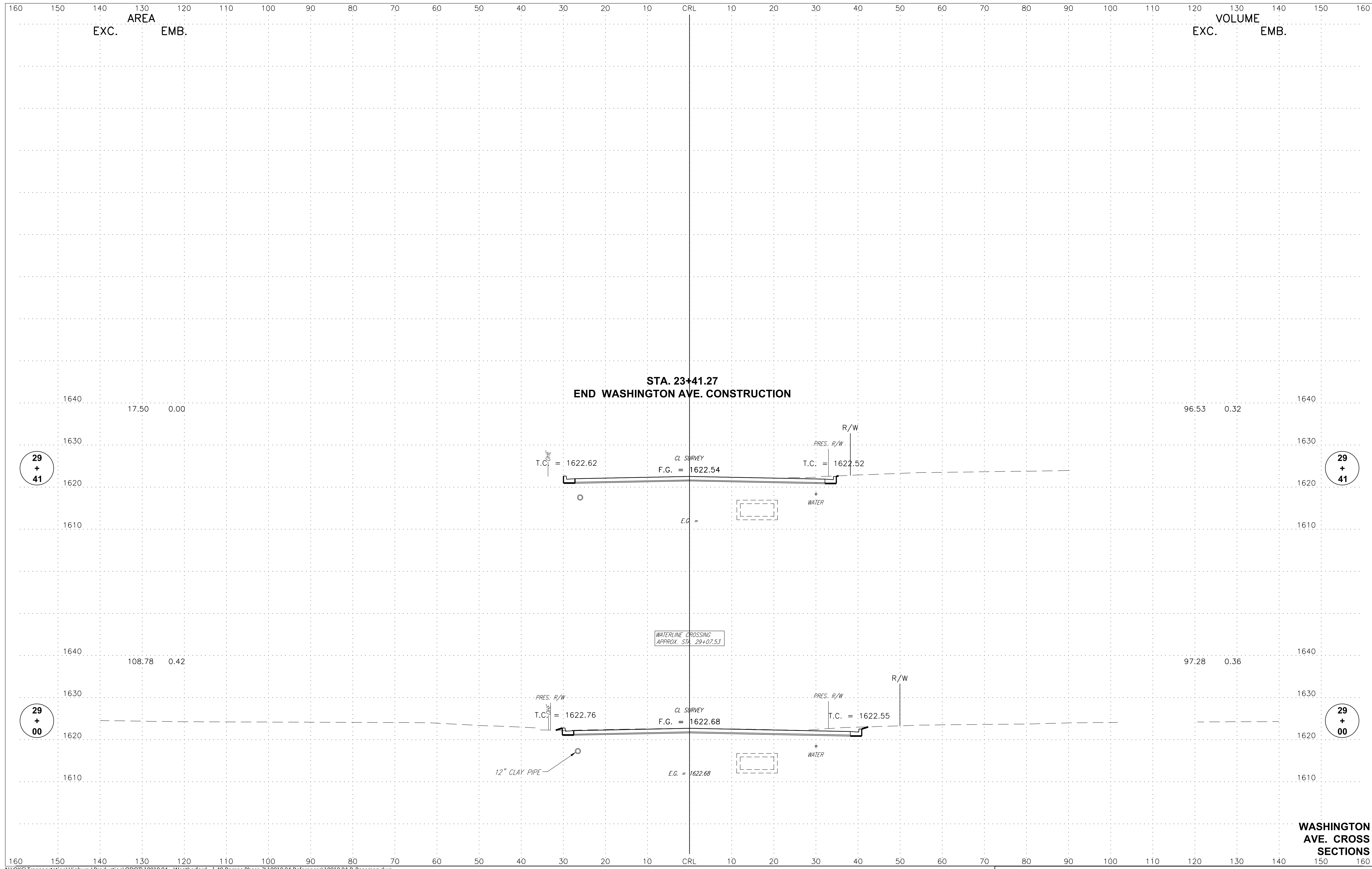
**WASHINGTON
 AVE. CROSS
 SECTIONS**

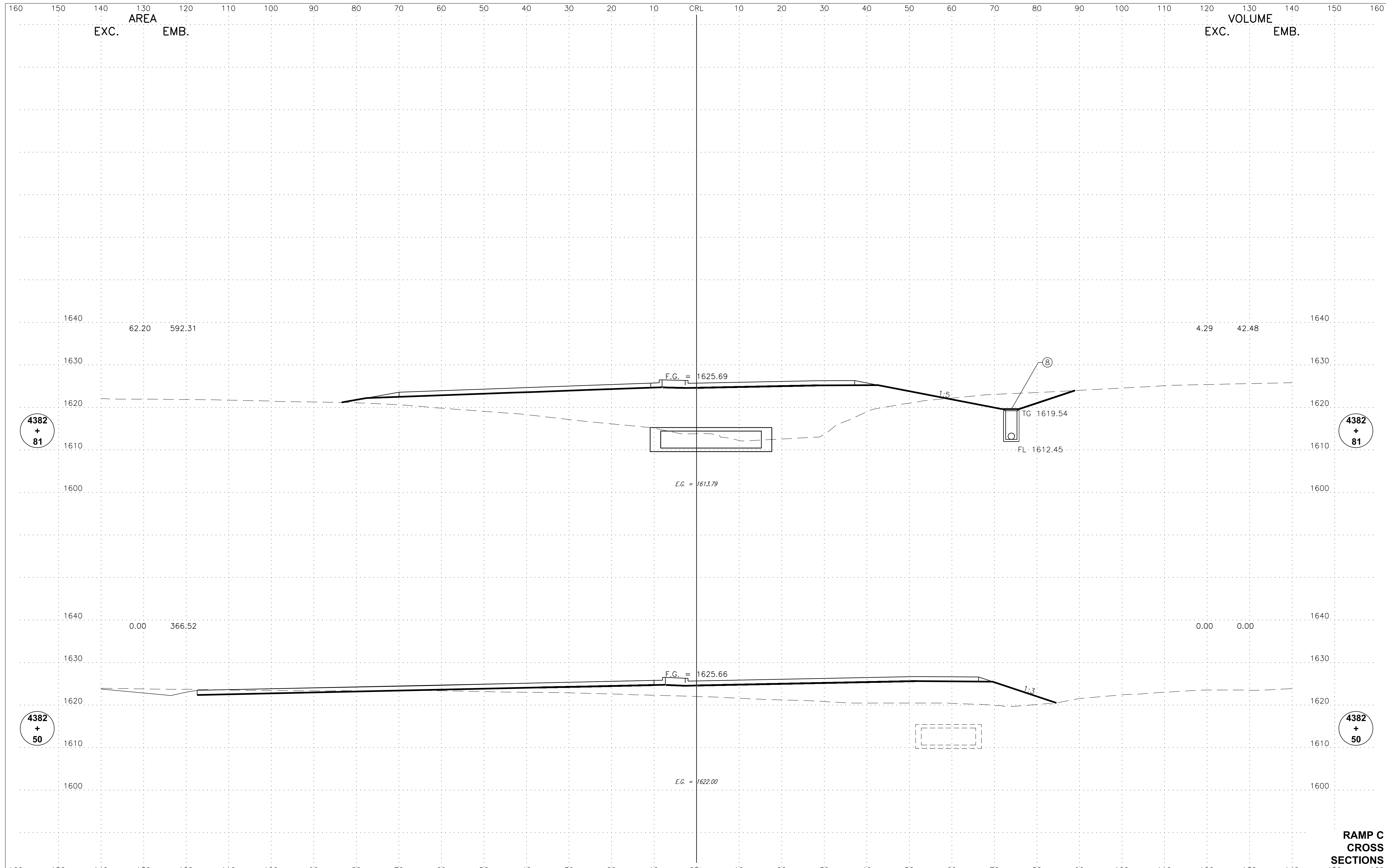


**WASHINGTON
AVE. CROSS
SECTIONS**

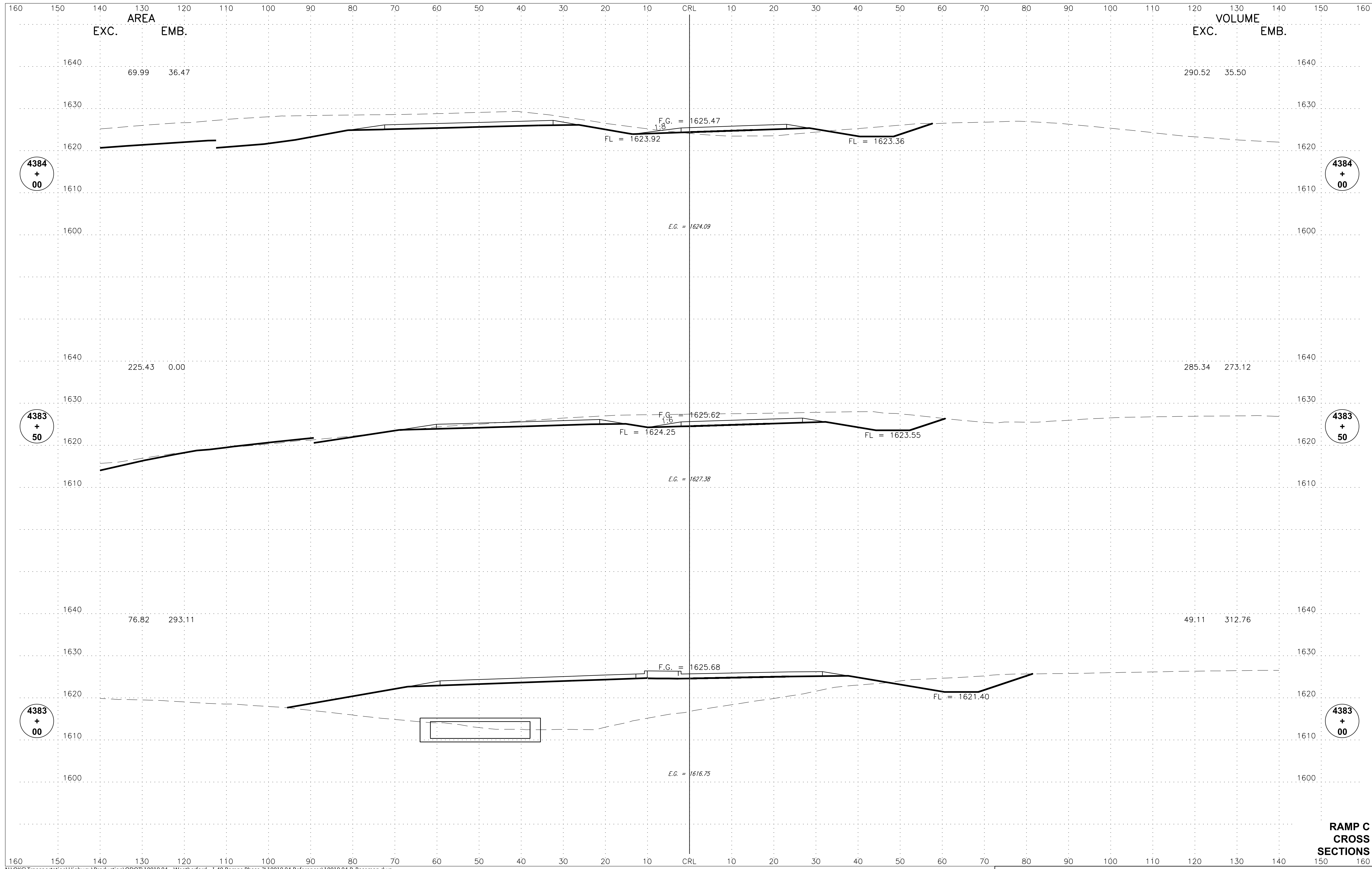


**WASHINGTON
 AVE. CROSS
 SECTIONS**

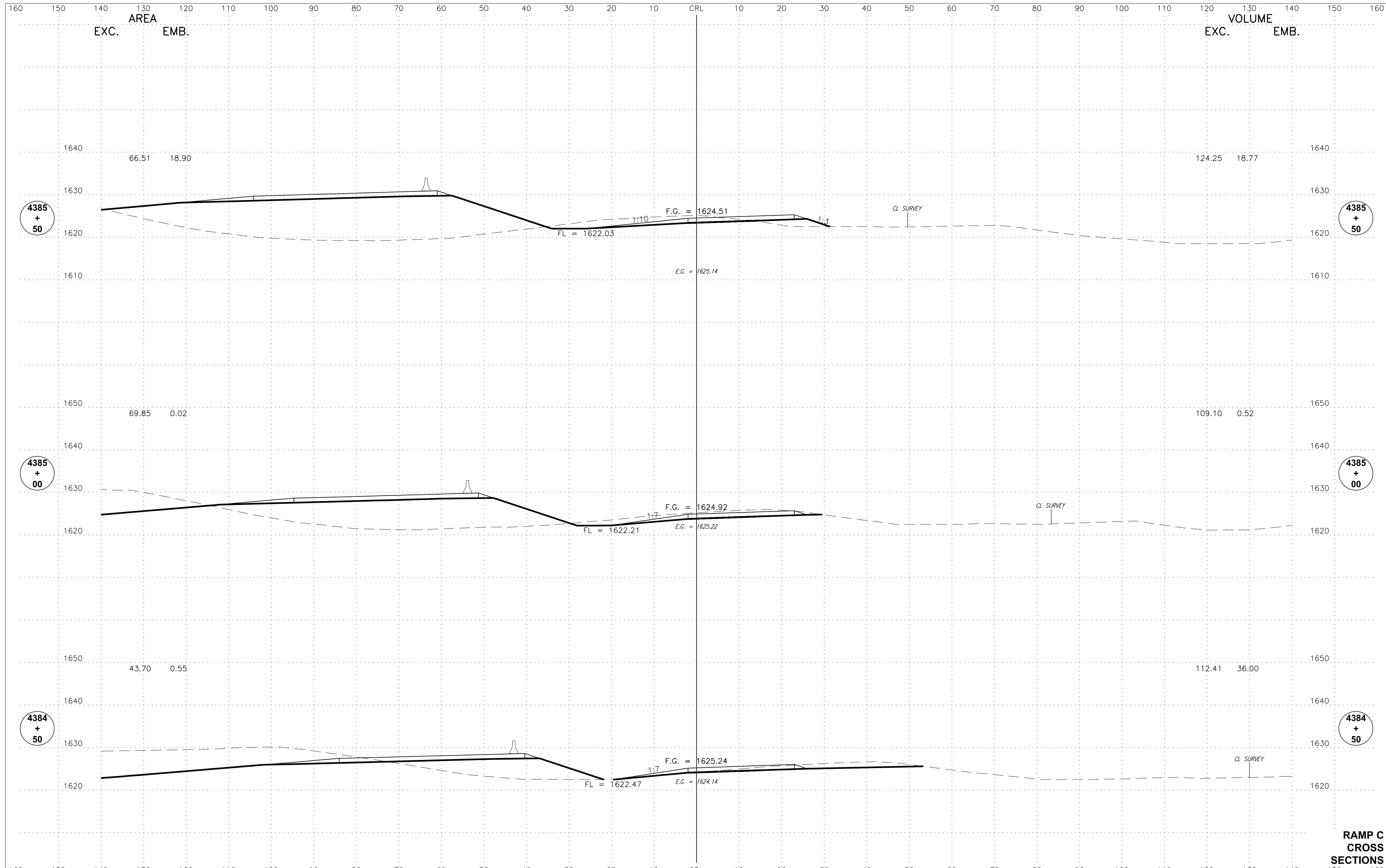




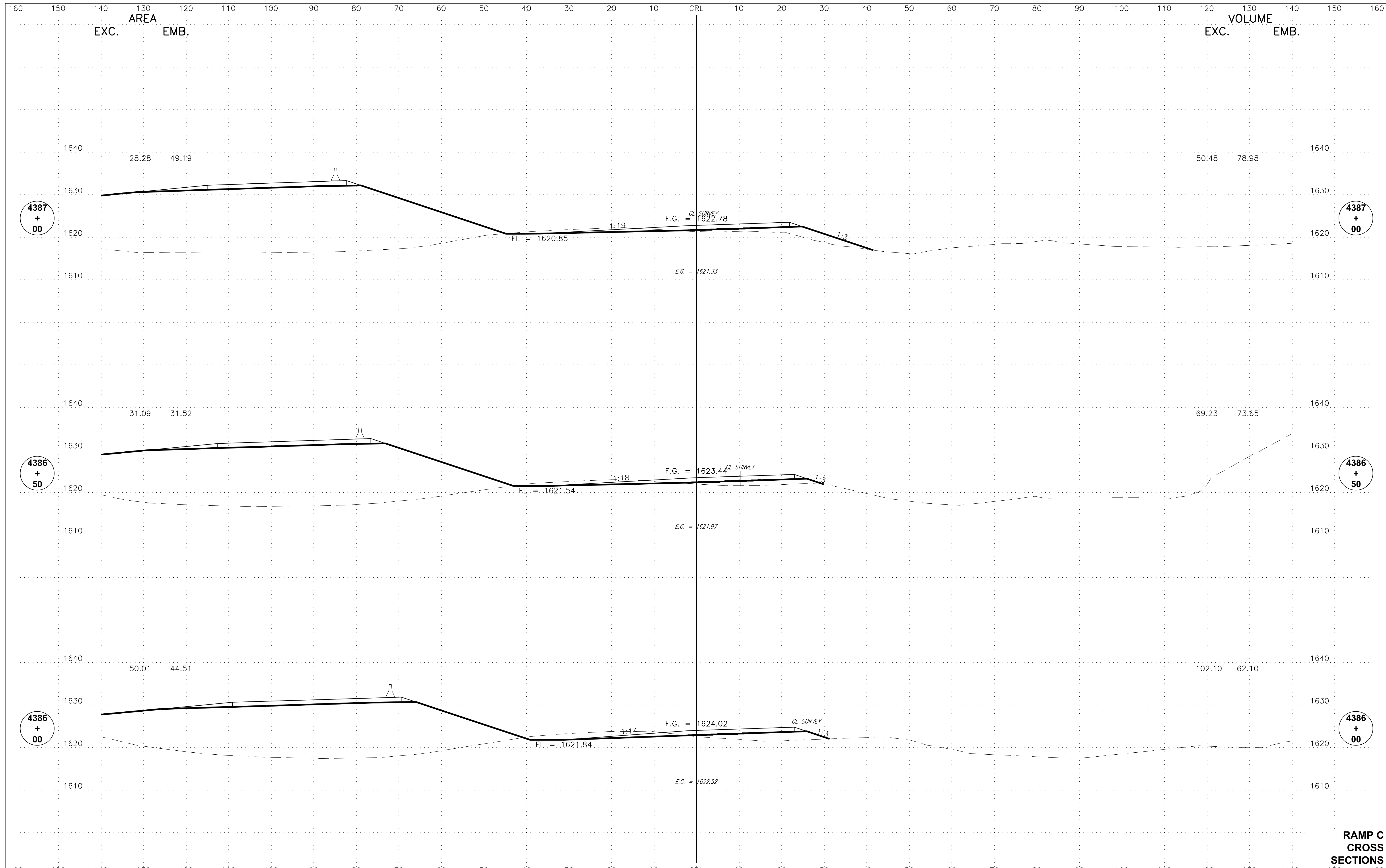
**RAMP C
 CROSS
 SECTIONS**



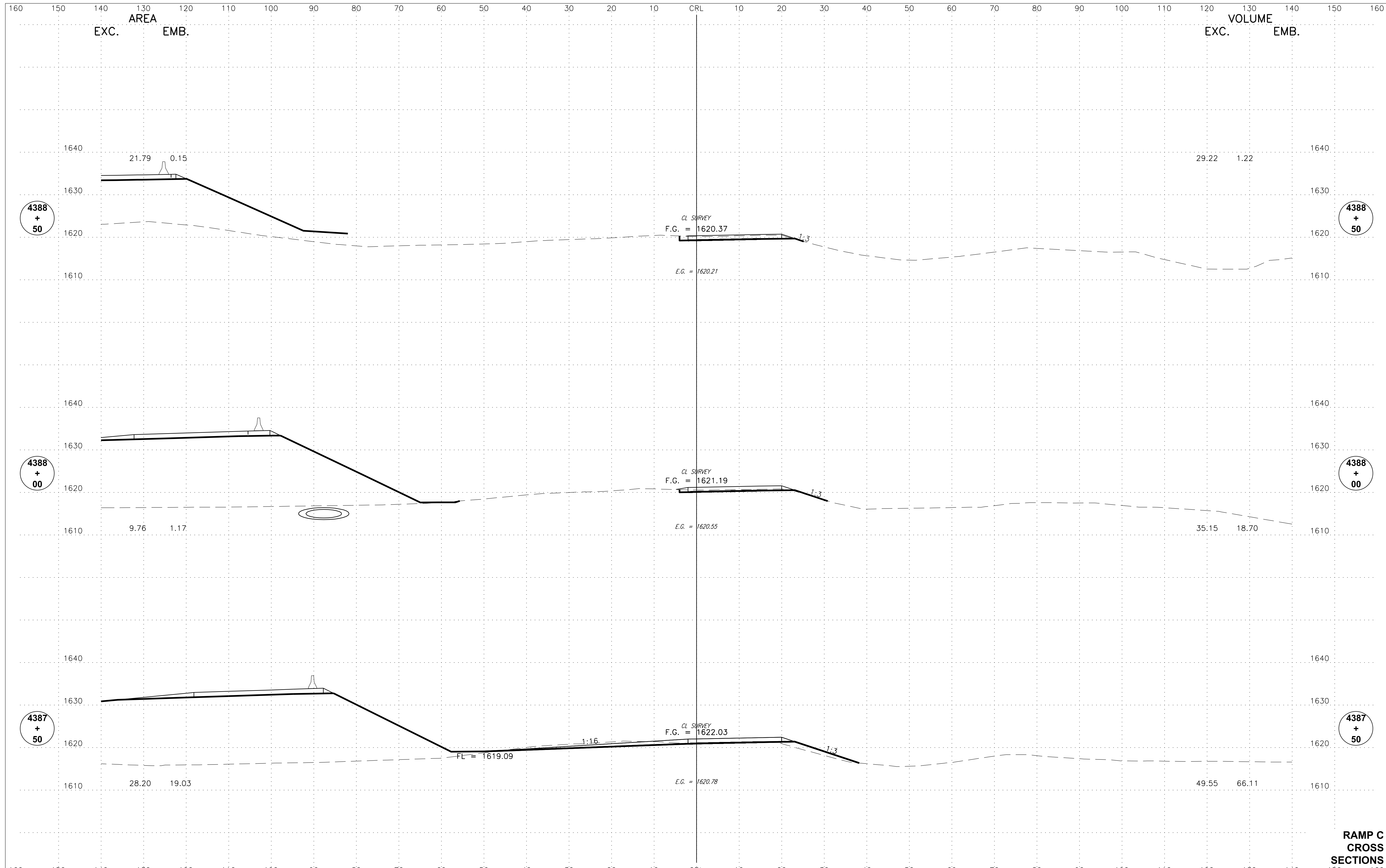
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 CROSS
 SECTIONS**



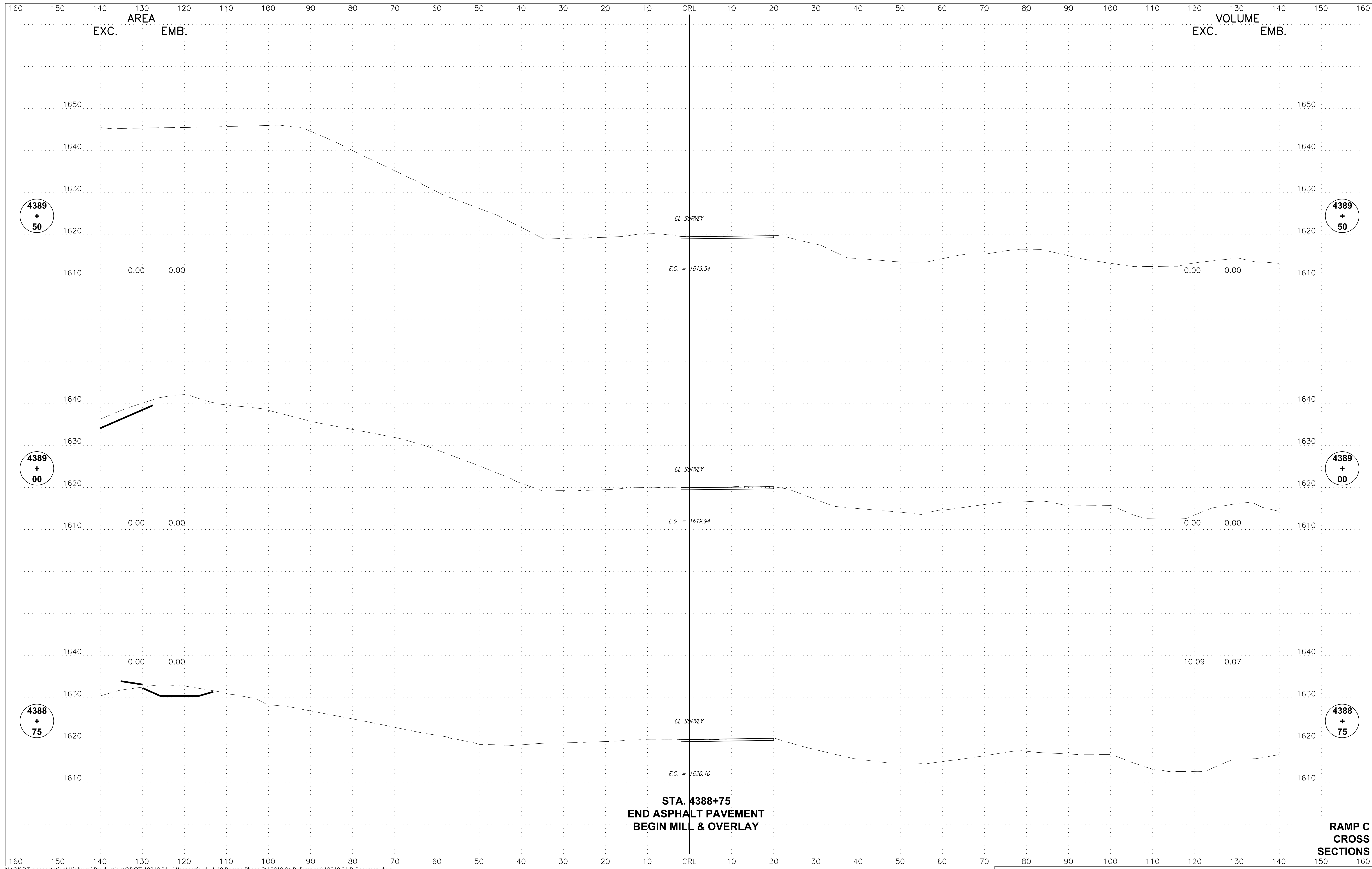
**RAMP C
CROSS
SECTIONS**



**RAMP C
CROSS
SECTIONS**



**RAMP C
CROSS
SECTIONS**



AREA
EXC. EMB.

VOLUME
EXC. EMB.

4389
+
50

4389
+
50

4389
+
00

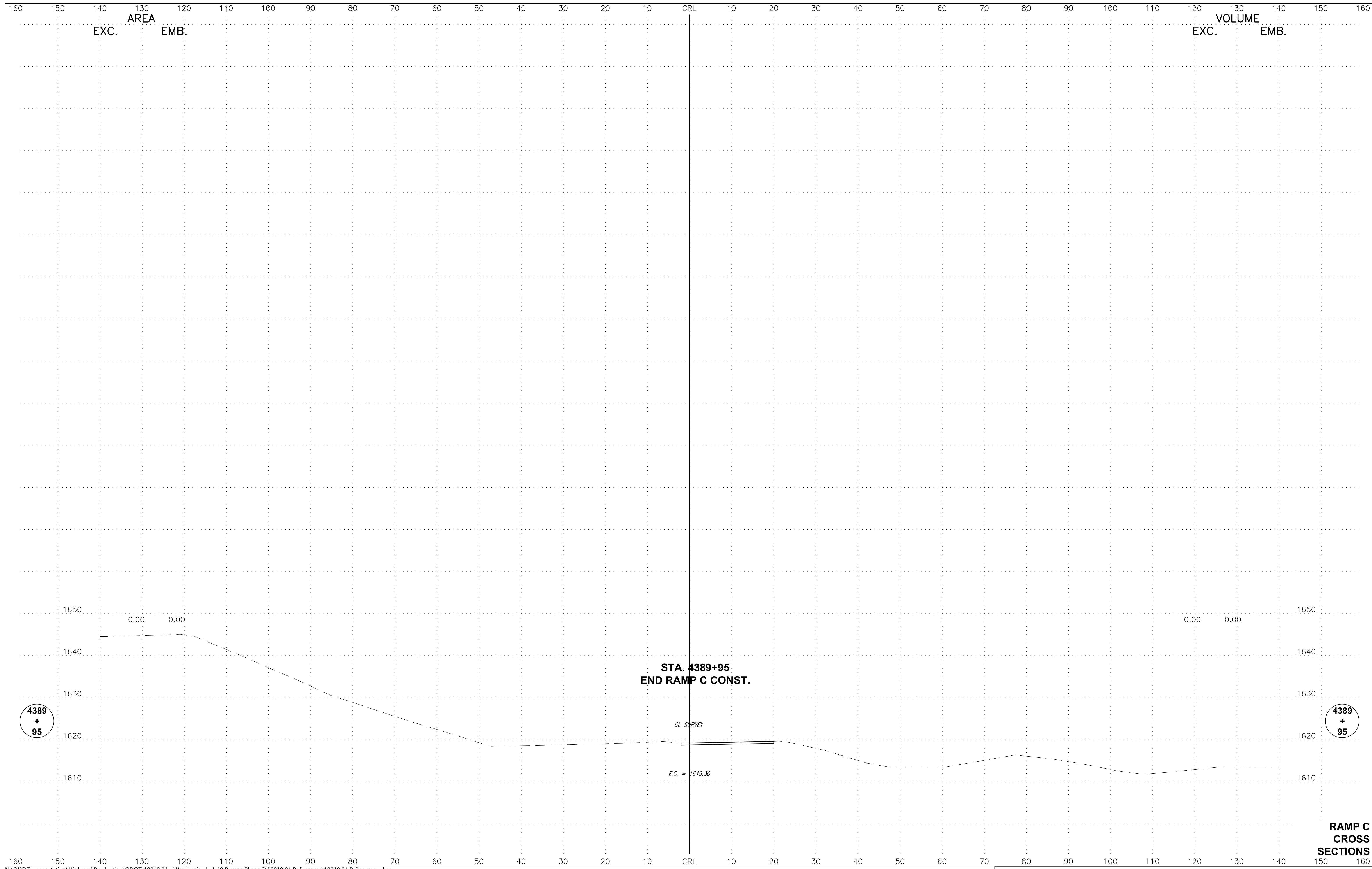
4389
+
00

4388
+
75

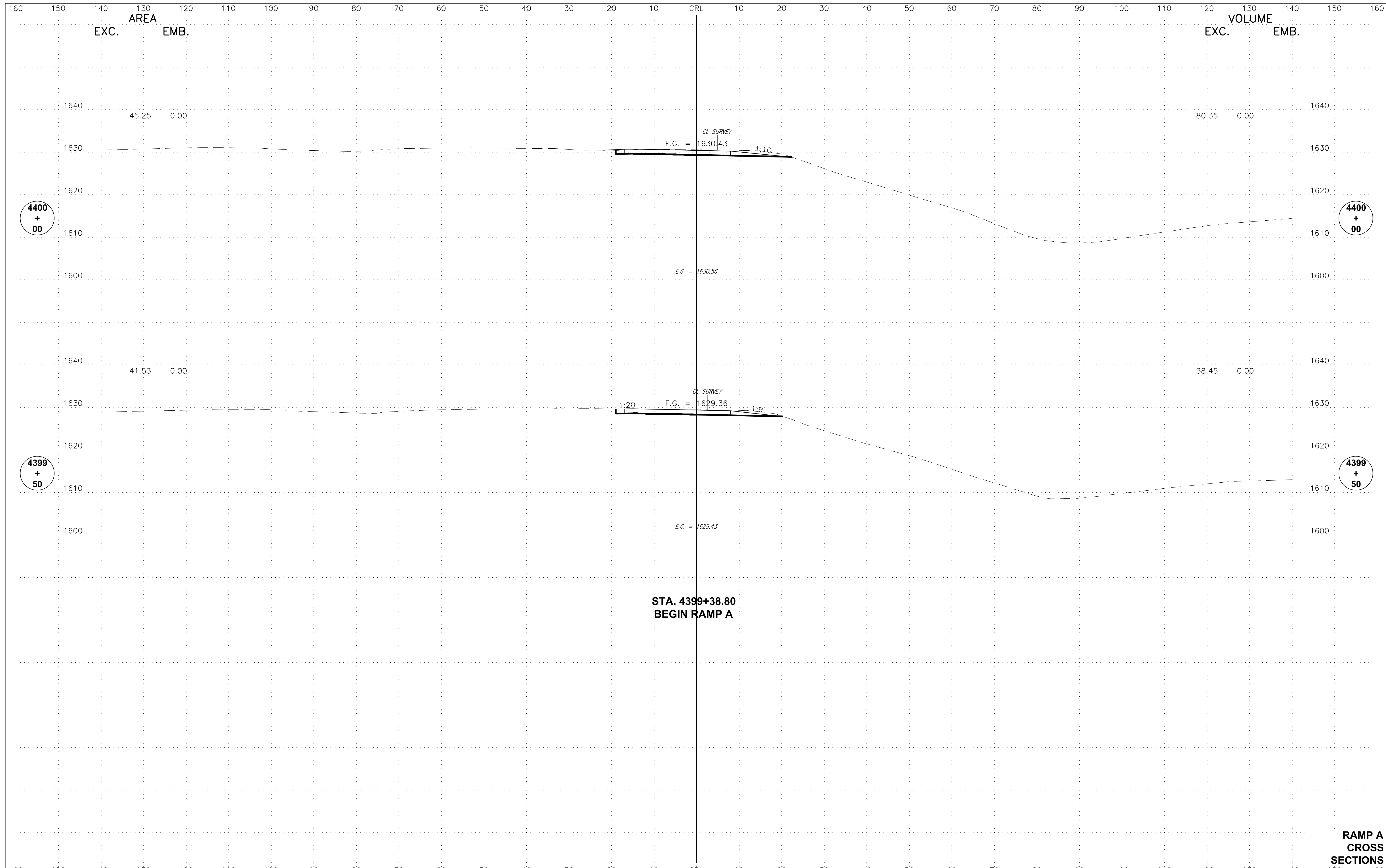
4388
+
75

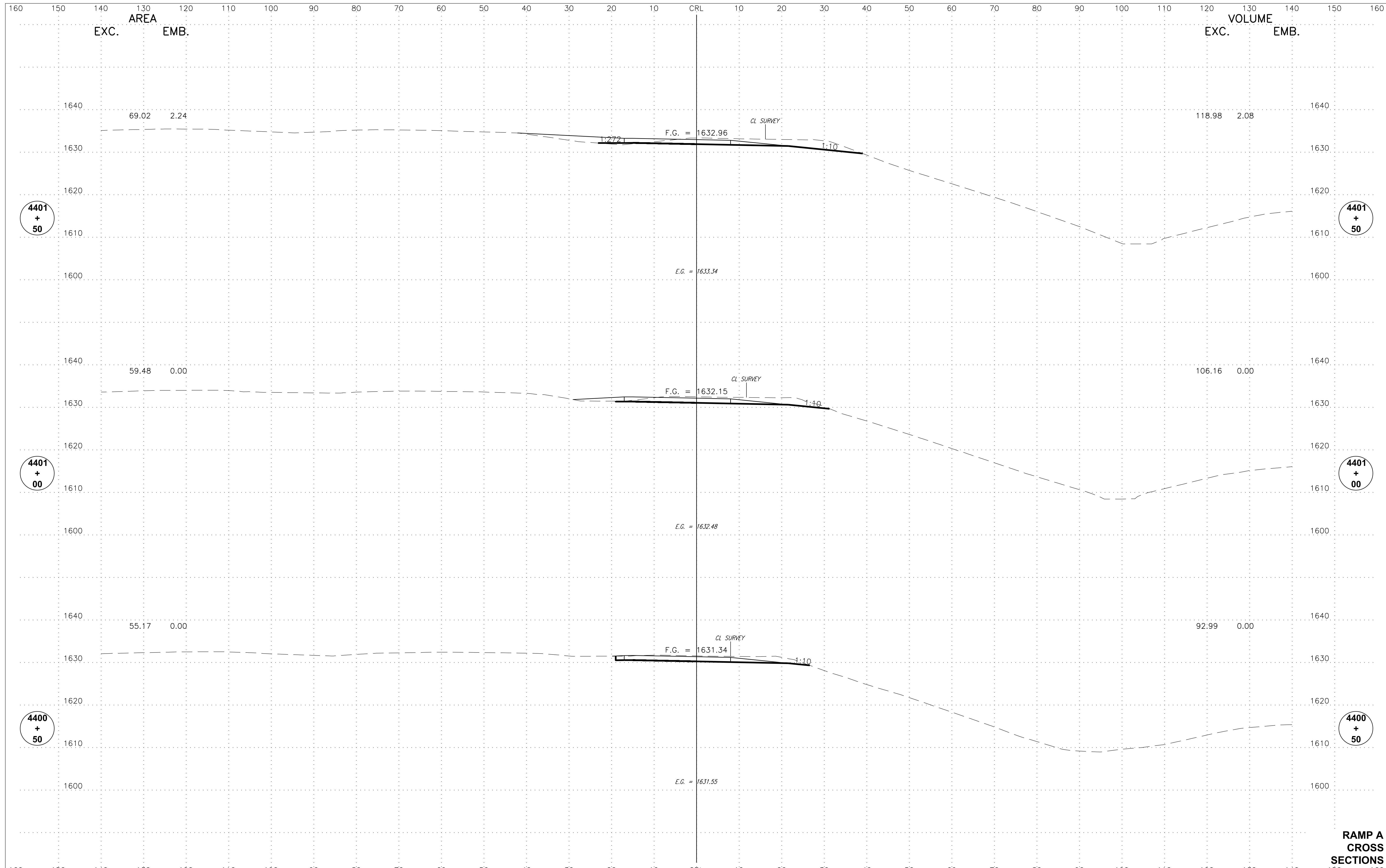
STA. 4388+75
END ASPHALT PAVEMENT
BEGIN MILL & OVERLAY

RAMP C
CROSS
SECTIONS

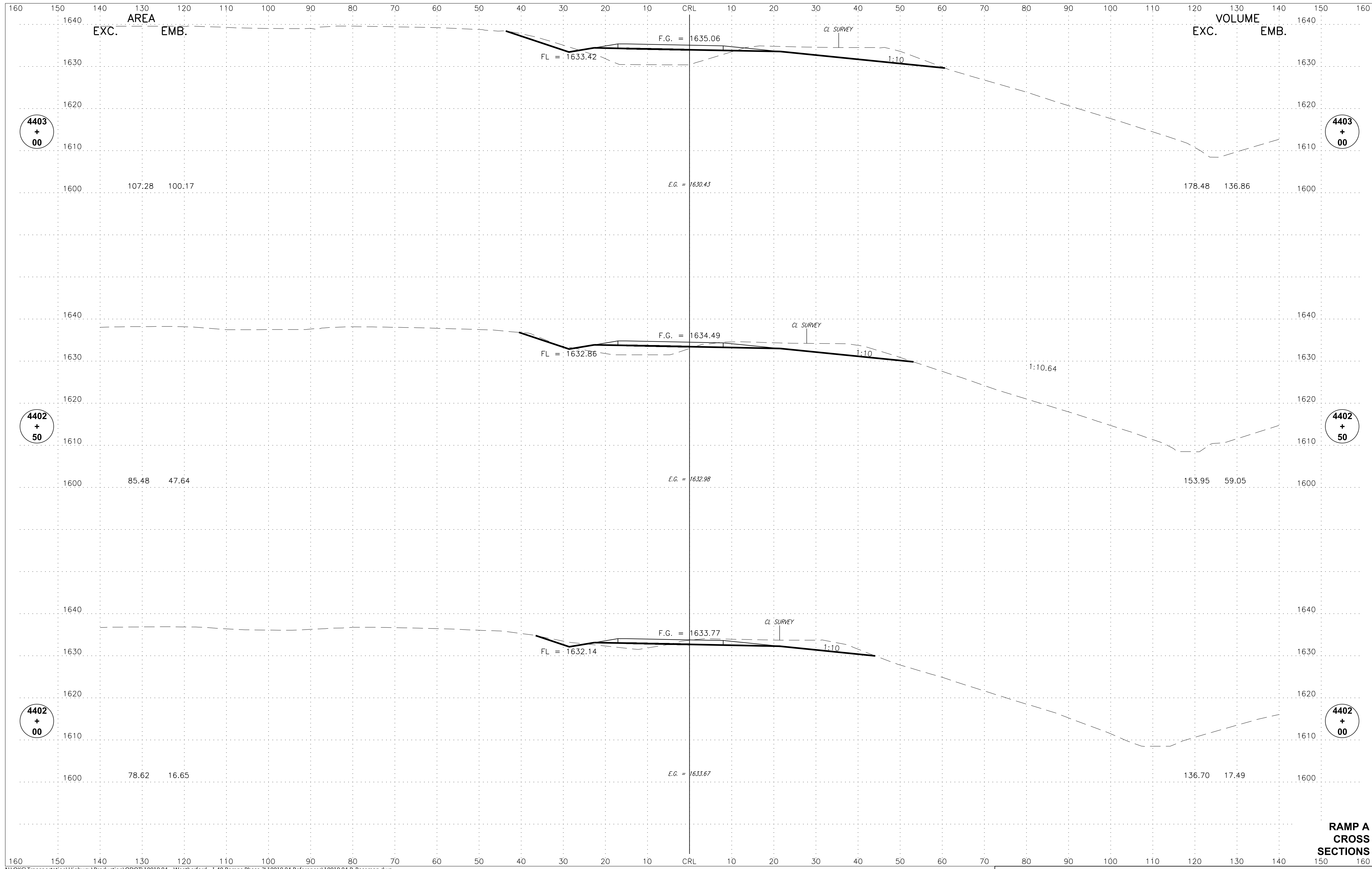


**RAMP C
 CROSS
 SECTIONS**

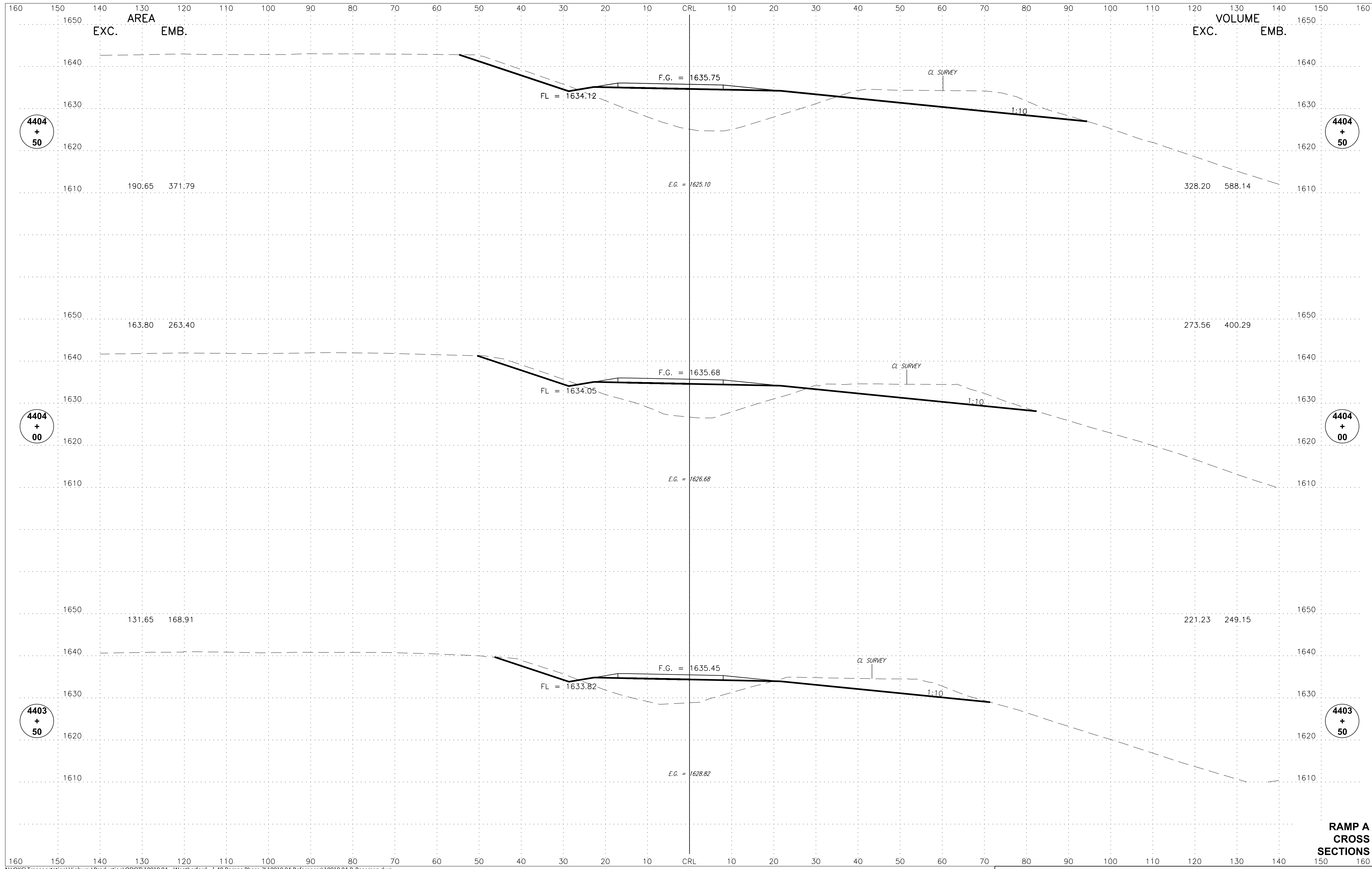




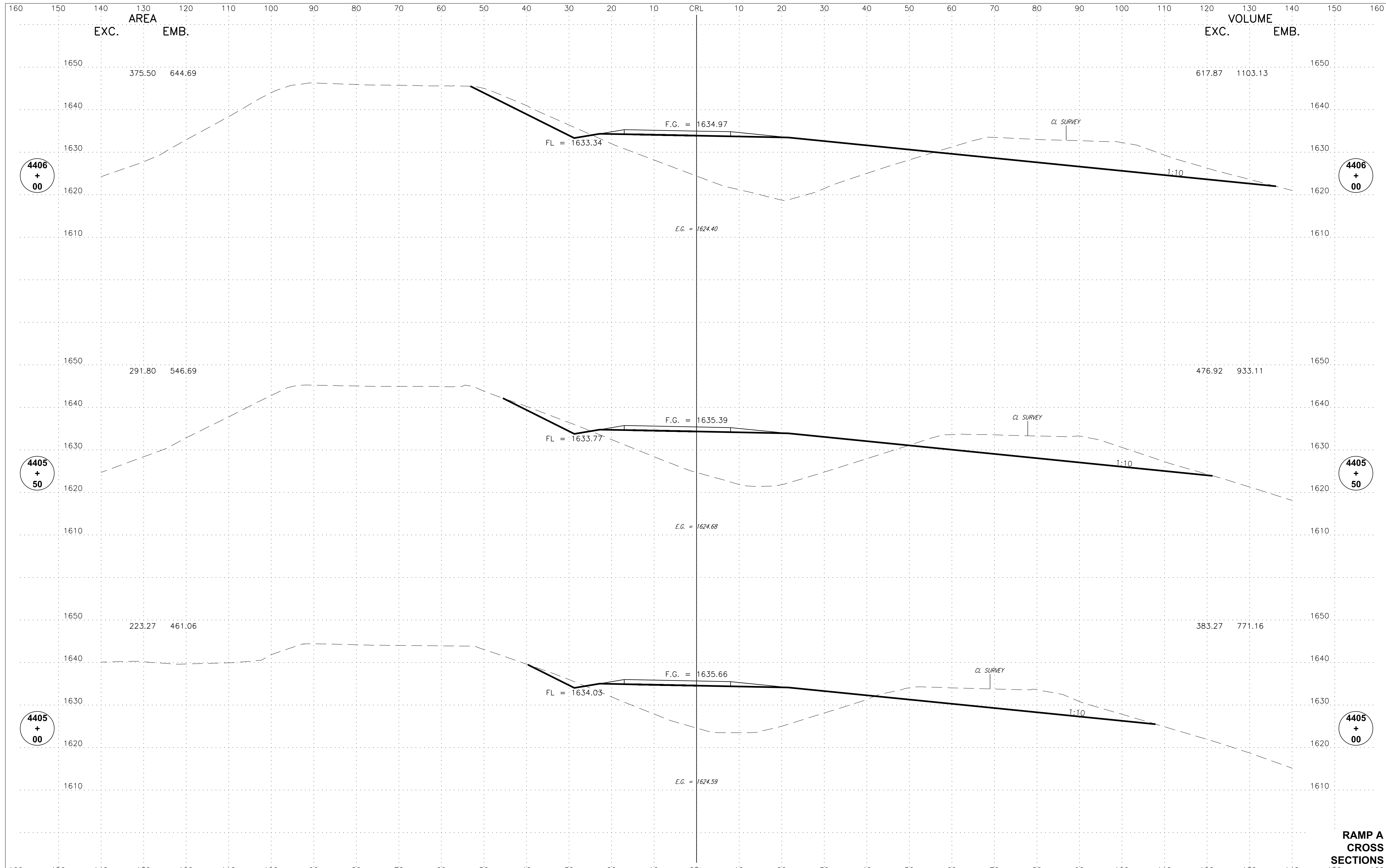
**RAMP A
CROSS
SECTIONS**



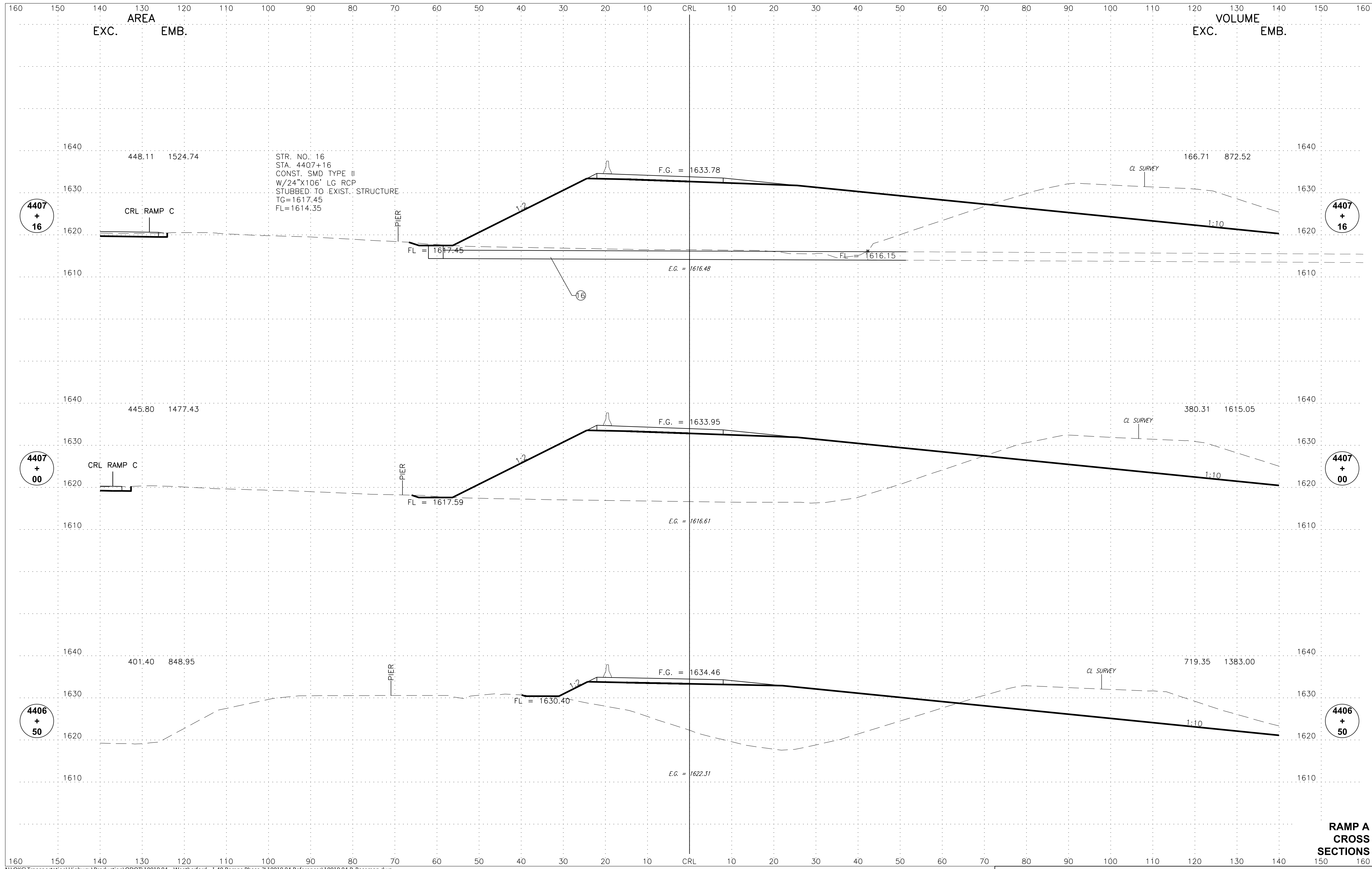
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 CROSS
 SECTIONS**



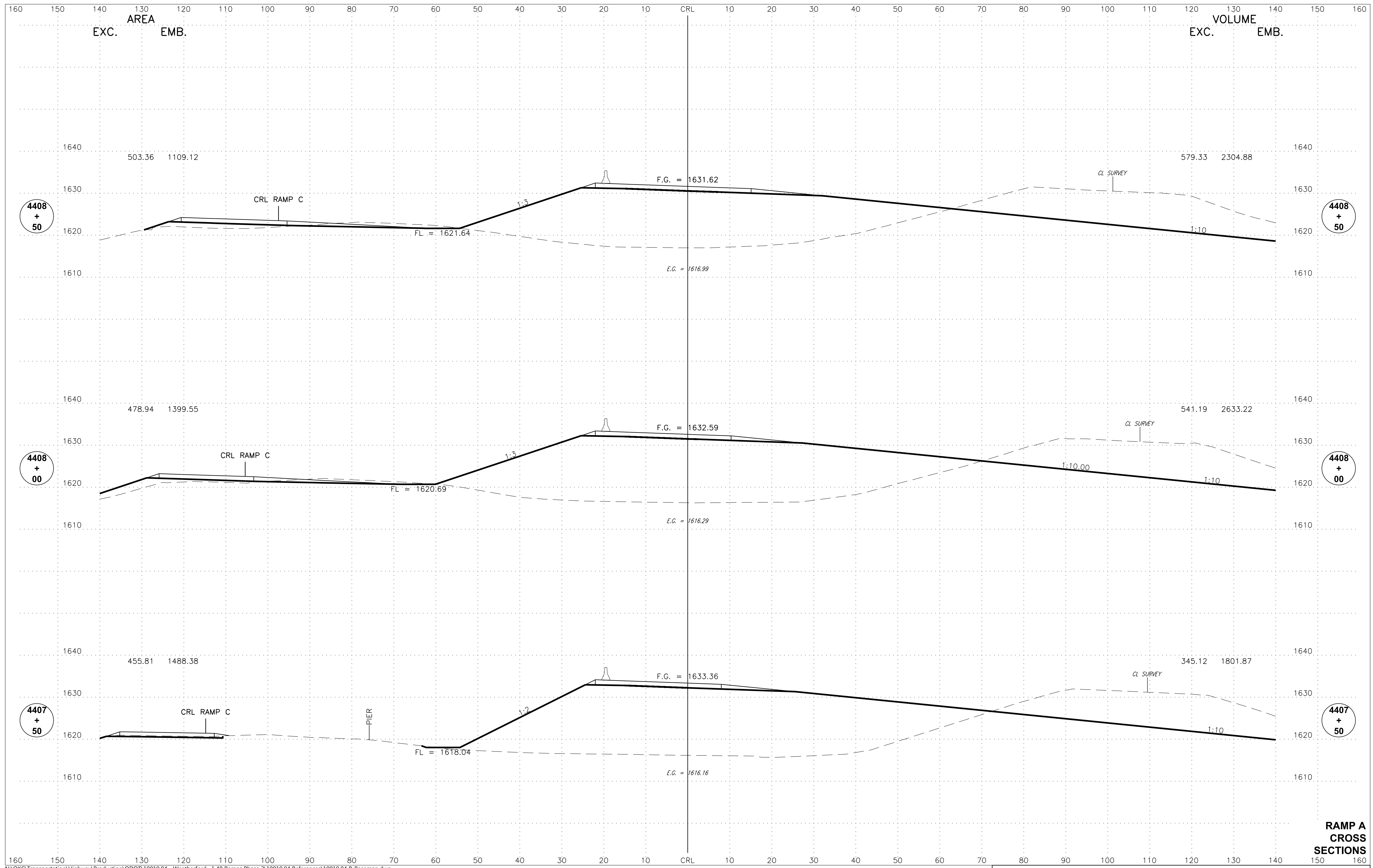
**RAMP A
 CROSS
 SECTIONS**



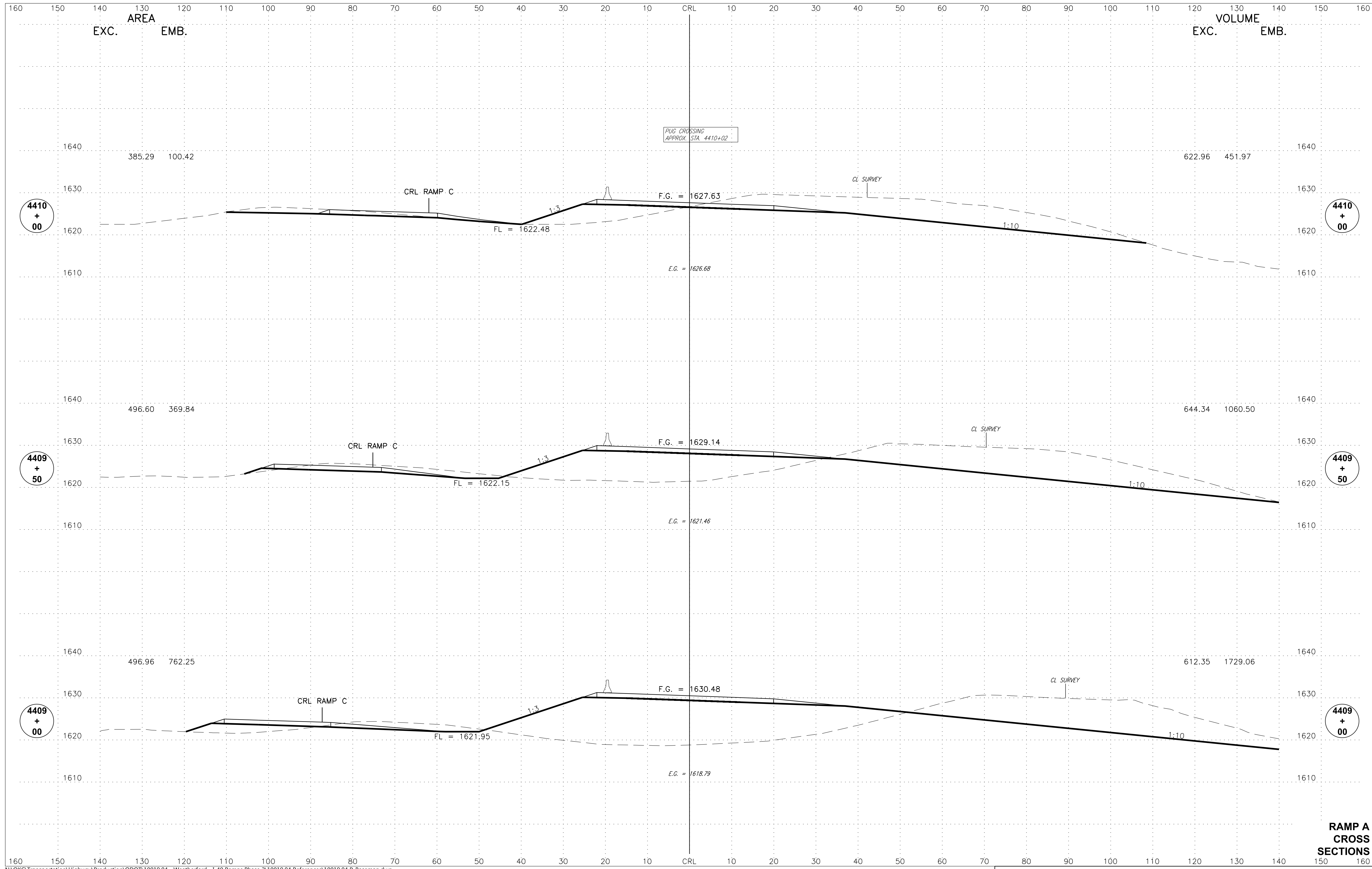
**RAMP A
CROSS
SECTIONS**



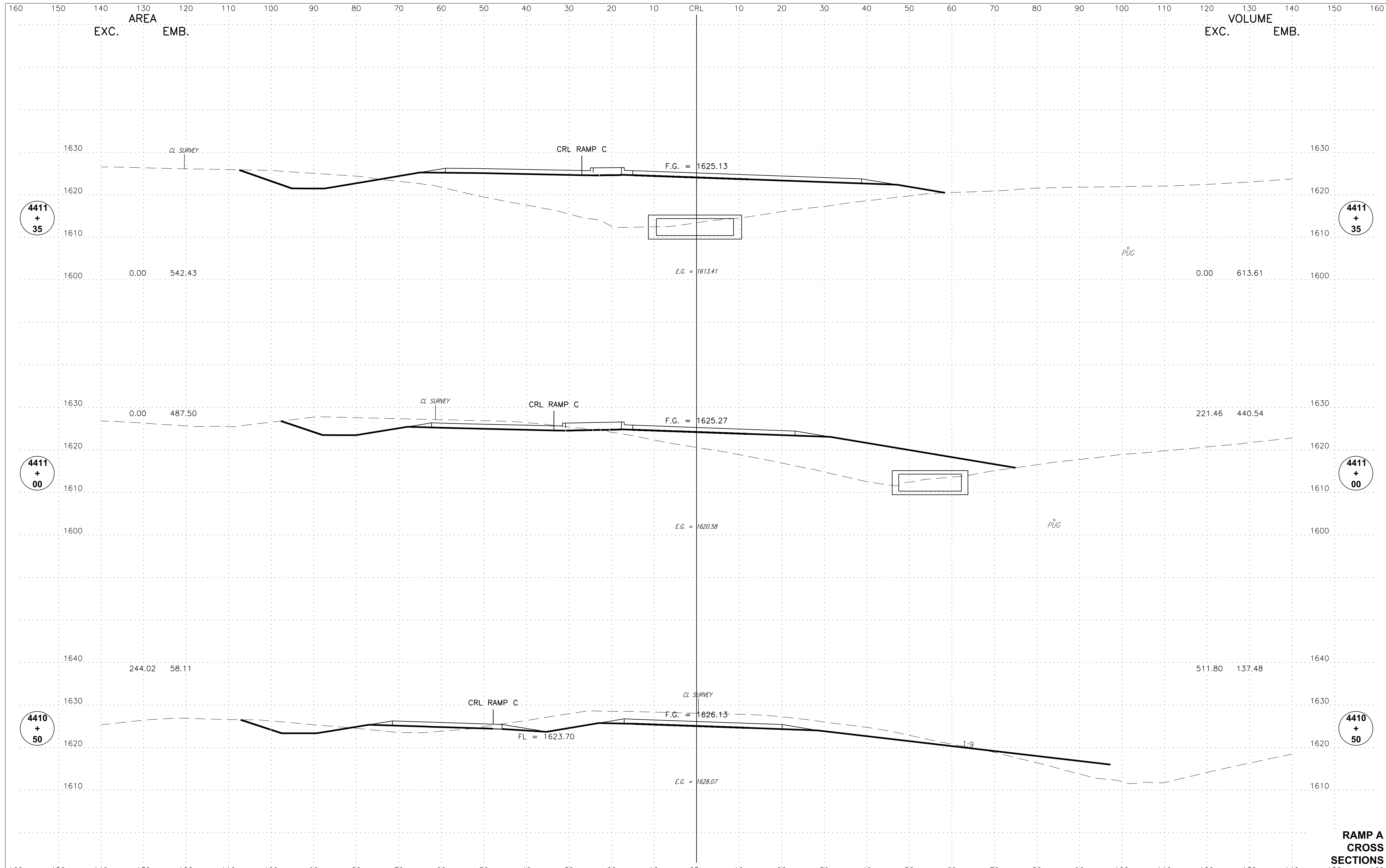
**RAMP A
 CROSS
 SECTIONS**



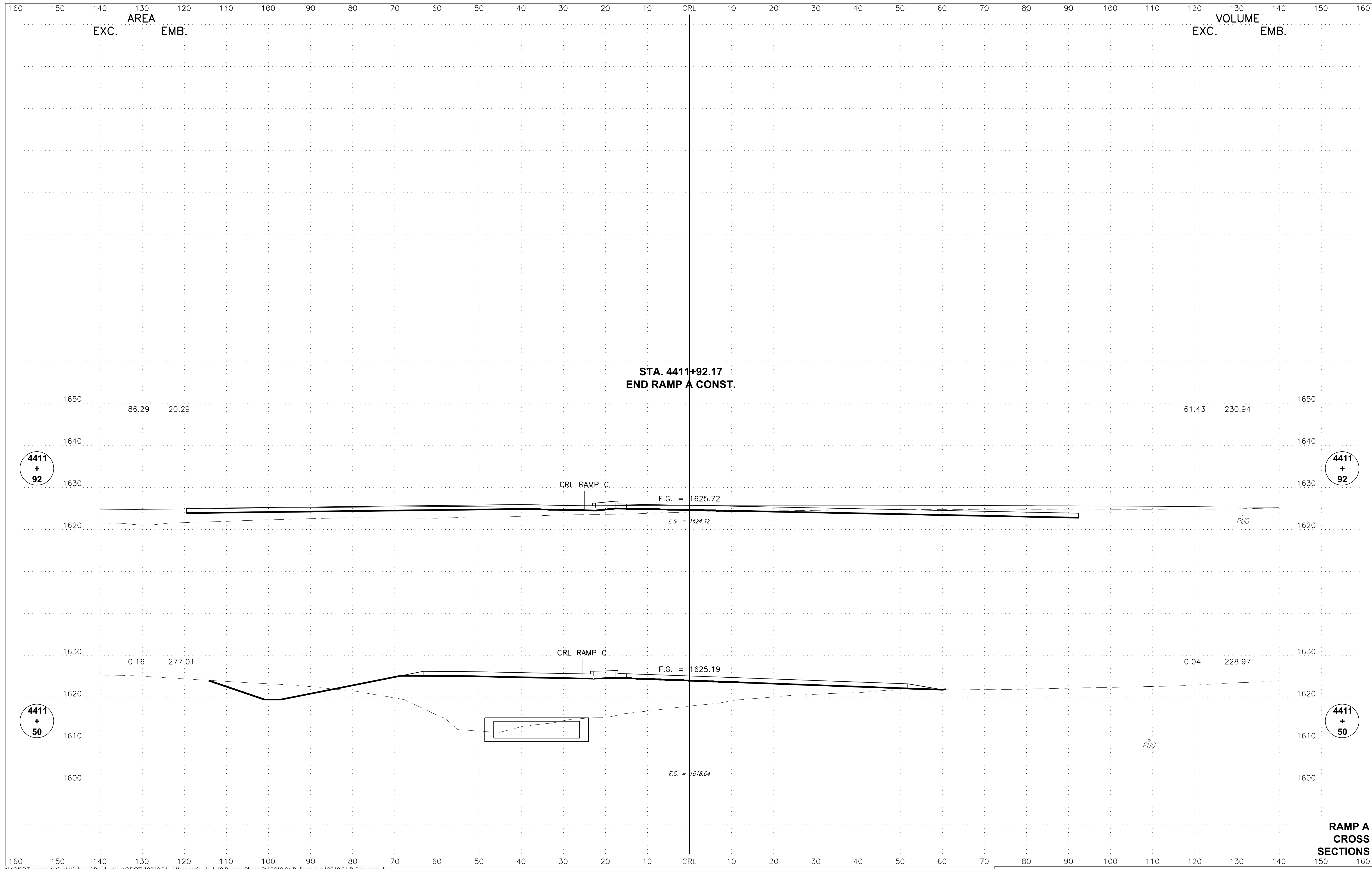
**RAMP A
CROSS
SECTIONS**



**RAMP A
CROSS
SECTIONS**

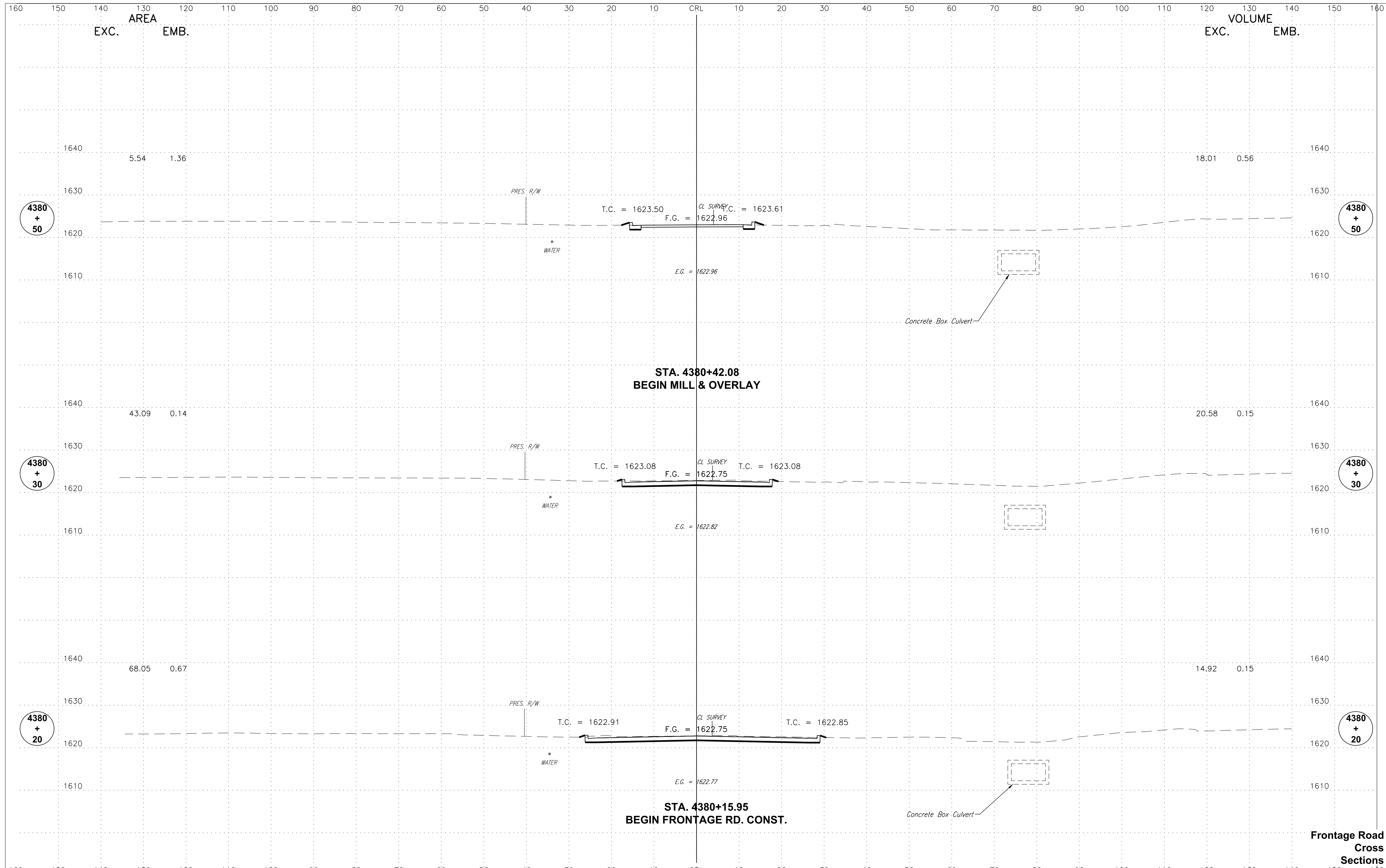


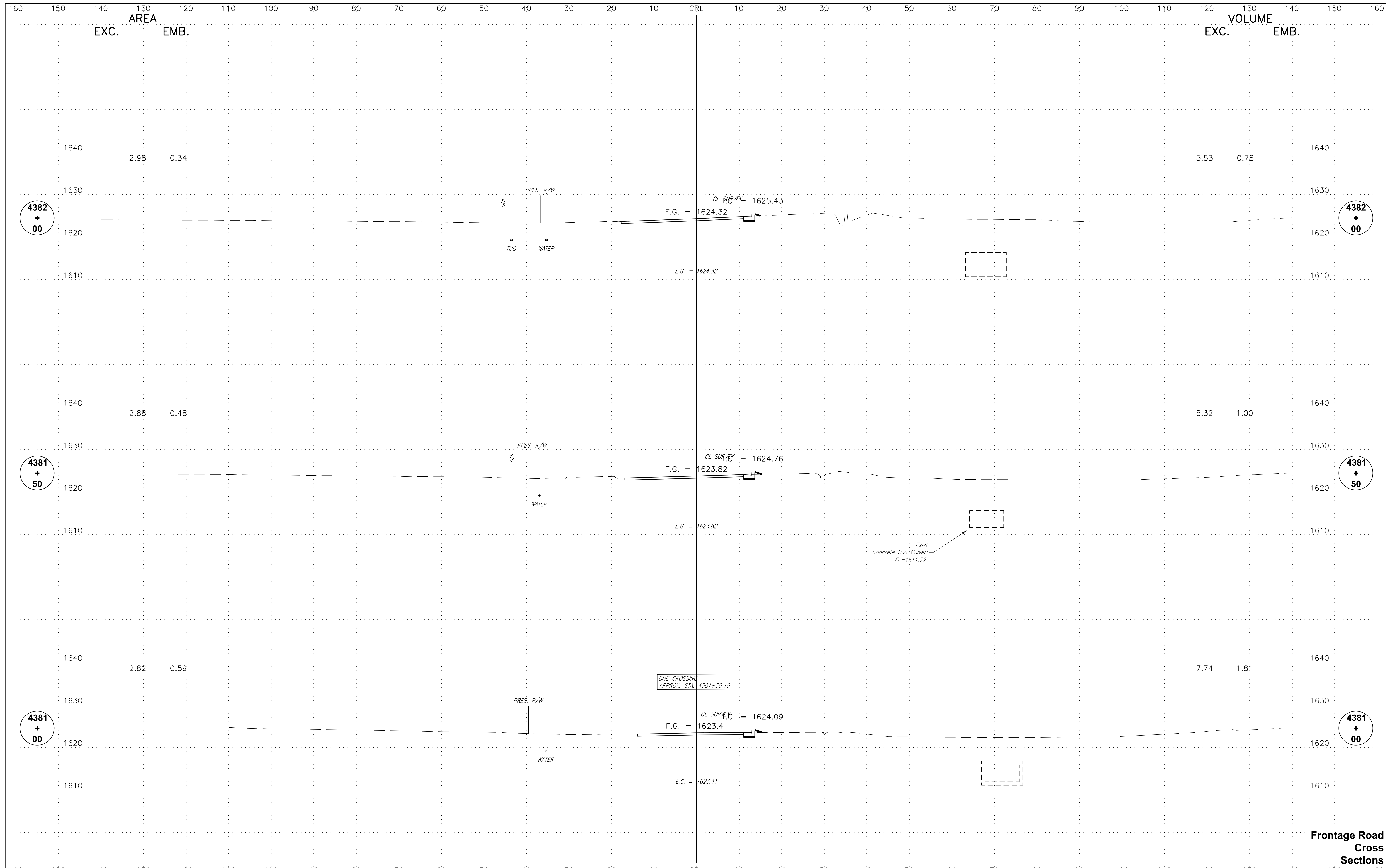
**RAMP A
CROSS
SECTIONS**



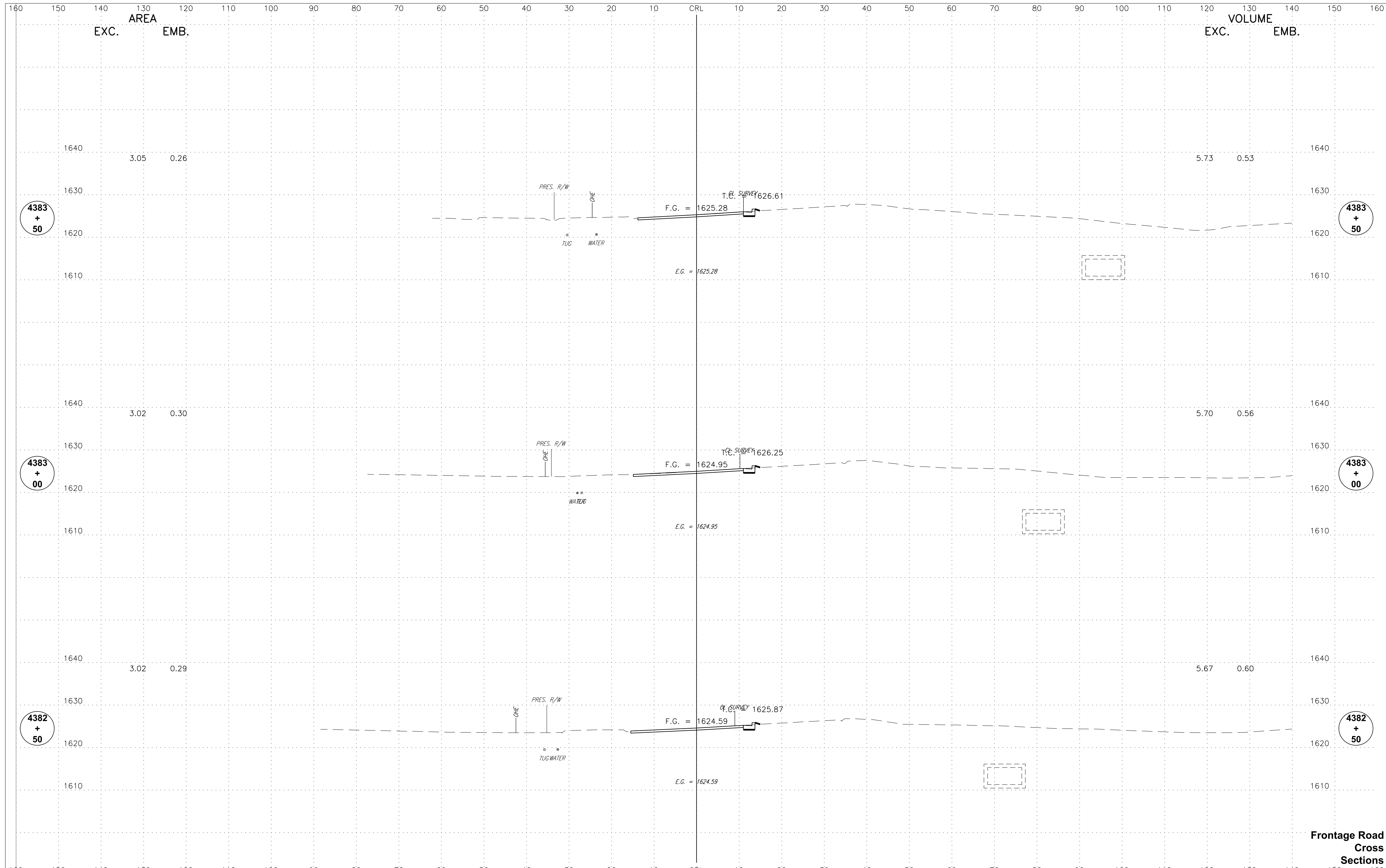
STA. 4411+92.17
END RAMP A CONST.

RAMP A
CROSS
SECTIONS

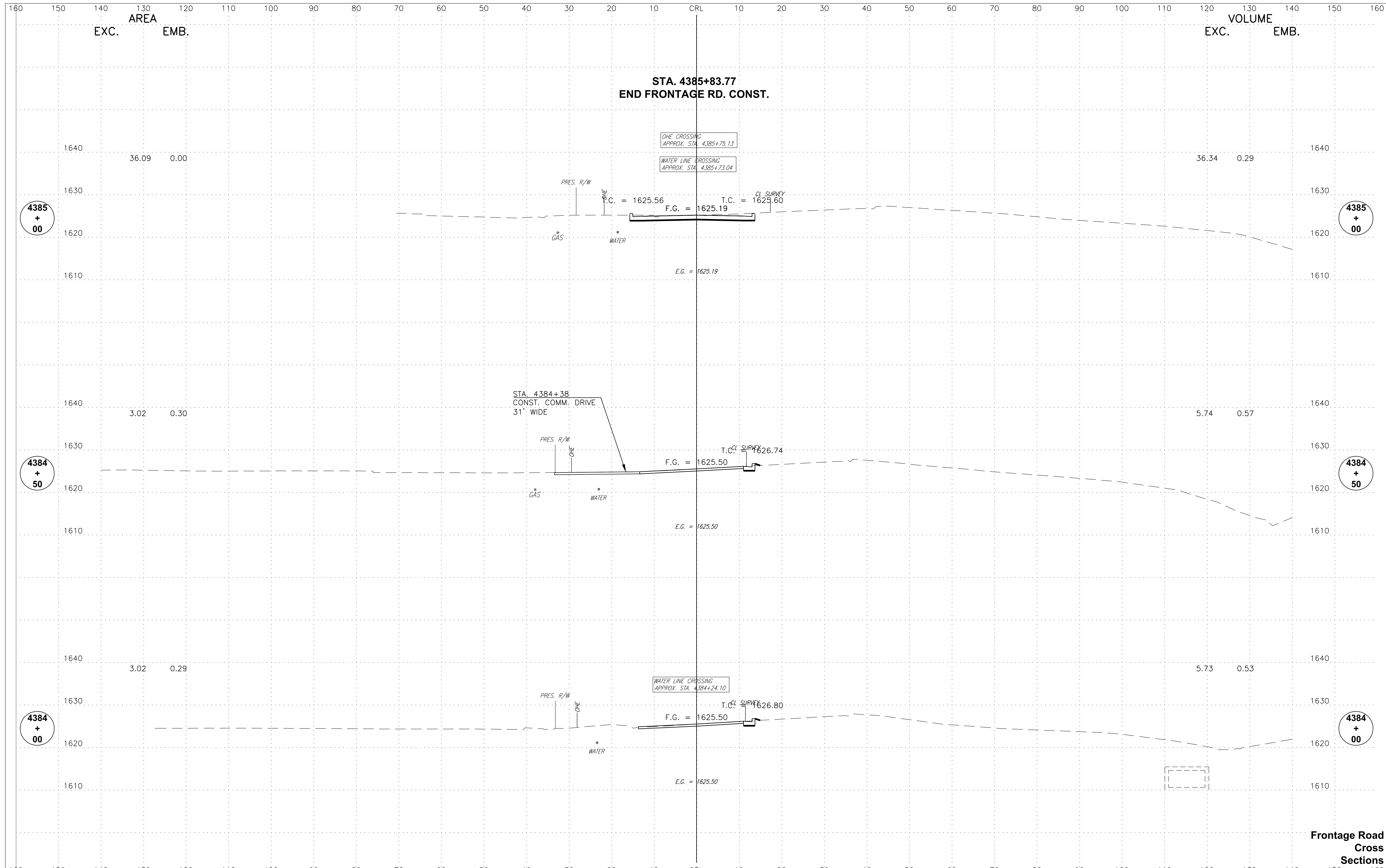




**Frontage Road
Cross
Sections**



**Frontage Road
Cross
Sections**



Frontage Road
Cross
Sections